



## AGENDA

GARDEN GROVE PLANNING COMMISSION

May 19, 2022 - 7:00 PM

COMMUNITY MEETING CENTER  
11300 STANFORD AVENUE

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**COVID-19 Information:** If you plan to attend the meeting in person, those who are unvaccinated must wear face masks indoors at all times. Please do not attend this meeting if you have had direct contact with someone who has tested positive for COVID-19, or if you are experiencing symptoms such as coughing, sneezing, fever, difficulty breathing or have other flu-like symptoms.

**Meeting Assistance:** Any person requiring auxiliary aids and services, due to a disability, to address the Planning Commission, should contact the Department of Community & Economic Development at (714) 741-5312 or email [planning@ggcity.org](mailto:planning@ggcity.org) 72 hours prior to the meeting to arrange for special accommodations. (Government Code §5494.3.2).

**Agenda Item Descriptions:** Are intended to give a brief, general description of the item. The Planning Commission may take legislative action deemed appropriate with respect to the item and is not limited to the recommended action indicated in staff reports or the agenda.

**Documents/Writings:** Any revised or additional documents/writings related to an item on the agenda distributed to all or a majority of the Planning Commission within 72 hours of a meeting, are made available for public inspection at the same time (1) in the Planning Services Division Office at 11222 Acacia Parkway, Garden Grove, CA 92840, during normal business hours; and (1) at the Community Meeting Center at the time of the meeting.

**Public Comments:** Members of the public who attend the meeting in-person and would like to address the Planning Commission are requested to complete a yellow speaker card indicating their name and address, and identifying the subject matter they wish to address. This card should be given to the Recording Secretary before the meeting begins. General comments are made during "Oral Communications" and are limited to three (3) minutes and to matters the Planning Commission has jurisdiction over. Persons wishing to address the Planning Commission regarding a Public Hearing matter will be called to the podium at the time the matter is being considered. Members of the public who wish to comment on matters before the Commission, in lieu of doing so in person, may submit comments by emailing [planning@ggcity.org](mailto:planning@ggcity.org) no later than 3:00 p.m. the day of the meeting. The comments will be provided to the Commission as part of the meeting record.

**PLEASE SILENCE YOUR CELL PHONES DURING THE MEETING.**

REGULAR MEETING AGENDA

ROLL CALL: CHAIR RAMIREZ, VICE CHAIR LINDSAY  
COMMISSIONERS ARESTEGUI, CUNNINGHAM, LEHMAN, PEREZ,  
SOEFFNER

PLEDGE OF ALLEGIANCE TO THE FLAG OF THE UNITED STATES OF AMERICA

- A. ORAL COMMUNICATIONS - PUBLIC
- B. APPROVAL OF MINUTES – April 21, 2022
- C. PUBLIC HEARING(S) (Authorization for the Chair to execute Resolution shall be included in the motion.)

C.1. CONDITIONAL USE PERMIT NO. CUP-217-2022

APPLICANT: HEDRA MAMLOUK  
LOCATION: NORTH OF GARDEN GROVE BOULEVARD, BETWEEN LORNA STREET AND JOSEPHINE STREET, AT 8725 GARDEN GROVE BOULEVARD

REQUEST: Conditional Use Permit approval to allow a new convenience store, Crazy Beer & Wine Market, to operate with an original Alcoholic Beverage Control (ABC) Type "20" (Off-Sale, Beer and Wine) License. The site is in the GGMU-2 (Garden Grove Boulevard Mixed Use 2) zone. The project is exempt from the CEQA pursuant to Government Code Section 15301 – Existing Facilities – of the State CEQA Guidelines.

STAFF RECOMMENDATION: Approval of Conditional Use Permit No. CUP-217-2022, subject to the recommended Conditions of Approval.

C.2. SITE PLAN NO. SP-112-2022  
TENTATIVE PARCEL MAP NO. PM-2021-112

APPLICANT: HENRY BALANZA  
LOCATION: NORTHEAST CORNER OF BEVERLY LANE AND ENEO PLACE AT 9811 BEVERLY LANE

REQUEST: Site Plan and Tentative Parcel Map approval to reconfigure two (2) existing adjacent parcels for the purpose of constructing a new two-story single-family home on each lot. Lot 1 will have a lot size of 10,500 square feet, while Lot 2 will have a lot size of 7,238 square feet. The existing single-family dwelling will be demolished to accommodate the proposed development. The site is in the R-1 (Single-Family Residential) zone. The project is exempt from CEQA pursuant to Government Code Section 15303 – New Construction or Conversion of Small Structures and Section 15315 – Minor Land Divisions, of the State CEQA Guidelines.

STAFF RECOMMENDATION: Approval of Site Plan No. SP-112-2022 and Tentative Parcel Map No. PM-2021-112, subject to the recommended Conditions of Approval.

C.3. SITE PLAN NO. SP-110-2022  
VARIANCE NO. V-036-2022  
LOT LINE ADJUSTMENT NO. LLA-029-2022

APPLICANT: PROLOGIS, L.P.

LOCATION: WEST SIDE OF PALA DRIVE AND INDUSTRY STREET, NORTH OF ACACIA AVENUE, AND SOUTH OF LAMPSON AVENUE AT 12641 INDUSTRY STREET AND 12691 PALA DRIVE

REQUEST: Site Plan approval to demolish the 65,880 square-foot building at 12641 Industry Street, and the 83,100 square-foot building at 12691 Pala Drive, in order to build a new 148,284 square-foot industrial building. Also, a Variance request to deviate from the maximum building height requirement of the M-P (Industrial Park) zone, and a Lot Line Adjustment request to consolidate 12641 Industry Street and 12691 Pala Drive into a single parcel. The site is in the M-P (Industrial Park) zone. The project is exempt from CEQA pursuant to Government Code Section 15302 – Replacement or Reconstruction, of the State CEQA Guidelines.

STAFF RECOMMENDATION: Approval of Site Plan No. SP-110-2022, Variance No. V-036-2022, and Lot Line Adjustment No. LLA-029-2022, subject to the recommended Conditions of Approval.

C.4. SITE PLAN NO. SP-111-2022  
TENTATIVE PARCEL MAP NO. PM-2021-190

APPLICANT: DR. MICHAEL DAO

LOCATION: NORTHEAST CORNER OF GARDEN GROVE BOULEVARD AND BROOKHURST STREET AT 10201 AND 10231 GARDEN GROVE BOULEVARD

REQUEST: Site Plan approval to construct a five-story mixed-use development on a 1.86-acre site consisting of two parcels with 9,786 square feet of retail space, 9,270 square feet of medical space, and 52 apartment units with a 22.5% affordable housing density bonus for “very low-income” households. Pursuant to the State Density Bonus law, the applicant is requesting one (1) concession and three (3) waivers from the GGMU-2 (Garden Grove Boulevard Mixed Use 2) zone development standards: (1) a concession to reduce the minimum required private balcony area and dimensions for each unit, (2) a waiver to reduce the minimum

active recreation dimensions and to allow the indoor and roof deck recreation areas to contribute to more than 50% of the required open space; (3) a waiver to allow the building to exceed the maximum building height of 50 feet or 4-stories; and (4) a waiver to deviate from the minimum 300 cubic feet of private storage per unit. Also, Tentative Parcel Map approval to consolidate the two (2) existing parcels into a single lot to accommodate the proposed development. The site is in the GGMU-2 (Garden Grove Boulevard Mixed Use 2) zone. The project is exempt from CEQA pursuant to Government Code Section 15332 – In-Fill Development Projects, of the State CEQA Guidelines.

STAFF RECOMMENDATION: Approval of Site Plan No. SP-111-2022 and Tentative Parcel Map No. PM-2021-190, subject to the recommended Conditions of Approval.

- D. MATTERS FROM COMMISSIONERS
- E. MATTERS FROM STAFF
- F. ADJOURNMENT

GARDEN GROVE PLANNING COMMISSION  
Community Meeting Center  
11300 Stanford Avenue, Garden Grove, CA 92840

Meeting Minutes  
Thursday, April 21, 2022

CALL TO ORDER: 7:10 p.m.

ROLL CALL:

Chair Ramirez  
Vice Chair Lindsay  
Commissioner Arestegui  
Commissioner Cunningham  
Commissioner Lehman  
Commissioner Perez  
Commissioner Soeffner

Absent: Cunningham, Lehman, Soeffner

PLEDGE OF ALLEGIANCE: Led by Commissioner Lindsay.

ORAL COMMUNICATIONS – PUBLIC – None.

April 7, 2022 MINUTES:

Action: Received and filed.

Motion: Perez Second: Lindsay

Ayes: (4) Arestegui, Lindsay, Perez, Ramirez  
Noes: (0) None  
Absent: (3) Cunningham, Lehman, Soeffner

STUDY SESSION – FOCUSED ZONING AMENDMENTS UPDATE: A PRESENTATION TO DISCUSS THE PROPOSED FOCUSED ZONING CODE TEXT AMENDMENTS RELATING TO THE CREATION OF OBJECTIVE DEVELOPMENT STANDARDS FOR SINGLE-FAMILY RESIDENTIAL, MULTI-FAMILY RESIDENTIAL, MIXED-USE, SUPPORTIVE HOUSING, SINGLE-ROOM OCCUPANCY (SRO) HOUSING, AND MOTEL/HOTEL CONVERSIONS –  
Consultant Laura Stetson of MIG (Moore Iacofano Goltsman, Inc.) presented an overview of the Zoning Code text amendments related to the creation of Objective Development Standards with the goal to ensure that development standards comply with applicable State housing laws. The Zoning Code amendments would create greater certainty for applicants, decision makers, and residents and will address supportive housing, motel conversions, single-room occupancy, single-family residential interior standards, open space requirements, and objective design

standards (ODS). The ODS will clarify the difference between Design Guidelines and Design Standards, as well as between Objective Design Standards and Development Standards. The City's current subjective standards will be updated with objective standards as applied to single-family subdivisions, multi-family, and mixed-use residential projects. The ODS does not apply to commercial-only projects, single-family residential, non-residential development, historical properties, and ADU/JADU/SB9 units. The timeline goal is for the Ordinance revisions to take effect in October of 2022.

Discussion included the suggestion to regulate paint colors, step-backs for multi-story buildings, recessed windows, building articulation, no limitations on modifying, adding or deleting objective standards, preventing graffiti on utility screens and trash enclosures, the number of homeless in Garden Grove (50% residents/50% transients), motel conversion security, limits on number of residential bathrooms in a dwelling, and lack of parking for transitional housing.

MATTERS FROM COMMISSIONERS: None.

MATTERS FROM STAFF: Staff noted the May 5th meeting would be cancelled and gave a brief description of the agenda items for May 19th.

ADJOURNMENT: At 8:10 p.m. to the next Meeting of the Garden Grove Planning Commission on Thursday, May 19, 2022, at 7:00 p.m. in the Community Meeting Center, 11300 Stanford Avenue, Garden Grove.

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Judith Moore  
Recording Secretary

# COMMUNITY AND ECONOMIC DEVELOPMENT DEPARTMENT PLANNING STAFF REPORT

<b>AGENDA ITEM NO.:</b> C.1	<b>SITE LOCATION:</b> North of Garden Grove Boulevard, between Lorna Street and Josephine Street, at 8725 Garden Grove Boulevard
<b>HEARING DATE:</b> May 19, 2022	<b>GENERAL PLAN:</b> Residential / Commercial Mixed Use 2
<b>CASE NO.:</b> Conditional Use Permit No. CUP-217-2022	<b>ZONE:</b> GGMU-2 (Garden Grove Boulevard Mixed Use 2)
<b>APPLICANT:</b> Hedra Mamlouk	<b>CEQA DETERMINATION:</b> Exempt – Section 15301 – Existing Facilities
<b>PROPERTY OWNER:</b> Keith To	<b>APN:</b> 133-462-20

**REQUEST:**

The applicant is requesting approval of a Conditional Use Permit to allow a new convenience store, Crazy Beer & Wine Market, to operate with an original State Alcoholic Beverage Control (ABC) Type “20” (Off-Sale, Beer and Wine) License.

**BACKGROUND:**

The subject site is an existing in-line, commercial building located at 8725 Garden Grove Boulevard, between Lorna Street and Josephine Street. The site has a General Plan Land Use designation of Residential/Commercial Mixed Use 2, and is zoned GGMU-2 (Garden Grove Boulevard Mixed Use 2). The shopping center is adjacent to GGMU-2 zoned properties to the north, east, and across Lorna Street to the west; and GGMU-2 and Planned Unit Development No. PUD-131-99 zoned properties, across Garden Grove Boulevard, to the south. Although directly abutting the site are commercial properties, the site is located within a neighborhood that consists of different types of uses, including retail commercial, single-family, and multifamily residential.

The specific tenant space is located on the west side of the in-line commercial building. The space is approximately 1,290 square feet in size. According to Business Tax records, the subject tenant space has been used as an office for a cargo business, Savvy N Save. However, the space is currently vacant as the cargo business has ceased its operation in May 2021. The applicant took over the subject tenant space in October 2021, and proposes to operate a new convenience store with a State Alcoholic Beverage Control (ABC) Type “20” (Off-Sale, Beer and Wine) License. The Municipal Code requires the approval of a Conditional Use Permit when there is an application for a new original ABC license. Approval of this Conditional

Use Permit will establish conditions of approval to regulate the operation of the convenience store to minimize potential impacts to the community due to the sale of alcohol. The convenience store is anticipated to be open in May 2022.

The request was originally considered by the Zoning Administrator on March 24, 2022 public hearing. Three (3) members of the general public spoke in opposition to the project. Concerns raised by these speakers included: (1) that the subject area is already saturated with criminal activities including, but not limited to, transients, vandalism, sex offenders, and drug-related crimes; (2) that the area is already served with multiple stores that sell alcohol, including gas stations and grocery stores along Garden Grove and Beach Boulevard; and (3) that the immediate area would become more unsafe with the addition of a new beer and wine market. In response to the opposition, the applicant's representative confirmed that the operator of the convenience store would work diligently with the community to address all of their concerns. Otherwise, no other speaker from the public came forward to speak in favor of the project. After hearing the testimony from both sides, the Zoning Administrator remanded Conditional Use Permit No. CUP-217-2022 to the Planning Commission for consideration.

### **DISCUSSION:**

The Crazy Beer & Wine Market is approximately 1,290 square feet in size. The interior of the store will consist of a cashier counter area, a sales area with coolers and display shelves, storage rooms, and a restroom. The Crazy Beer & Wine Market will sell different types of products that are commonly found in any convenience store, including snacks, tobacco, soft drinks, and small household items.

The store will be open from 9:00 a.m. to 9:00 p.m., seven (7) days a week. Pursuant to the proposed Conditions of Approval, in the event problems arise concerning the operation of the business, the hours of operation may be reduced by order of the Chief of the Police Department.

The convenience store is located in a high-crime district. Per Police records, there were a total of 2,218 calls for services to the area within one mile from the subject site for the period between January 1 and March 31, 2022. Among these calls for services, there were 145 arrests, 392 reports, and 1681 calls for service that were cleared by other means. The majority of the arrests were narcotics related (49 arrests), and the majority of the reports taken were for theft related offenses (81 reports).

The site is located in an area of under-concentration of Alcoholic Beverage Control off-sale licenses. Currently, there is only one (1) off-sale license in the District. The approval of this CUP will increase the number of off-sale licenses in District 881.07 to two (2). For convenience, a map showing the location of all of the nearby active off-sale licenses is also provided (Exhibit A). In order to approve the request, a Finding of Public Convenience or Necessity is required. A summary of the district



and the Findings of Public Convenience or Necessity can be found in Resolution No. 6043-22 for Conditional Use Permit No. CUP-217-2022.

The Community and Economic Development Department and the Police Department have reviewed the request and are supporting the proposal. All standard conditions of approval for an ABC Type "20" License will apply.

**CEQA:**

CEQA's Class 1 exemption applies to the operation, repair, maintenance, permitting, leasing, licensing, and minor alterations of existing facilities, with negligible or no expansion of use (CEQA Guidelines §15301). The subject location is an existing tenant space within an in-line commercial building. In addition, a convenience store with alcohol sales is conditionally allowed in the GGMU-2 zone. Provided the conditions of approval are adhered to for the life of the project, the use will be compatible with other adjacent uses. Therefore, the proposed project is exempt from CEQA.

**RECOMMENDATION:**

Staff recommends that the Planning Commission take the following action:

1. Adopt Resolution No. 6043-22, approving Conditional Use Permit No. CUP-217-2022, subject to the recommended conditions of approval.



LEE MARINO  
Planning Services Manager



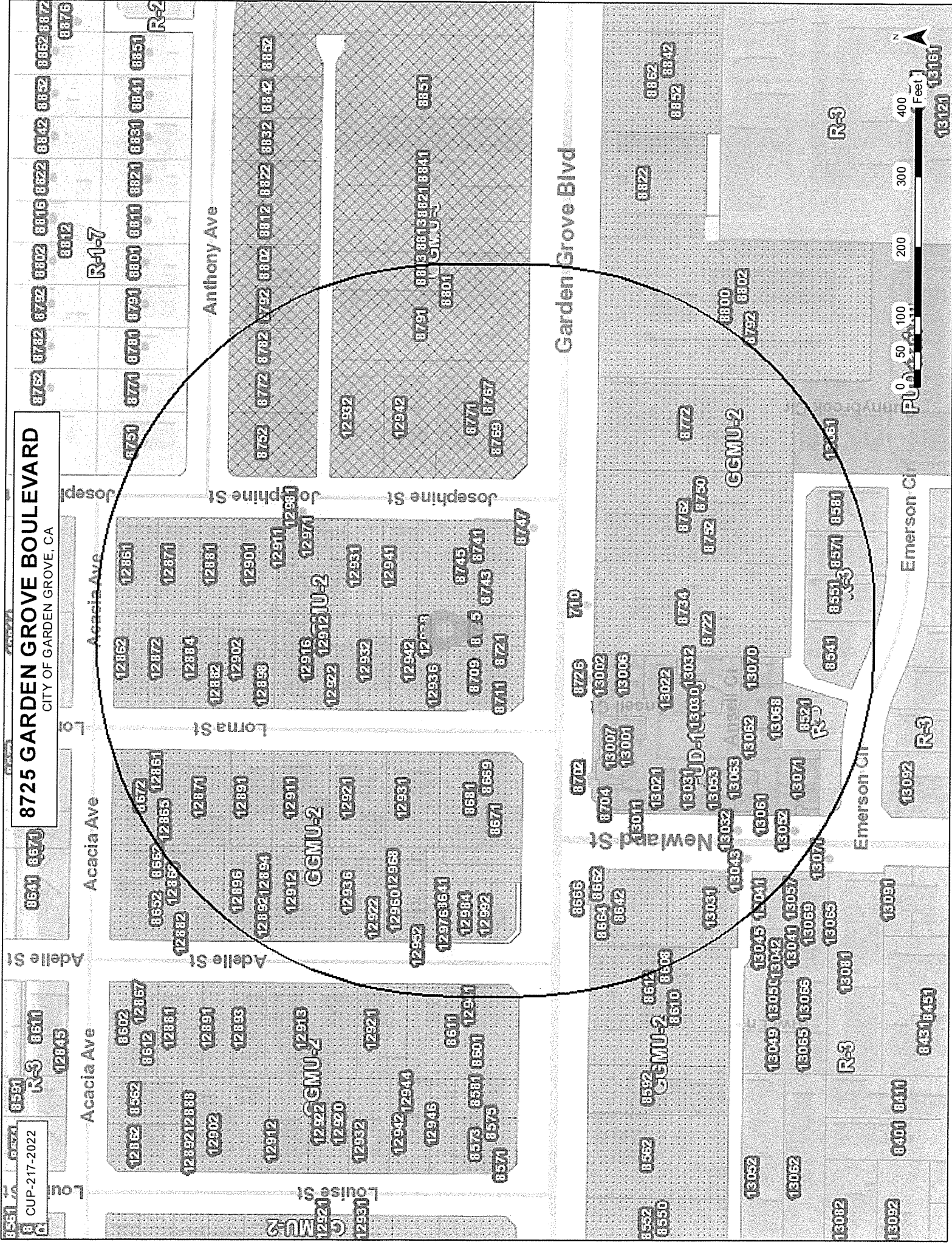
By: Huong Ly  
Assistant Planner

Attachment:

- Exhibit A: Location of all of the nearby active off-sale ABC Licenses.
- Exhibit B: Zoning Administrator staff report, and decision with conditions of approval
- Exhibit C: Zoning Administrator Minutes of March 24, 2022

# 8725 GARDEN GROVE BOULEVARD

CITY OF GARDEN GROVE, CA



**Project:**  
 8701-8725 GARDEN GROVE

**Address:**  
 ..

**Account Manager:**  
 Ryan To

**Designer:**  
 Minh N.

**Date:**  
 Feb 15, 2018

**Contractor Lic.:**  
 901709 CA 45

**Invoice #**

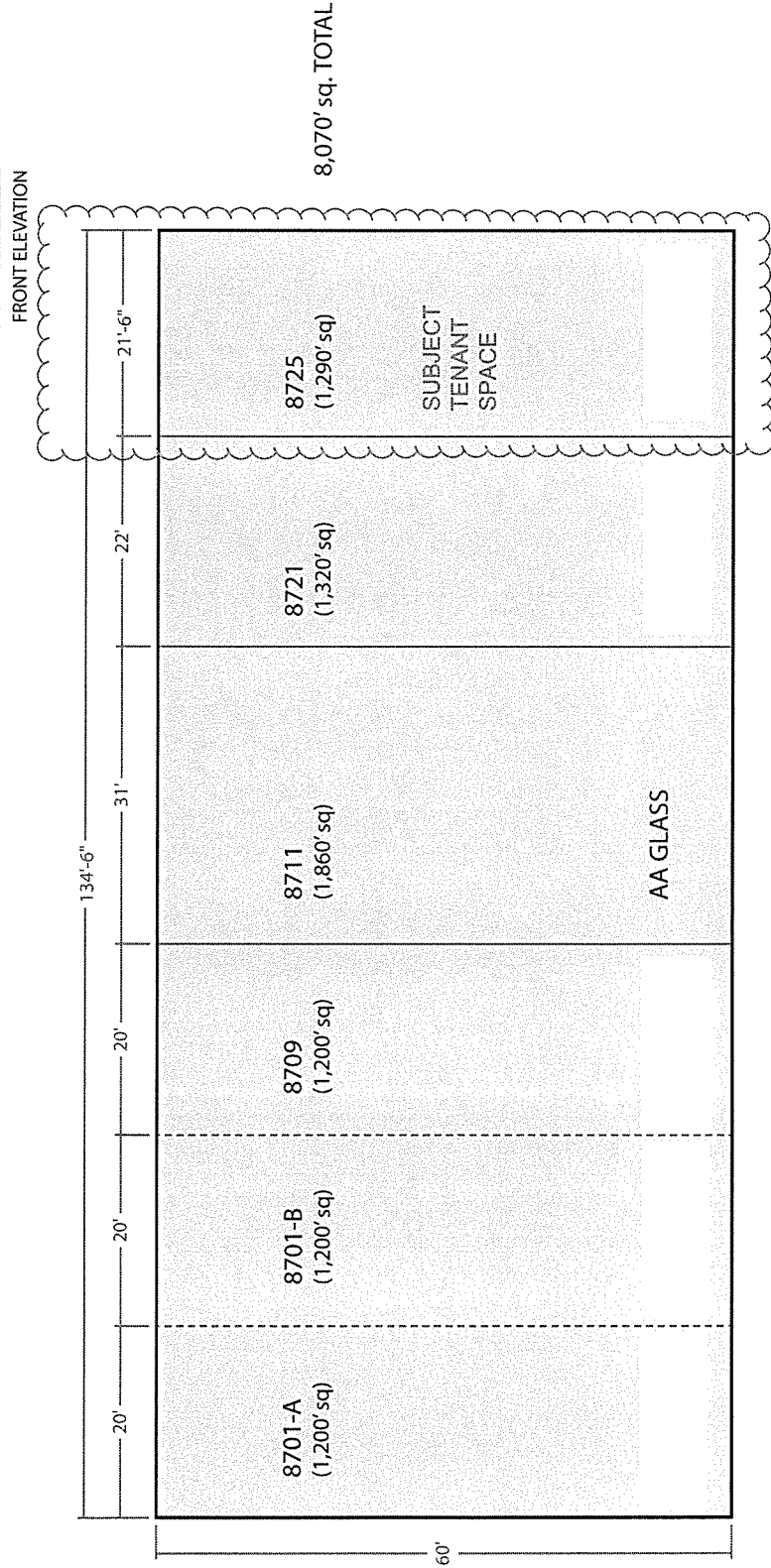
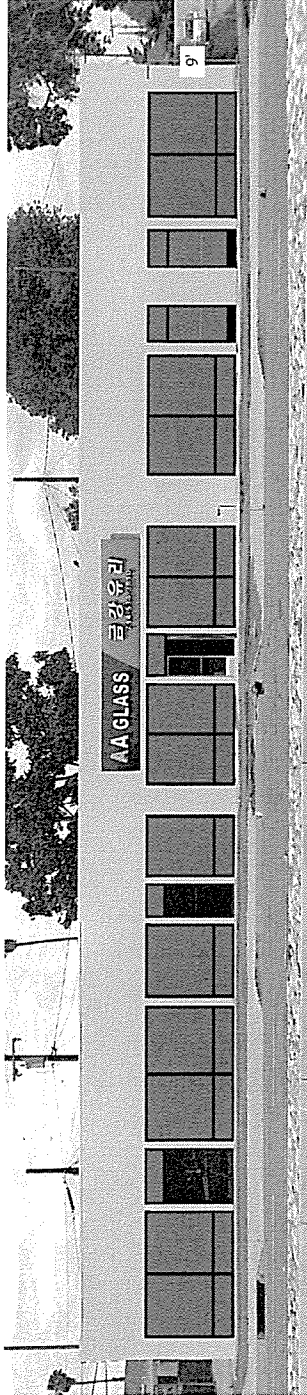
**Revisions:**

**Page: A01.00**

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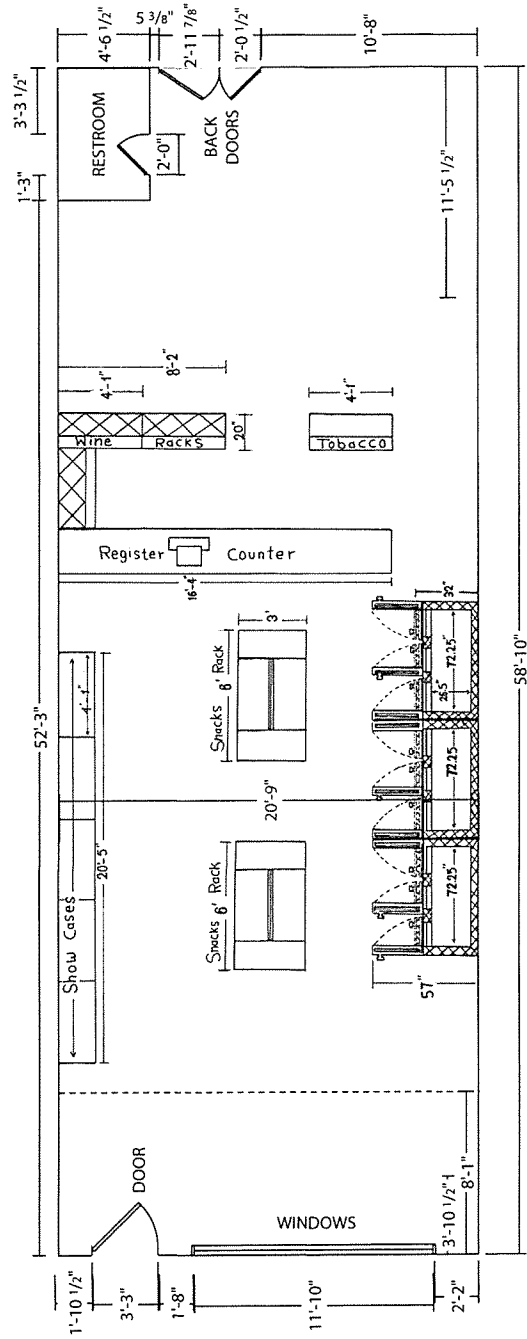
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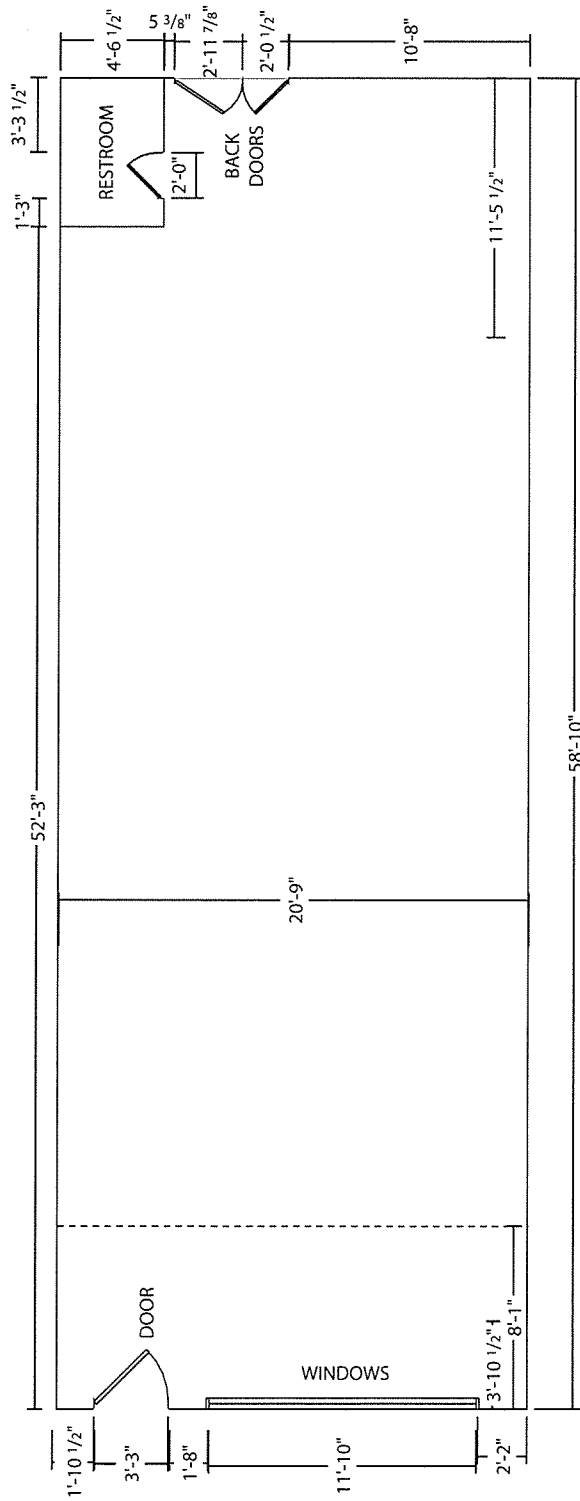
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**FLOOR PLAN - 8701-8725 GARDEN GROVE**

SCALE: 1:150





RESOLUTION NO. 6043-22

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF GARDEN GROVE APPROVING CONDITIONAL USE PERMIT NO. CUP-217-2022 FOR A PROPERTY LOCATED ON NORTH SIDE OF GARDEN GROVE BOULEVARD, BETWEEN LORNA STREET AND JOSEPHINE STREET, AT 8725 GARDEN GROVE BOULEVARD, ASSESSOR'S PARCEL NO. 133-462-20.

BE IT RESOLVED that the Planning Commission of the City of Garden Grove, in regular session assembled on May 19, 2022, does hereby approve Conditional Use Permit No. CUP-217-2022, for a property located on the north side of Garden Grove Boulevard, between Lorna Street and Josephine Street, at 8725 Garden Grove Boulevard, Assessor's Parcel No. 133-462-20, subject to the Conditions of Approval attached hereto as "Exhibit A", and hereby determines that the public convenience and necessity would be served by issuance of a State Alcoholic Beverage Control ABC Type "20" (Off-sale, Beer and Wine) License at this location.

BE IT FURTHER RESOLVED in the matter of Conditional Use Permit No. CUP-217-2022, the Planning Commission of the City of Garden Grove does hereby report as follows:

1. The subject case was initiated by Hedra Mamlouk for Crazy Beer & Wine Market on behalf of the property owner, Keith To.
2. The applicant is requesting Conditional Use Permit approval to operate a new convenience store, Crazy Beer & Wine Market, with a new original State Alcoholic Beverage Control (ABC) Type "20" (Off-sale, Beer and Wine) License.
3. The City of Garden Grove has determined that this project is exempt from review under the California Environmental Quality Act ("CEQA") pursuant to Article 19, Section 15301, Existing Facilities, of the CEQA Guidelines.
4. The property has a General Plan Land Use designation of Residential/Commercial Mixed Use 2, and is zoned GGMU-2 (Garden Grove Boulevard Mixed Use 2).
5. Existing land use, zoning, and General Plan designation of property within the vicinity of the subject property have been reviewed.
6. Report submitted by City Staff was reviewed.
7. Pursuant to a legal notice, a public hearing was held on May 19, 2022 and all interested persons were given an opportunity to be heard.
8. The Planning Commission gave due and careful consideration to the matter during its meeting of May 19, 2022 and

BE IT FURTHER RESOLVED, FOUND AND DETERMINED that the facts and reasons supporting the conclusion of the Planning Commission, as required under Municipal Code Section 9.32.030 (Conditional Use Permits), are as follows:

FACTS:

The subject site is an existing in-line, commercial building located at 8725 Garden Grove Boulevard, between Lorna Street and Josephine Street. The site has a General Plan Land Use designation of Residential/Commercial Mixed Use 2, and is zoned GGMU-2 (Garden Grove Boulevard Mixed Use 2). The shopping center is adjacent to GGMU-2 zoned properties to the north, east, and across Lorna Street to the west; and GGMU-2 and Planned Unit Development No. PUD-131-99 zoned properties, across Garden Grove Boulevard, to the south. Although directly abutting the site are commercial properties, the site is located within a neighborhood that consists of different types of uses, including retail commercial, single-family, and multifamily residential.

The specific tenant space is located on the west side of the in-line commercial building. The space is approximately 1,290 square feet in size. According to Business Tax records, the subject tenant space has been used as an office for a cargo business, Savvy N Save. However, the space is currently vacant as the cargo business ceased its operation in May 2021. The applicant took over the subject tenant space in October 2021, and proposes to operate a new convenience store with a State Alcoholic Beverage Control (ABC) Type "20" (Off-Sale, Beer and Wine) License. The Municipal Code requires the approval of a Conditional Use Permit when there is an application for a new original ABC license. Approval of this Conditional Use Permit will establish conditions of approval to regulate the operation of the convenience store to minimize potential impacts to the community due to the sale of alcohol. The convenience store is anticipated to be open in May 2022.

The subject convenience store is located in a high-crime reporting district. A summary of the district is as follows:

- The subject site is located in Crime Reporting District No. 74.
- The crime count for the District is 143.
- Average crime count per district in the City is 91.
- A District is considered high when it exceeds the Citywide average by 20%.
- The subject District has a crime count of 57% above the Citywide average; therefore, it is considered a high-crime area.
- The subject site is located in Alcoholic Beverage Control Census Report District No. 881.07.
- ABC Census Reporting District No. 881.07 allows for three off-sale licenses within the District. Currently, there is only one (1) off-sale license in the District. The approval of this CUP will increase the number of off-sale licenses in District 881.07 to two (2).

PUBLIC CONVENIENCE OR NECESSITY:

Because the establishment is located within a district with a high crime reporting rate, issuance of the requested ABC license would result in an "undue concentration" of licenses, and, pursuant to Business and Provisions Code Sections 23958 and 23958.4, ABC may not issue a new alcohol license to the applicant unless the Planning Commission determines that the public convenience or necessity would be served by issuance of the license.

Although the proposed convenience store is located in an area considered to be in a high-crime reporting district, a finding for public convenience or necessity is warranted given the potential community benefit through the addition of a beer and wine license. Since the area has only one active off-sale license, the Crazy Beer & Wine Market would provide an additional amenity to nearby local residents. The use will not adversely affect the health, peace, comfort or welfare of persons residing or working in the surrounding area. The conditions of approval will minimize potential impacts to the adjoining area. Provided the conditions of approval are adhered to for the life of the project, the use will be harmonious with the persons who work and live in the area.

The Community and Economic Development Department and the Police Department have reviewed the request and support the proposal. All standard conditions of approval for an ABC Type "20" License will apply.

FINDINGS AND REASONS:

1. That the proposed use will be consistent with the City's adopted General Plan and redevelopment plan.

The subject site has a General Plan Land Use Designation of Residential/Commercial Mixed Use 2 (RC2), and is zoned GGMU-2 (Garden Grove Boulevard Mixed Use 2). The RC2 designation is intended to allow a range of commercial activities that serve local residential neighborhoods and the larger community. In particular, the subject proposal is consistent with the following General Plan Goals, Policies, and Implementation Measures:

*Policy LU-1.3 Encourage a wide variety of retail and commercial services, such as restaurants and cultural arts / entertainment, in appropriate locations.* The subject convenience store is located in an area already developed with retail, and other commercial services. Thus, the proposed convenience store with an ABC Type "20" License at the subject location is consistent with surrounding uses.

*Goal LU-4 The City seeks to develop uses that are compatible with one another.* The subject in-line commercial building consists of different types of retail uses including a smoke shop, a glass store, a signage company, and a



plant shop. Thus, the proposed convenience store is compatible with all of the existing retail uses at this location.

*Goal ED-2 The City must attract new businesses, while supporting and assisting those already located within Garden Grove.* The proposed Conditional Use Permit is intended to provide additional amenities at the new convenience store, Crazy Beer & Wine Market. Should the Conditional Use Permit be approved, the City is providing new businesses all of the resources they needed to be successful.

*SAF-IMP-2C Involve law enforcement agencies in the design and planning phases of ABC licensed establishments to reduce design elements that conceal or encourage criminal activity.* The Garden Grove Police Department has reviewed all relevant data pertaining to the proposed Conditional Use Permit. The intent of their review is to reduce the number of alcohol-related crimes, and promote the safe operation of the convenience store. No concerns were raised by the Police Department regarding the addition of an ABC License to the convenience store, and they are therefore supportive of the Conditional Use Permit request.

2. That the requested use at the location proposed will not: adversely affect the health, peace, comfort, or welfare of the persons residing or working in the surrounding area, or unreasonably interfere with the use, enjoyment, or valuation of the property of other persons located in the vicinity of the site, or jeopardize, endanger, or otherwise constitute a menace to public health, safety, or general welfare.

The use will not adversely affect the health, peace, comfort or welfare of persons residing or working in the surrounding area. The proposed convenience store with beer and wine sales will be consistent and compatible with the existing uses in the area. The proposed use complies with the intent of the GGMU-2 zone, by providing a small-scale convenience shopping facility, oriented to the neighborhood level. The proposed convenience store will be open from 9:00 a.m. to 9:00 p.m., seven (7) days a week. Standard hours of operation that apply to Type "20" Licensed convenience stores have been applied. The conditions of approval will minimize potential impacts to the adjoining area. Provided the conditions of approval are adhered to for the life of the project, the use will be harmonious with the persons who work and live in the area.

The use will not jeopardize, endanger, or otherwise constitute a menace to public health, safety, or general welfare. The Police Department has reviewed the proposal, and all appropriate conditions of approval have been incorporated. The conditions of approval will ensure public health, safety and welfare.

3. That the proposed site is adequate in size and shape to accommodate the yards, walls, fences, parking and loading facilities, landscaping and other development features prescribed in this title or as is otherwise required in order to integrate such use with the uses in the surrounding area.

All site improvements are existing. No change to the structure is included in the proposal. The site, with the existing site improvements, is of adequate size to accommodate the proposed use within the surrounding area.

4. That the proposed site is adequately served: by highways or streets or sufficient width and improved as necessary to carry the kind and quantity of traffic such as to be generated, and by other public or private service facilities as required.

The site is located north of Garden Grove Boulevard, between Lorna Street and Josephine Street, which are fully developed streets that provide adequate traffic circulation and driveway access to public parking areas. The site is also sufficiently served by the public service facilities required, such as public utilities: gas, electric, water, and sewer facilities.

#### INCORPORATION OF FACTS AND REASONS SET FORTH IN THE STAFF REPORT

In addition to the foregoing, the Planning Commission incorporates herein by this reference, the facts and reasons set forth in the staff report.

BE IT FURTHER RESOLVED that the Planning Commission does conclude:

1. The Conditional Use Permit does possess characteristics that would indicate justification of the request in accordance with Municipal Code Section 9.32.030 (Conditional Use Permits) and Chapter 9.18.060 (Alcohol Beverage Sales).
2. In order to fulfill the purpose and intent of the Municipal Code and thereby promote the health, safety, and general welfare, the following Conditions of Approval, attached as Exhibit "A", shall apply to Conditional Use Permit No. CUP-217-2022.

**EXHIBIT "A"**  
**Conditional Use Permit No. CUP-217-2022**

8725 Garden Grove Boulevard  
(APN: 133-462-20)

**CONDITIONS OF APPROVAL**

**General Conditions**

1. Each owner of the property shall execute, and the applicant shall record against the property, a "Notice of Discretionary Permit Approval and Agreement with Conditions of Approval," as prepared by the City Attorney's Office, within 30 days of approval. This Conditional Use Permit runs with the land and is binding upon the property owner, his/her/its heirs, assigns, and successors in interest.
2. All Conditions of Approval set forth herein shall be binding on and enforceable against each of the following, and whenever used herein, the term "applicant" shall mean and refer to each of the following: the property owner, Keith To, the project applicant, Hedra Mamlouk for Crazy Beer & Wine Market, the developer of the project, the future owner(s) and tenants(s) of the property, and each of their respective successors and assigns. The applicant and subsequent owner/operators of such business shall adhere to the conditions of approval for the life of the project, regardless of property ownership. Any changes of the conditions of approval require approval by the applicable City hearing body, except as otherwise provided herein.
3. This Conditional Use Permit only authorizes the operation of a 1,290 square foot convenience store with an Alcoholic Beverage Control Type "20" (Off-Sale, Beer and Wine) License as identified on the floor plan attached to these Conditions of Approval. Approval of this Conditional Use Permit shall not be construed to mean any waiver of applicable and appropriate zoning and other regulations; and wherein not otherwise specified, all requirements of the City of Garden Grove Municipal Code shall apply.
4. Minor modifications to the approved site plan, floor plan, and/or these Conditions of Approval may be approved by the Community and Economic Development Director, in his or her discretion. Proposed modifications to the approved floor plan, site plan, or Conditions of Approval that would result in the intensification of the project or create impacts that have not been previously addressed, and which are determined by the Community and Economic Development Director not to be minor in nature shall be subject to approval of new and/or amended land use entitlements by the applicable City hearing body.

5. All conditions of approval shall be implemented at the applicant's expense, except where specified in the individual condition.

**Police Department**

6. There shall be no gaming tables or gaming machines, as outlined in City Code Sections 8.20.010 and 8.20.050, on the premises at any time.
7. Hours of operation shall be permitted from 9:00 a.m. to 9:00 p.m., seven (7) days a week. The hours of operation shall be applicable to the entire premises. The City of Garden Grove reserves the right to reduce hours of operation, by order of the Chief of the Police Department, in the event problems arise due to noise, disturbances or other problems that may be resolved by modifying the hours of operation.
8. There shall be no customers or patrons in or about the premises when the establishment is closed.
9. In the event security problems occur, and at the request of the Police Department, the permittee, at his own expense, shall provide a California licensed, uniformed security guard(s) on the premises during such hours as requested by the Police Department.
10. The sale of alcoholic beverages for consumption on the premises is prohibited. Consumption of alcoholic beverages shall not occur anywhere on the site or within the premises.
11. All pay phones located on the property, adjacent to the premises, shall be limited to out-going calls only. This condition shall be complied with within 30 days following approval of this application.
12. The business, prior to obtaining the Alcoholic Beverage Control (ABC) Type "20" (Off-Sale, Beer and Wine) License, shall show proof to the Police Department that all members of the business staff have completed the LEAD training (Licensee Education on Alcohol and Drugs) through the Alcoholic Beverage Control Department or an ABC approved "Responsible Beverage Service (RBS) Training" program.
13. In the event that the Alcoholic Beverage Control (ABC) License is suspended for any period of time and/or fined for any ABC violation as a result of disciplinary action, the Conditional Use Permit shall be presented to the Planning Commission for review or further consideration.

14. Any violations or noncompliance with the conditions of approval may result in the issuance of an Administrative Citation of up to \$1,000 pursuant to GGMC 1.22.010 (a).

**Community and Economic Development Department**

15. This approval shall allow the convenience store to operate with ancillary alcoholic beverage sales.
16. The sales area for alcoholic beverages shall not exceed 5% of the retail sales floor area of the store.
17. No outdoor storage or display shall be permitted outside of the building, including, but not limited to, storage and display of merchandise, cardboard, pallets, or boxes.
18. A prominent, permanent sign stating "NO LOITERING IS ALLOWED ON OR IN FRONT OF THE PREMISES" shall be posted in a place that is clearly visible to patrons of the licensee. The sign lettering shall be four (4) to six (6) inches high with black letters on a white background. The sign shall be displayed near or at the entrance, and shall also be visible to the public.
19. There shall be no amusement devices permitted on the premises at any time.
20. Any adult merchandise (books, magazines, videos) shall be kept under the control of the management, behind the cashier's counter, and must be segregated and screened from minors.
21. There shall be no uses or activities permitted of an adult-oriented nature as outlined in City Code Section 9.08.070.
22. There shall be no deliveries to or from the premises between the hours of 10:00 p.m. and 7:00 a.m., seven (7) days a week.
23. All rear doors shall be kept closed at all times, except to permit employee ingress and egress, and in emergencies.
24. Litter shall be removed daily from the premises, including adjacent public sidewalks, and from all parking areas under the control of the licensee. These areas shall be swept or cleaned, either mechanically or manually, on a weekly basis, to control debris.

25. All trash bins shall be kept inside the trash enclosure, and gates closed at all times, except during disposal and pick-up. Trash pick-up shall be at least three (3) times a week.
26. The applicant/property owner shall maintain all existing landscaped areas in a neat and healthy condition. Landscaping maintenance shall include pruning or removal of overgrown weeds and vegetation.
27. The applicant/property owner shall abate all graffiti vandalism within the premises. The applicant/property owner shall implement best management practices to prevent and abate graffiti vandalism within the premises throughout the life of the project, including, but not limited to, timely removal of all graffiti, the use of graffiti resistant coatings and surfaces, the installation of vegetation screening of frequent graffiti sites, and the installation of signage, lighting, and/or security cameras, as necessary. Graffiti shall be removed/eliminated by the applicant/property owner as soon as reasonably possible after it is discovered, but not later than 72 hours after discovery.
28. The applicant is advised that the establishment is subject to the provisions of State Labor Code Section 6404.5 (ref: State Law AB 13), which prohibits smoking inside the establishment as of January 1, 1995.
29. No roof-mounted mechanical equipment shall be permitted unless a method of screening complementary to the architecture of the building is approved by the Community and Economic Development Department, Planning Division. Said screening shall block visibility of any roof-mounted mechanical equipment from view of public streets and surrounding properties.
30. The applicant shall be responsible for providing adequate parking area lighting in compliance with City regulations. Lighting in the parking area shall be directed, positioned, or shielded in such a manner so as not to unreasonably illuminate adjacent properties.
31. No satellite dish antennas shall be installed on said premises unless, and until, plans have been submitted to and approved by the Community and Economic Development Department, Planning Division. No advertising material shall be placed thereon.
32. There shall be no outdoor activities conducted on the premises without approval of a Special Event Permit or Community Event Permit.

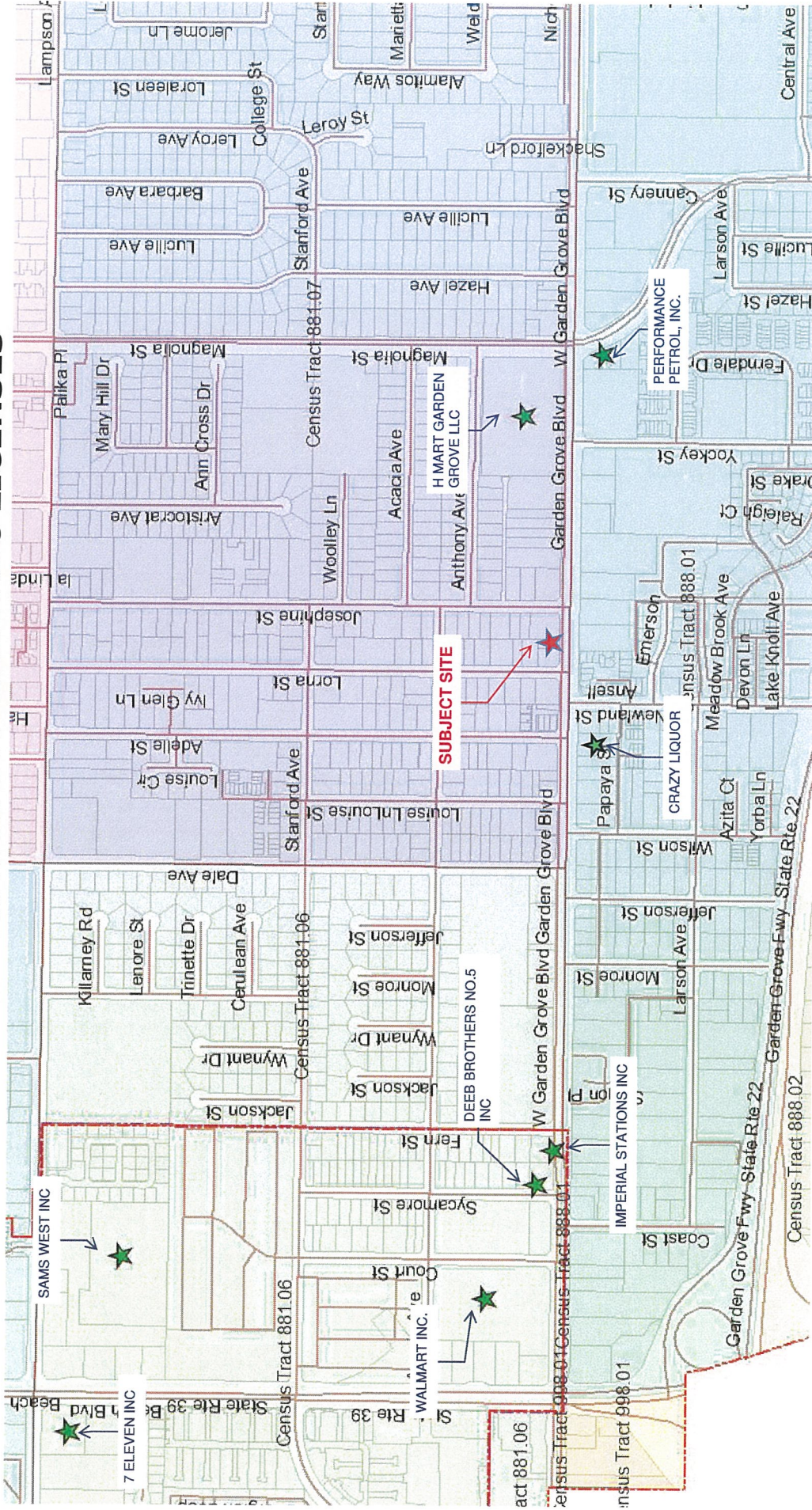
33. Permits from the City of Garden Grove shall be obtained prior to displaying any temporary advertising (i.e., banners).
34. Signs shall comply with the City of Garden Grove sign requirements. No more than 15% of the total window area and clear doors shall bear advertising or signs of any sort. No signs advertising alcoholic beverages shall be placed on the windows. Any opaque material applied to the store front, such as window tint, shall count toward the maximum window coverage area.
35. Any modifications to existing signs or the installation of new signs shall require approval by the Community and Economic Development Department, Planning Services Division prior to issuance of a building permit.
36. A copy of the decision and the conditions of approval for Conditional Use Permit No. CUP-217-2022 shall be kept on the premises at all times.
37. The permittee shall submit a signed letter acknowledging receipt of the decision approving Conditional Use Permit No. CUP-217-2022 and his/her agreement with all conditions of the approval.
38. The Conditional Use Permit may be called for review by City staff, the City Council, or Planning Commission, if noise or other complaints are filed and verified as valid by the Code Enforcement office or other City department concerning the violation of approved conditions, the Garden Grove Municipal Code, or any other applicable provisions of law.
39. If deemed necessary by the Community and Economic Development Director, the Conditional Use Permit may be reviewed within one year from the date of this approval, and every three (3) years thereafter, in order to determine if the business is operating in compliance.
40. The applicant shall, as a condition of project approval, at its sole expense, defend, indemnify and hold harmless the City, its officers, employees, agents and consultants from any claim, action, or proceeding against the City, its officers, agents, employees and/or consultants, which action seeks to set aside, void, annul or otherwise challenge any approval by the City Council, Planning Commission, or other City decision-making body, or City staff action concerning Conditional Use Permit No. CUP-217-2022. The applicant shall pay the City's defense costs, including attorney fees and all other litigation related expenses, and shall reimburse the City for court costs, which the City may be required to pay as a result of such defense. The applicant shall further pay any adverse financial award, which may issue against the City

including but not limited to any award of attorney fees to a party challenging such project approval. The City shall retain the right to select its counsel of choice in any action referred to herein.

41. Unless a time extension is granted pursuant to Section 9.32.030.D.9 of Title 9 of the Municipal Code, the use authorized by this approval of Conditional Use Permit No. CUP-217-2022 shall become null and void if the subject use or construction necessary and incidental thereto is not commenced within one (1) year of the expiration of the appeal period and thereafter diligently advanced until completion of the project.
42. The applicant is advised that if the use of the establishment ceases to operate for more than 90 days, then the existing Conditional Use Permit will become null and void and the new applicant shall be required to apply for a new Conditional Use Permit subject to the approval by the Community and Economic Development Department, Planning Services Division. In the event the use(s) authorized by the CUP cease and the property owner no longer desires to continue such use(s) on the property, property owner may voluntarily terminate the CUP and all rights and obligations thereunder by executing and recording a request for voluntary revocation and termination of the CUP in a form acceptable to the City.
43. No alcoholic beverages shall be sold until all conditions of approval have been met, as determined by the Planning Services Division, and the State Alcoholic Beverage Control Board (ABC) has approved the release of the ABC License.



# EXHIBIT A CURRENT ACTIVE OFF-SALE ABC LICENSES



# COMMUNITY AND ECONOMIC DEVELOPMENT DEPARTMENT PLANNING STAFF REPORT

<b>AGENDA ITEM NO.:</b> 1.a.	<b>SITE LOCATION:</b> North of Garden Grove Boulevard, between Lorna Street and Josephine Street, at 8725 Garden Grove Boulevard
<b>HEARING DATE:</b> March 24, 2022	<b>GENERAL PLAN:</b> Residential / Commercial Mixed Use 2
<b>CASE NO.:</b> Conditional Use Permit No. CUP-217-2022	<b>ZONE:</b> GGMU-2 (Garden Grove Mixed Use 2)
<b>APPLICANT:</b> Hedra Mamlouk	<b>CEQA DETERMINATION:</b> Exempt – Section 15301 – Existing Facilities
<b>PROPERTY OWNER:</b> Keith To	<b>APN:</b> 133-462-20

**REQUEST:**

The applicant is requesting approval of a Conditional Use Permit to allow a new convenience store, Crazy Beer & Wine Market, to operate with an original State Alcoholic Beverage Control (ABC) Type "20" (Off-Sale, Beer and Wine) License.

**BACKGROUND:**

The subject site is an existing in-line, commercial building located at 8725 Garden Grove Boulevard, between Lorna Street and Josephine Street. The site has a General Plan Land Use designation of Residential/Commercial Mixed Use 2, and is zoned GGMU-2 (Garden Grove Boulevard Mixed Use 2). The shopping center is adjacent to GGMU-2 zoned properties to the north, east, and across Lorna Street to the west; and GGMU-2 and Planned Unit Development No. PUD-131-99 zoned properties, across Garden Grove Boulevard, to the south. Although directly abutting the site are commercial properties, the site is located within a mixed neighborhood that consists of different types of uses, including retail commercial, single-family, and multifamily residential.

The specific tenant space is located on the west side of the in-line commercial building. The space is approximately 1,290 square feet in size. According to Business Tax records, the subject tenant space has been used as an office for a cargo business, Savvy N Save. However, the space is currently vacant as the cargo business has ceased its operation in May 2021. The applicant took over the subject tenant space in October 2021, and wishes to operate a new convenience store with a State Alcoholic Beverage Control (ABC) Type "20" (Off-Sale, Beer and Wine) License. The Municipal Code requires the approval of a Conditional Use Permit when there is an application for a new original ABC license. Approval of this Conditional

Use Permit will establish conditions of approval to regulate the operation of the convenience store to minimize potential impacts to the community due to the sale of alcohol. The convenience store is anticipated to be open in May 2022.

**DISCUSSION:**

The Crazy Beer & Wine Market is approximately 1,290 square feet in size. The interior of the store will consist of a cashier counter area, a sales area with coolers and display shelves, storage rooms, and a restroom.

The store will be open from 9:00 a.m. to 9:00 p.m., seven (7) days a week. In the event problems arise concerning the operation of the business, the hours of operation may be reduced by order of the Chief of the Police Department.

The convenience store is located in a high-crime district, and in an area of an under concentration of Alcoholic Beverage Control off-sale licenses. In order to approve the request, a Finding of Public Convenience or Necessity is required. A summary of the district and the Findings of Public Convenience or Necessity can be found in Decision 1814-22 for Conditional Use Permit No. CUP-217-2022.

The Community and Economic Development Department and the Police Department have reviewed the request and are supporting the proposal. All standard conditions of approval for an ABC Type "20" License will apply.

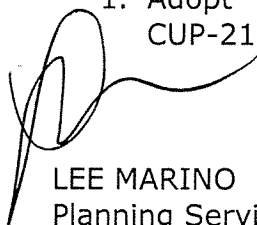
**CEQA:**

CEQA's Class 1 exemption applies to the operation, repair, maintenance, permitting, leasing, licensing, and minor alterations of existing facilities, with negligible or no expansion of use (CEQA Guidelines §15301). The subject location is an existing tenant space within an in-line commercial building. In addition, a convenience store with alcohol sales is conditionally allowed in the GGMU-2 zone. Provided the conditions of approval are adhered to for the life of the project, the use will be compatible with other adjacent uses. Therefore, the proposed project is exempt from CEQA.

**RECOMMENDATION:**

Staff recommends that the Zoning Administrator take the following action:

1. Adopt Decision No. 1814-22, approving Conditional Use Permit No. CUP-217-2022, subject to the recommended conditions of approval.



LEE MARINO  
Planning Services Manager



By: Huong Ly  
Assistant Planner

DECISION NO. 1814-22

A DECISION OF THE ZONING ADMINISTRATOR OF THE CITY OF GARDEN GROVE APPROVING CONDITIONAL USE PERMIT NO. CUP-217-2022 FOR A PROPERTY LOCATED AT 8725 GARDEN GROVE BOULEVARD.

BE IT RESOLVED that the Zoning Administrator of the City of Garden Grove does hereby approve Conditional Use Permit No. CUP-217-2022 for a property located north of Garden Grove Boulevard, between Lorna Street and Josephine Street, at 8725 Garden Grove Boulevard , Assessor's Parcel No. 133-462-20.

BE IT FURTHER RESOLVED in the matter of Conditional Use Permit No. CUP-217-2022, the Zoning Administrator of the City of Garden Grove does hereby report as follows:

1. The subject case was initiated Hedra Mamlouk for Crazy Beer & Wine Market.
2. The applicant is requesting an approval for Conditional Use Permit to operate a new convenience store, Crazy Beer & Wine Market, with a new original State Alcoholic Beverage Control ABC Type "20" (Off-sale, Beer and Wine) License.
3. The City of Garden Grove has determined that this project is exempt pursuant to Article 19, Section 15301, Existing Facilities, of the California Environmental Quality Act.
4. The property has a General Plan Land Use designation of Residential/Commercial Mixed Use 2, and is zoned GGMU-2 (Garden Grove Boulevard Mixed Use 2).
5. Existing land use, zoning, and General Plan designation of property within the vicinity of the subject property have been reviewed.
6. Report submitted by City Staff was reviewed.
7. Pursuant to a legal notice, a public hearing was held on March 24, 2022 and all interested persons were given an opportunity to be heard.
8. The Zoning Administrator gave due and careful consideration to the matter during its meeting of March 24, 2022 and

BE IT FURTHER RESOLVED, FOUND AND DETERMINED that the facts and reasons supporting the conclusion of the Zoning Administrator, as required under Municipal Code Section 9.32.030 (Conditional Use Permits), are as follows:

FACTS:

The subject site is an existing in-line, commercial building located at 8725 Garden Grove Boulevard, between Lorna Street and Josephine Street. The site has a General Plan Land Use designation of Residential/Commercial Mixed Use 2, and is zoned GGMU-2 (Garden Grove Boulevard Mixed Use 2). The shopping center is adjacent to GGMU-2 zoned properties to the north, east, and across Lorna Street to the west; and GGMU-2 and Planned Unit Development No. PUD-131-99 zoned properties, across Garden Grove Boulevard, to the south. Although directly abutting the site are commercial properties, the site is located within a mixed neighborhood that consists of different types of uses, including retail commercial, single-family and multifamily residential.

The specific tenant space is located on the west side of the in-line commercial building. The space is approximately 1,290 square feet in size. According to Business Tax records, the subject tenant space has been used as an office for a cargo business, Savvy N Save. However, the space is currently vacant as the cargo business has ceased its operation in May 2021. The applicant took over the subject tenant space in October 2021, and wishes to operate a new convenience store with a State Alcoholic Beverage Control (ABC) Type "20" (Off-Sale, Beer and Wine) License. The Municipal Code requires the approval of a Conditional Use Permit when there is an application for a new original ABC license. Approval of this Conditional Use Permit will establish conditions of approval to regulate the operation of the convenience store to minimize potential impacts to the community due to the sale of alcohol. The convenience store is anticipated to be open in May 2022.

The subject restaurant is located in a high-crime district, and in an area with an under-concentration of Alcoholic Beverage Control Off-Sale Licenses. A summary of the district is as follows:

- The subject site is located in Crime Reporting District No. 74.
- The crime count for the District is 143.
- Average crime count per district in the City is 91.
- A District is considered high when it exceeds the Citywide average by 20%.
- The subject District has a crime count of 57% above the Citywide average; therefore, it is considered a high-crime area.
- The subject site is located in Alcoholic Beverage Control Census Report District No. 881.07.
- ABC Census Reporting District No. 881.07 allows for three off-sale licenses within the District. Currently, there is only one (1) off-sale license in the District. The approval of this CUP will increase the number of off-sale licenses in District 881.07 to two (2).

PUBLIC CONVENIENCE OR NECESSITY:

A finding for public convenience or necessity would have to be made in order to approve an establishment that is requesting an original Alcoholic Beverage Control license that is located within a district with a high-crime rate and/or in an area with an over-concentration of ABC Licenses. California Business and Professions Code Section 23817.5 prohibits the ABC from issuing new alcoholic licenses in areas of over-concentration. Business and Professions Code Section 23817.7 provides the following exception:

(a) Notwithstanding Section 23817.5, the department may approve an application for an off-sale beer and wine license in areas covered by Section 23817.5, if the applicant shows that public convenience or necessity would be served by the issuance, and where all of the following conditions are found to exist:

(1) The applicant premises are located in a crime-reporting district that is below that specified pursuant to paragraph (1) of subdivision (a) of Section 23958.4.

(2) The applicant premises are located in an area that falls below the concentration level provided in paragraph (3) of subdivision (a) of Section 23958.4.

(3) The local governing body of the area in which the applicant premises are located, or its designated subordinate officer or body determines that public convenience or necessity would be served by the issuance.

Furthermore, California Business and Professions Code Section 23958 states the following:

The department further shall deny an application for a license if issuance of that license would tend to create a law enforcement problem or if issuance would result in or add to an undue concentration of licenses, except as provided in Section 23958.4

California Business and Professions Code Section 23958.4(b) provides the following exception:

(b) Notwithstanding Section 23958, the department may issue a license as follows:

(1) With respect to a nonretail license, a retail on-sale bona fide eating place license, a retail license issued for a hotel, motel, or other lodging establishment, as defined in subdivision (b) of Section 25503.16, a retail license issued in conjunction with a beer manufacturer's license, or a winegrower's license, if the applicant shows that public convenience or necessity would be served by the issuance.

(2) With respect to any other license, if the local governing body of the area in which the applicant premises are located, or its designated subordinate officer or

body, determines within 90 days of notification of a completed application that public convenience or necessity would be served by the issuance. The 90-day period shall commence upon receipt of the local governing body of (A) notification by the department of an application for licensure, or (B) a completed application according to local requirements, if any, whichever is later.

ABC Census Reporting District No. 881.07 shows the subject site to be located in an area considered to be in a high-crime district. Although the proposed convenience store is located in a high-crime area, staff is able to support a finding for public convenience or necessity given the potential community benefit through the addition of a beer and wine license. Since the area has only one active off-sale license, the Crazy Beer & Wine Market would provide an additional amenity to nearby local residents. The use will not adversely affect the health, peace, comfort or welfare of persons residing or working in the surrounding area. The conditions of approval will minimize potential impacts to the adjoining area. Provided the conditions of approval are adhered to for the life of the project, the use will be harmonious with the persons who work and live in the area.

The Community and Economic Development Department and the Police Department have reviewed the request and support the proposal. All standard conditions of approval for an ABC Type "20" License will apply.

#### FINDINGS AND REASONS:

1. That the proposed use will be consistent with the City's adopted General Plan and redevelopment plan.

The subject site has a General Plan Land Use Designation of Residential/Commercial Mixed Use 2 (RC2), and is zoned GGMU-2 (Garden Grove Boulevard Mixed Use 2). The RC2 designation is intended to allow a range of commercial activities that serve local residential neighborhoods and the larger community. In particular, the subject proposal is consistent with the following General Plan Goals, Policies, and Implementation Measures:

*Policy LU-1.3 Encourage a wide variety of retail and commercial services, such as restaurants and cultural arts / entertainment, in appropriate locations.* The subject convenience store is located in an area already developed with retail, and other commercial services. Thus, the proposed convenience store with an ABC Type "20" License at the subject location is consistent with surrounding uses.

*Goal LU-4 The City seeks to develop uses that are compatible with one another.* The subject in-line commercial building consists of different types of retail uses including a smoke shop, a glass store, a signage company, and a plant shop. Thus, the proposed convenience store is compatible with all of the existing retail uses at this location.

*Goal ED-2 The City must attract new businesses, while supporting and assisting those already located within Garden Grove.* The proposed Conditional Use Permit is intended to provide additional amenities at the new convenience store, Crazy Beer & Wine Market. Should the Conditional Use Permit be approved, the City is providing new businesses all of the resources they needed to be successful.

*SAF-IMP-2C Involve law enforcement agencies in the design and planning phases of ABC licensed establishments to reduce design elements that conceal or encourage criminal activity.* The Garden Grove Police Department has reviewed all relevant data pertaining to the proposed Conditional Use Permit. The intent of their review is to reduce the number of alcohol-related crimes, and promote the safe operation of the convenience store. No concerns were raised by the Police Department regarding the addition of an ABC License to the convenience store, and they are therefore supportive of the Conditional Use Permit request.

2. That the requested use at the location proposed will not: adversely affect the health, peace, comfort, or welfare of the persons residing or working in the surrounding area, or unreasonably interfere with the use, enjoyment, or valuation of the property of other persons located in the vicinity of the site, or jeopardize, endanger, or otherwise constitute a menace to public health, safety, or general welfare.

The use will not adversely affect the health, peace, comfort or welfare of persons residing or working in the surrounding area. The proposed convenience store with beer and wine sales will be consistent and compatible with the existing uses in the area. The proposed use complies with the intent of the GGMU-2 zone, by providing a small-scale convenience shopping facility, oriented to the neighborhood level. The proposed convenience store will be open from 9:00 a.m. to 9:00 p.m., seven (7) days a week. Standard hours of operation that apply to Type "20" Licensed convenience stores have been applied. The conditions of approval will minimize potential impacts to the adjoining area. Provided the conditions of approval are adhered to for the life of the project, the use will be harmonious with the persons who work and live in the area.

The use will not jeopardize, endanger, or otherwise constitute a menace to public health, safety, or general welfare. The Police Department has reviewed the proposal, and all appropriate conditions of approval have been incorporated. The conditions of approval will ensure public health, safety and welfare.

3. That the proposed site is adequate in size and shape to accommodate the yards, walls, fences, parking and loading facilities, landscaping and other development features prescribed in this title or as is otherwise required in



order to integrate such use with the uses in the surrounding area.

All site improvements are existing. No change to the structure is included in the proposal. The site, with the existing site improvements, is of adequate size to accommodate the proposed use within the surrounding area.

4. That the proposed site is adequately served: by highways or streets or sufficient width and improved as necessary to carry the kind and quantity of traffic such as to be generated, and by other public or private service facilities as required.

The site is located north of Garden Grove Boulevard, between Lorna Street and Josephine Street, which are fully developed streets that provide adequate traffic circulation and driveway access to public parking areas. The site is also sufficiently served by the public service facilities required, such as public utilities: gas, electric, water, and sewer facilities.

#### INCORPORATION OF FACTS AND REASONS SET FORTH IN THE STAFF REPORT

In addition to the foregoing, the Zoning Administrator incorporates herein by this reference, the facts and reasons set forth in the staff report.

BE IT FURTHER RESOLVED that the Zoning Administrator does conclude:

1. The Conditional Use Permit does possess characteristics that would indicate justification of the request in accordance with Municipal Code Section 9.32.030 (Conditional Use Permits).
2. In order to fulfill the purpose and intent of the Municipal Code and thereby promote the health, safety, and general welfare, the following Conditions of Approval, attached as Exhibit "A", shall apply to Conditional Use Permit No. CUP-217-2022.

Dated: March 24, 2022

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GREG BLODGETT  
ZONING ADMINISTRATOR

**EXHIBIT "A"**  
**Conditional Use Permit No. CUP-217-2022**

8725 Garden Grove Boulevard  
(APN: 133-462-20)

**CONDITIONS OF APPROVAL**

**General Conditions**

1. Each owner of the property shall execute, and the applicant shall record against the property, a "Notice of Discretionary Permit Approval and Agreement with Conditions of Approval," as prepared by the City Attorney's Office, within 30 days of approval. This Conditional Use Permit runs with the land and is binding upon the property owner, his/her/its heirs, assigns, and successors in interest.
2. All Conditions of Approval set forth herein shall be binding on and enforceable against each of the following, and whenever used herein, the term "applicant" shall mean and refer to each of the following: the project applicant, Hedra Mamlouk for Crazy Beer & Wine Market, the developer of the project, the owner(s) and tenants(s) of the property, and each of their respective successors and assigns. The applicant and subsequent owner/operators of such business shall adhere to the conditions of approval for the life of the project, regardless of property ownership. Any changes of the conditions of approval require approval by the applicable City hearing body, except as otherwise provided herein.
3. Approval of this Conditional Use Permit shall not be construed to mean any waiver of applicable and appropriate zoning and other regulations; and wherein not otherwise specified, all requirements of the City of Garden Grove Municipal Code shall apply.
4. This Conditional Use Permit only authorizes the operation of a 1,290 square foot convenience store with an Alcoholic Beverage Control Type "20" (Off-Sale, Beer and Wine) License as identified on the floor plan attached to these Conditions of Approval. Approval of this Conditional Use Permit shall not be construed to mean any waiver of applicable and appropriate zoning and other regulations; and wherein not otherwise specified, all requirements of the City of Garden Grove Municipal Code shall apply. Modifications, which do not change the intent of the project, may be approved by the Community and Economic Development Director.
5. Minor modifications to the approved site plan, floor plan, and/or these Conditions of Approval may be approved by the Community and Economic Development Director, in his or her discretion. Proposed modifications to the

approved floor plan, site plan, or Conditions of Approval that would result in the intensification of the project or create impacts that have not been previously addressed, and which are determined by the Community and Economic Development Director not to be minor in nature shall be subject to approval of new and/or amended land use entitlements by the applicable City hearing body.

6. All conditions of approval shall be implemented at the applicant's expense, except where specified in the individual condition.

### **Police Department**

7. There shall be no gaming tables or gaming machines, as outlined in City Code Sections 8.20.010 and 8.20.050, on the premises at any time.
8. Hours of operation shall be permitted from 9:00 a.m. to 9:00 p.m., seven (7) days a week. The hours of operation shall be applicable to the entire premises. The City of Garden Grove reserves the right to reduce hours of operation, by order of the Chief of the Police Department, in the event problems arise due to noise, disturbances or other problems that may be resolved by modifying the hours of operation.
9. There shall be no customers or patrons in or about the premises when the establishment is closed.
10. In the event security problems occur, and at the request of the Police Department, the permittee, at his own expense, shall provide a California licensed, uniformed security guard(s) on the premises during such hours as requested by the Police Department.
11. The sale of alcoholic beverages for consumption on the premises is prohibited. Consumption of alcoholic beverages shall not occur anywhere on the site or within the premises.
12. All pay phones located on the property, adjacent to the premises, shall be limited to out-going calls only. This condition shall be complied with within 30 days following approval of this application.
13. The business, prior to obtaining the Alcoholic Beverage Control (ABC) Type "20" (Off-Sale, Beer and Wine) License, shall show proof to the Police Department that all members of the business staff have completed the LEAD training (Licensee Education on Alcohol and Drugs) through the Alcoholic

Beverage Control Department or an ABC approved "Responsible Beverage Service (RBS) Training" program.

14. In the event that the Alcoholic Beverage Control (ABC) License is suspended for any period of time and/or fined for any ABC violation as a result of disciplinary action, the Conditional Use Permit shall be presented to the Planning Commission for review or further consideration.
15. Any violations or noncompliance with the conditions of approval may result in the issuance of an Administrative Citation of up to \$1,000 pursuant to GGMC 1.22.010 (a).

### **Community and Economic Development Department**

16. This approval shall allow the convenience store to operate with ancillary alcoholic beverage sales.
17. The sales area for alcoholic beverages shall not exceed 5% of the retail sales floor area of the store.
18. No outdoor storage or display shall be permitted outside of the building, including, but not limited to, storage and display of merchandise, cardboard, pallets, or boxes.
19. A prominent, permanent sign stating "NO LOITERING IS ALLOWED ON OR IN FRONT OF THE PREMISES" shall be posted in a place that is clearly visible to patrons of the licensee. The sign lettering shall be four (4) to six (6) inches high with black letters on a white background. The sign shall be displayed near or at the entrance, and shall also be visible to the public.
20. There shall be no amusement devices permitted on the premises at any time.
21. Any adult merchandise (books, magazines, videos) shall be kept under the control of the management, behind the cashier's counter, and must be segregated and screened from minors.
22. There shall be no uses or activities permitted of an adult-oriented nature as outlined in City Code Section 9.08.070.
23. There shall be no deliveries to or from the premises between the hours of 10:00 p.m. and 7:00 a.m., seven (7) days a week.

24. All rear doors shall be kept closed at all times, except to permit employee ingress and egress, and in emergencies.
25. Litter shall be removed daily from the premises, including adjacent public sidewalks, and from all parking areas under the control of the licensee. These areas shall be swept or cleaned, either mechanically or manually, on a weekly basis, to control debris.
26. All trash bins shall be kept inside the trash enclosure, and gates closed at all times, except during disposal and pick-up. Trash pick-up shall be at least three (3) times a week.
27. The applicant/property owner shall maintain all existing landscaped areas in a neat and healthy condition. Landscaping maintenance shall include pruning or removal of overgrown weeds and vegetation.
28. The applicant/property owner shall abate all graffiti vandalism within the premises. The applicant/property owner shall implement best management practices to prevent and abate graffiti vandalism within the premises throughout the life of the project, including, but not limited to, timely removal of all graffiti, the use of graffiti resistant coatings and surfaces, the installation of vegetation screening of frequent graffiti sites, and the installation of signage, lighting, and/or security cameras, as necessary. Graffiti shall be removed/eliminated by the applicant/property owner as soon as reasonably possible after it is discovered, but not later than 72 hours after discovery.
29. The applicant is advised that the establishment is subject to the provisions of State Labor Code Section 6404.5 (ref: State Law AB 13), which prohibits smoking inside the establishment as of January 1, 1995.
30. No roof-mounted mechanical equipment shall be permitted unless a method of screening complementary to the architecture of the building is approved by the Community and Economic Development Department, Planning Division. Said screening shall block visibility of any roof-mounted mechanical equipment from view of public streets and surrounding properties.
31. The applicant shall be responsible for providing adequate parking area lighting in compliance with City regulations. Lighting in the parking area shall be directed, positioned, or shielded in such a manner so as not to unreasonably illuminate adjacent properties.

32. No satellite dish antennas shall be installed on said premises unless, and until, plans have been submitted to and approved by the Community and Economic Development Department, Planning Division. No advertising material shall be placed thereon.
33. There shall be no outdoor activities conducted on the premises without approval of a Special Event Permit or Community Event Permit.
34. Permits from the City of Garden Grove shall be obtained prior to displaying any temporary advertising (i.e., banners).
35. Signs shall comply with the City of Garden Grove sign requirements. No more than 15% of the total window area and clear doors shall bear advertising or signs of any sort. No signs advertising alcoholic beverages shall be placed on the windows. Any opaque material applied to the store front, such as window tint, shall count toward the maximum window coverage area.
36. Any modifications to existing signs or the installation of new signs shall require approval by the Community and Economic Development Department, Planning Services Division prior to issuance of a building permit.
37. A copy of the decision and the conditions of approval for Conditional Use Permit No. CUP-217-2022 shall be kept on the premises at all times.
38. The permittee shall submit a signed letter acknowledging receipt of the decision approving Conditional Use Permit No. CUP-217-2022 and his/her agreement with all conditions of the approval.
39. The Conditional Use Permit may be called for review by City staff, the City Council, or Planning Commission, if noise or other complaints are filed and verified as valid by the Code Enforcement office or other City department concerning the violation of approved conditions, the Garden Grove Municipal Code, or any other applicable provisions of law.
40. If deemed necessary by the Community and Economic Development Director, the Conditional Use Permit may be reviewed within one year from the date of this approval, and every three (3) years thereafter, in order to determine if the business is operating in compliance.
41. The applicant shall, as a condition of project approval, at its sole expense, defend, indemnify and hold harmless the City, its officers, employees, agents and consultants from any claim, action, or proceeding against the City, its

officers, agents, employees and/or consultants, which action seeks to set aside, void, annul or otherwise challenge any approval by the City Council, Planning Commission, or other City decision-making body, or City staff action concerning Conditional Use Permit No. CUP-217-2022. The applicant shall pay the City's defense costs, including attorney fees and all other litigation related expenses, and shall reimburse the City for court costs, which the City may be required to pay as a result of such defense. The applicant shall further pay any adverse financial award, which may issue against the City including but not limited to any award of attorney fees to a party challenging such project approval. The City shall retain the right to select its counsel of choice in any action referred to herein.

42. Unless a time extension is granted pursuant to Section 9.32.030.D.9 of Title 9 of the Municipal Code, the use authorized by this approval of Conditional Use Permit No. CUP-217-2022 shall become null and void if the subject use or construction necessary and incidental thereto is not commenced within one (1) year of the expiration of the appeal period and thereafter diligently advanced until completion of the project.
43. The applicant is advised that if the use of the establishment ceases to operate for more than 90 days, then the existing Conditional Use Permit will become null and void and the new applicant shall be required to apply for a new Conditional Use Permit subject to the approval by the Community and Economic Development Department, Planning Services Division. In the event the use(s) authorized by the CUP cease and the property owner no longer desires to continue such use(s) on the property, property owner may voluntarily terminate the CUP and all rights and obligations thereunder by executing and recording a request for voluntary revocation and termination of the CUP in a form acceptable to the City.
44. No alcoholic beverages shall be sold until all conditions of approval have been met, as determined by the Planning Services Division, and the State Alcoholic Beverage Control Board (ABC) has approved the release of the ABC License.

**EXHIBIT C**

MINUTE EXCERPT

GARDEN GROVE ZONING ADMINISTRATOR

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PUBLIC HEARING – CONDITIONAL USE PERMIT NO. CUP-217-2022

Applicant: Hedra Mamlouk  
Location: 8725 Garden Grove Boulevard  
Date: March 24, 2022

Request: To allow a new convenience store, Crazy Beer & Wine Market, to operate with an original State Alcoholic Beverage Control (ABC) Type "20" (Off-Sale, Beer and Wine) License. The site is in the GGMU-2 (Garden Grove Boulevard Mixed Use 2) zone. In conjunction with the request, the Zoning Administrator will also consider a determination that the project is categorically exempt from the California Environmental Quality Act (CEQA) pursuant to Section 15301 – Existing Facilities – of the State CEQA Guidelines.

Action: Public Hearing Held. Speaker(s): Martina (Applicant's Representative), Maureen Blackmun, Derrick Grayer, Matthew Hickam. Though the proposed business is a market with beer and wine sales, concerns mentioned related to high crime in the area, drugs, transients, narcotics, stolen vehicles, ID theft, weapons, smoke shop clientele, signage, area already saturated with liquor availability, trespassing, sex offenders, and vandalism.

Action: Due to public concerns, the Zoning Administrator remanded Conditional Use Permit No. CUP-217-2022 to the Planning Commission. The project would be re-noticed for the next available regular meeting.



# COMMUNITY AND ECONOMIC DEVELOPMENT DEPARTMENT PLANNING STAFF REPORT

<b>AGENDA ITEM NO.:</b> C.2	<b>SITE LOCATION:</b> Northeast corner of Beverly Lane and Eneo Place, at 9801 and 9811 Beverly Lane.
<b>HEARING DATE:</b> May 19, 2022	<b>GENERAL PLAN:</b> Low Density Residential
<b>CASE NO.:</b> Site Plan No. SP-112-2022, and Tentative Parcel Map No. PM-2021-112	<b>ZONE:</b> R-1 (Single-Family Residential)
<b>APPLICANT:</b> Henry Balanza c/o HENMER CORP.	<b>CEQA DETERMINATION:</b> Exempt Section 15303 – New Construction or Conversion of Small Structures; Section 15315 – Minor Land Divisions;
<b>PROPERTY OWNER(S):</b> Same as applicant	<b>APN:</b> 133-132-01 and 133-132-02

## **REQUEST:**

The applicant is requesting Site Plan and Tentative Parcel Map approval to reconfigure two (2) existing adjacent parcels to create two (2) new lots for the purpose of constructing a new two-story single-family home on each lot. Lot 1 will have a lot size of 10,500 square feet. Lot 2 will have a lot size of 7,238 square feet. The existing single-family dwelling will be demolished to accommodate the proposed development.

## **BACKGROUND:**

The subject properties are located within a single-family residential (R-1) zoned neighborhood, at the northeast corner of Beverly Lane and Eneo Place. The subject site is comprised of two (2) adjoining Assessor Parcels: 133-132-01 and 133-132-02, under two (2) different addresses: 9801 and 9811 Beverly Lane, respectively. Both parcels have a General Plan Land Use Designation of Low Density Residential and are zoned R-1-7 (Single-Family Residential). The subject site is surrounded by properties improved with one and two-story residential dwellings.

9801 Beverly Lane, or Lot A, is a vacant, narrow strip of land located at the corner of Beverly Lane and Eneo Place. 9811 Beverly Lane, or Lot B, is an interior lot abutting Lot A to the east, with the lot fronting onto Beverly Lane. Lot B is improved with a one-story, single-family dwelling that consists of four (4) bedrooms and a detached two-car garage. The dwelling was used as an adult residential care facility until January 2022.

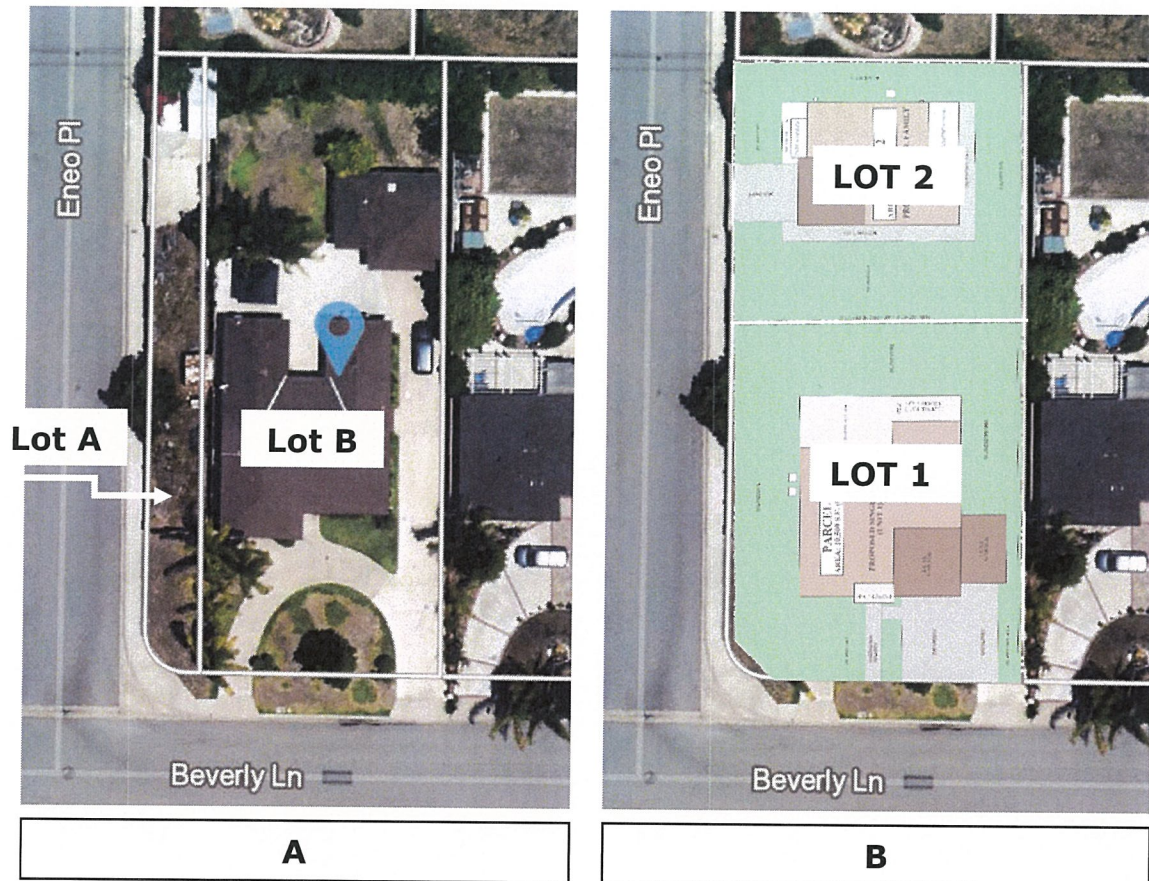
The applicant, who owns both lots, wishes to reconfigure the two (2) existing lots to create two (2) new lots for the purpose of constructing a new single-family dwelling on each lot. The existing dwelling will be demolished to facilitate the proposed development. Although the project site already consists of two (2) parcels, the proposed lot orientation will be significantly different from the existing configuration; thus, a new Parcel Map is required. Section 9.40.030 of the Municipal Code requires a Tentative Parcel Map approval whenever a parcel, or contiguous parcels, of land is/are proposed to be subdivided for the purpose of creating four or fewer lots. Section 9.32.030 of the code requires a Site Plan approval for any building to be constructed as part of a single-family subdivision. In addition, since the proposed housing project requires the demolition of an existing rental unit, Government Code §66300(d), as known as SB 330, requires the City to impose specified replacement housing and affordability requirements as conditions of approval.

**PROJECT STATISTICS:**

		<b>Provided</b>	<b>Code requirement</b>	<b>Meet Code</b>
<b>Lot size</b>				
	Lot 1	10,500 s.f.	Min. 7,200 s.f.	Yes
	Lot 2	7,238 s.f.		Yes
<b>Lot width</b>				
	Lot 1	90'-0"	65 feet (corner)	Yes
	Lot 2	80'-0"	60 feet (interior)	Yes
<b>Lot coverage</b>				
	Lot 1	48%	Max. 50%	Yes
	Lot 2	46.7%		Yes
<b>Proposed building height</b>				
	Lot 1	28'-8"	Max. 35 feet	Yes
	Lot 2	27'-5"		Yes
<b>Setbacks</b>				
<i>Front</i>				
	Lot 1	25'-0"	Min. 20 feet	Yes
	Lot 2	20'-0"		Yes
<i>Side</i>				
	Lot 1	20'-6" (street side) 5'-0" (interior side)	10 feet (Street side) 5 feet (interior side)	Yes
	Lot 2	30'-0" 12'-9"		Yes
<i>Rear (20% of lot depth, not to exceed 25 feet)</i>				
	Lot 1	22'-9"	Min. 22'-6"	Yes
	Lot 2	19'-6"	Min. 19'-6"	Yes

**DISCUSSION:**

**TENTATIVE PARCEL MAP:**



**Figure 1:** (A) The subject site consists of Lot A and Lot B. Lot A and B share an interior lot line that runs north-south. (B) The new parcel map will consist of Lot 1 and Lot 2, which have the shared lot line running in an east-west direction.

The project site consists of two (2) separate parcels that abut each other: Lot A and Lot B, as shown in Figure 1A. Lot A is a narrow corner lot with a lot area of 3,087 square feet and a lot width of fifteen (15'-0") feet. Section 9.08.040.020 of Municipal Code requires a minimum lot size of 7,200 square feet and a minimum lot width of sixty-five (65'-0") feet for any R-1-7 zoned corner property. In other words, Lot A is a legal nonconforming lot. Although Section 9.36.030 of the Municipal Code allows legal nonconforming lots to be developed upon approval of a variance, the physical characteristics of Lot 1 would make it impossible to be developed. On the other hand, Lot B is an interior lot with a total lot size of 14,400 square feet, and a lot width of seventy-five (75'-0") feet, which exceeds the required minimum lot size and lot width for R-1-7 zoned properties. The combined area of the two lots will be 17,487 square feet, which is adequate to accommodate two (2) 7,200 square foot parcels with one single-family dwelling on each lot. However, under the current map, only one (1) unit can be built at the subject location despite having two (2) separate lots.

The proposed Tentative Parcel Map is necessary since it will not only create two (2) new lots, Lot 1 and Lot 2, that conform to the R-1 zone development standards, but it will also encourage the production of one additional housing unit at the subject site. By reconfiguring the existing lots and establishing a new shared lot line, each new lot will have the capacity to accommodate one (1) new home, for a total of two (2) homes. While the existing shared lot line originally runs in a north-south direction, the new shared lot line will run in an east-west direction, as shown in Figure 1B.

Upon approval of the proposed Tentative Parcel Map, Lot 1 will be 10,500 square feet, while Lot 2 will be 7,238 square feet. Lot 1 will be located at the corner of Beverly Lane and Eneo Place with the lot fronting onto Beverly Lane. Lot 1 will have a width of 90 feet and a depth of 112 feet. Lot 2 will abut Lot 1 to the north, and will front onto Eneo Place. Lot 2 will have a lot width of 80 feet and a lot depth of 90 feet. The parcels comply with the minimum lot size of 7,200 square feet, and the minimum lot width of sixty feet (60'-0") for interior lots and sixty-five (65'-0") for corner lots. The proposed Tentative Parcel Map is in conformance with the City's General Plan, the City's Subdivision Ordinance, the R-1 zone requirements, and the State's Subdivision Map Act.

SITE PLAN:

Based on the proposed design, the existing single-family structure along with the detached garage will be demolished to leave space for a new two-story single-family residential dwelling on each new lot. The two (2) new homes are designed to meet all of the R-1 zone development standards including, but not limited to, setbacks, lot coverage, and rear open space.

Circulation and Unit Design

	<b>Number of Bedrooms</b>	<b>Number of Bathrooms</b>	<b>Total living area (garage excluded)</b>
<b>Unit 1 (Lot 1)</b>	7 bedrooms	4 bathrooms	4,344 square feet
<b>Unit 2 (Lot 2)</b>	4 Bedrooms	4 bathrooms	2,726 square feet

**Lot 1**

The two (2) existing driveway approaches along Beverly Lane will be removed and replaced with a new thirty foot (30'-0") driveway approach that allows vehicle access from the right-of-way to the property.

The new single-family dwelling on Lot 1 will be two-story with the front building oriented toward Beverly Lane. The total size of the home will be 5,325 square feet including 4,344 square feet of living area and 735 square feet of three (3) cars, in form of one (1) two-car garage and one (1) one-car garage. The living area is divided up into two floors. On the ground floor, the common area is designed with an open concept that consists of a foyer area, a great room, a kitchen, and a dining room. In addition, one (1) office, one (1) playroom, and one (1) common bathroom will be provided at the ground level. The second floor will consist of four (4)

standard bedrooms, one (1) master bedroom with one (1) private bathroom, two (2) common bathrooms, and a loft area that is open to below. The plan calls for an "office" and a "playroom" designed as wholly enclosed rooms; and, based on the City's definition of "sleeping rooms," these rooms are considered as sleeping rooms; therefore, the home is considered a seven (7) bedroom home. A covered entry and a covered patio will also be provided.

## Lot 2

The existing driveway approach along the Eneo Place will be removed and replaced with a new eighteen (18'-0") foot driveway approach that provides access from the public right-of-way to the property.

The new single-family dwelling on Lot 2 will also be two-story with the front of the building oriented toward Eneo Place. The total size of the home will be 3,377 square feet including 2,726 square feet of living area, and a 435 square feet two-car garage. Similar to the design of Lot 1, the living area of this unit also be divided into two floors. The living area on the ground floor is designed with an open concept that consists of a foyer area, a family area, a kitchen, and a dining area. The ground floor also includes one (1) master bedroom with a private bathroom, and one (1) common bathroom that is tucked underneath the stairs. The second floor will consist of an entertainment area that is open to below, two (2) common bedrooms with a jack-and-jill bathroom, and one (1) master bedroom with a private bathroom. A covered entry and a covered patio will also be provided.

### Parking

	<b>Number of Bedrooms</b>	<b>Required parking spaces</b>
<b>Unit 1 (Lot 1)</b>	7 bedrooms	6 spaces
<b>Unit 2 (Lot 2)</b>	4 Bedrooms	4 spaces

The required number of parking spaces for each new single-family home is based on the proposed number of bedrooms. Section 9.08.040.150 of the Municipal Code requires a two-car garage and two open parking spaces for a home with four (4) bedrooms, and a three-car garage and three open parking spaces for a home with seven (7) bedrooms.

Each new home adheres to these parking requirements of the Municipal Code. Lot 1 will provide a total of six (6) parking spaces in form of one (1) two-car enclosed garage, one (1) one-car enclosed garage, and three (3) tandem open parking spaces in front of the garages. Lot 2 will provide a total of four (4) parking spaces in form of one (1) two-car enclosed garage, and two (2) tandem open parking spaces, in front of the garage.

### Building Design

The single-family homes are designed in a contemporary style that is commonly found in the surrounding single-family residential neighborhood. Each unit's front

building elevation incorporates projecting and receding building masses, along with varied rooflines articulating each building's facade. The buildings architectural detailing includes the use of an entry porch, varied window shapes, and multi-pane windows to enhance the building.

The applicant has designed the units to comply with the second-story privacy provision requirements of the R-1 zone, and, at the same time, to comply with the egress provision of the California Building Code. Pursuant to the proposed Conditions of Approval, the Planning Services Division will review and approve the final design and placement of all second-story windows to ensure privacy compliance.

### CEQA

The California Environmental Quality Act (CEQA) Class 3 exemption applies to the construction and location of new, small facilities or structures (CEQA Guidelines §15303.). In urbanized areas, up to three single-family residences may be constructed or converted under this exemption (CEQA Guidelines §15303.a.). The subject request for the site plan will result in only two (2) new single-family homes. Therefore, the proposed subdivision is exempt from CEQA.

The California Environmental Quality Act (CEQA) Class 15 exemption applies to the division of property in urbanized areas zoned for residential use into four or fewer parcels (CEQA Guidelines §15315.). The division must be in conformance with the General Plan and zoning, no variances or exceptions be required, all services and access to the proposed parcels to local standards are available, the parcel was not part of a larger parcels within the previous two years, and the parcel does not have an average slope greater than 20 percent (CEQA Guidelines §15315.). The subject request provides no change to the number of subject parcels. Upon approval of the Tentative Parcel Map, the total number of newly created parcels will remain at two (2). Furthermore, the proposed Parcel Map will bring the existing nonconforming parcel into conformance to the current zoning code. In addition, the two (2) new parcels will be accessed and serviced by local infrastructure, the existing parcels have not been subdivided in the last two (2) years, and the average slope does not exceed 20%. Therefore, the proposed project is exempt from CEQA.

### SB 330 REPLACEMENT HOUSING DETERMINATION

Under Senate Bill 330 (Government Code §66300(d)), any residential rental unit that is demolished to facilitate a proposed development must be replaced, and may be subject to certain affordability limitations. In order to prevent new housing projects from displacing existing lower income rental households, SB 330 also imposes several requirements that the City must require a developer to comply with when a proposed housing development project requires the demolition of occupied or vacant "protected units". "Protected units" include residential dwelling units that are or were occupied by lower or very low income households within the past five (5) years. For that reason, the City requires applicants for housing development projects to complete a Replacement Housing Determination Form providing the information necessary for the City to determine if any "protected units" will be

demolished. Where the income category of the household in occupancy is not known, the statute provides that it shall be rebuttably presumed that lower income renter households occupied the existing units. Therefore, there is a rebuttable presumption that the existing dwelling unit is a "protected unit," and the applicant must comply with the housing and tenant protection provisions of SB 330 for the demolished unit, including: the replacement of the protected units, the relocation benefits to the occupants of protected units, and the right of first refusal for comparable and affordable units available in the new housing development.

Per City's records, the existing dwelling located at 9811 Beverly has been rented as an adult residential care facility under the name of Beverly Home. Beverly Home is a State licensed care facility that provides assisted living services for up to six (6) seniors, and/or adults with disabilities. The patients, or individuals under the care of Beverly Homes are not required to provide proof of income while residing at the facility. Beverly Home has been operating at this location since 2015. Although the actual lessee of the dwelling was not a traditional household, but a profitable business entity, the occupants of the dwelling were seniors and individuals with disabilities, who have little to no income. In order to determine whether and how the provisions of Government Code Section 66300(d) apply in this situation, City staff held several meetings with the State Department of Housing and Community Development (HCD) staff for direction and guidance related to the interpretation of "protected units". According to HCD staff with whom City staff spoke, a small residential care facility is a residential use and the focus of Section 66300(d) is on the income level of individuals or households who physically resided in the unit, not the business entity that entered into the lease. Although the subject unit is operating as a care facility, the structure remains as a single residential dwelling that was not owner-occupied; thus, the unit is subject to Section 66300(d). Therefore, if the actual occupants of the unit are low income individuals; or if the applicant cannot verify the income status of the current occupants, the unit will be deemed a "protected unit".

As a part of the submittal for the subject request, the applicant supplied the Replacement Housing Determination Form, in which the applicant indicated that the income level of the actual occupants of the existing unit is unknown. Therefore, the proposed project must provide at least one (1) "lower" income unit of comparable size (four bedrooms) on one of the two newly created parcels. The property owner is required to enter into an affordable housing regulatory agreement with the City to ensure the continued affordability of the unit pursuant to State law. The project has been conditioned as such. The Affordable Housing Agreement must be approved by the City, and recorded prior to issuance of a building permit for any structure in the housing development, and it will run with the land and bind all future owners and successors in interest.

At the time when the official request was submitted to the City, the applicant confirmed that the rental contract with the Beverly Home care facility ended at the end of January 2022. By March 31, 2022, the Beverly Home completed its moving process to another location within the City of Anaheim, at 1183 W. Chateau Avenue. The subject dwelling unit at 9811 Beverly Lane is currently vacant.

NO NET LOSS

Government Code Section 65863 requires jurisdictions to maintain adequate sites to accommodate their remaining unmet Regional Housing Needs Allocation (RHNA) by each income category at all times throughout the Housing Element planning period. A jurisdiction may not take any action to reduce a parcel's residential density unless it makes findings that the remaining sites identified in its Housing Element sites inventory can accommodate its remaining unmet RHNA by each income category or it identifies additional sites so that there is no net loss of residential unit capacity.

In addition, if a jurisdiction approves a development on a parcel identified in its Housing Element sites inventory with fewer units than shown in the Housing Element, the jurisdiction must either make findings that the Housing Element's remaining sites have sufficient capacity to accommodate the remaining unmet RHNA by each income level, or identify and make available additional adequate sites to accommodate the remaining unmet RHNA for each income category. However, a jurisdiction may not disapprove a housing development project on the basis that approval of the development would trigger the identification or zoning of additional adequate sites to accommodate the remaining RHNA.

The subject site is identified in the City's Housing Element site inventory an underutilized site with a realistic capacity to accommodate one "above moderate" housing unit. The proposed project would result in the development of two (2) new residential housing units on the site. Thus, the site is not being developed at a lower residential density than planned for, and approval of the proposed project will not result in a reduction of residential density on the site. Therefore, the City is not required to make "No Net Loss" findings pursuant to Government Code Section 65583.

**RECOMMENDATION:**

Staff recommends that the Planning Commission take the following action:

1. Adopt Resolution No. 6042-22, approving Site Plan No. SP-112-2022 and Tentative Parcel Map No. PM-2021-112, subject to the recommended Conditions of Approval.



LEE MARINO  
Planning Services Manager



By: Huong Ly  
Assistant Planner



# 9801 AND 9811 BEVERLY LANE

CITY OF GARDEN GROVE, CA

NMU  
Bixby Av (12295)

9831  
C-S  
9821

12272  
12292  
12302  
12322

12261  
12271  
12291  
12301  
12321

12251  
12271  
9672 9692 9702  
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12332 12341  
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12346 12352  
12372 12371  
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12381 12401  
12402 12421

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R-1-7  
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R-1-7  
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Lampson Ave  
12502 R-1-7 12501  
12502 R-1-7 9752

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Lampson Ave  
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12502 R-1-7 9752

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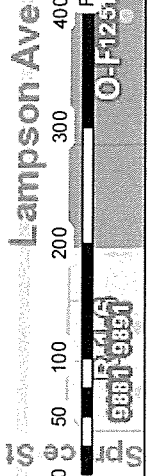
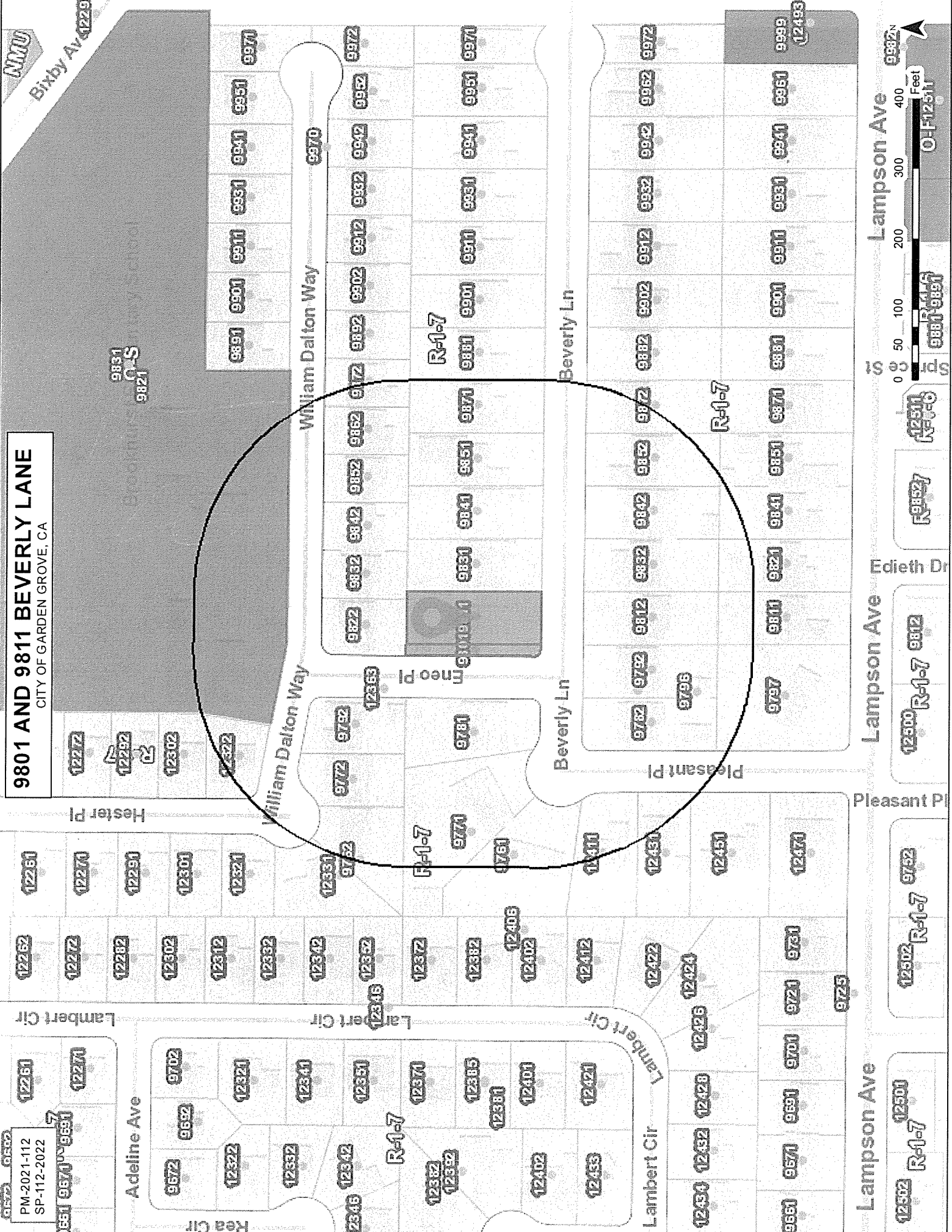
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12502 R-1-7 9752



12501 R-1-7  
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12300 R-1-7 9812  
9887  
12511 R-1-7  
9881 9891





JAA DESIGN & DRAFTING SERVICES  
 2515 W. Wilshire Blvd #4  
 Los Angeles, CA 90048  
 T 310.404.4142

PROJECT:  
**Hemmer Residence**

9811 Beverly Ln  
 Garden Grove, Ca  
 92841

REVISIONS SHALL TAKE PRECEDENCE OVER EXHIBITS AND SHALL BE REFLECTED ON THE SITE. ANY DISCREPANCY SHALL BE BROUGHT TO THE NOTICE OF THE DESIGNER AND EXHIBITS FROM THE COMPLETION OF ANY WORK.

NO.	DATE	DESCRIPTION
1	04/13/2022	City Submittal
2	04/13/2022	City Comments Submittal
3	04/13/2022	City Comments Submittal
4	04/13/2022	City Comments Submittal
5	04/13/2022	City Comments Submittal
6	04/13/2022	City Comments Submittal
7	04/13/2022	City Comments Submittal
8	04/13/2022	City Comments Submittal
9	04/13/2022	City Comments Submittal
10	04/13/2022	City Comments Submittal

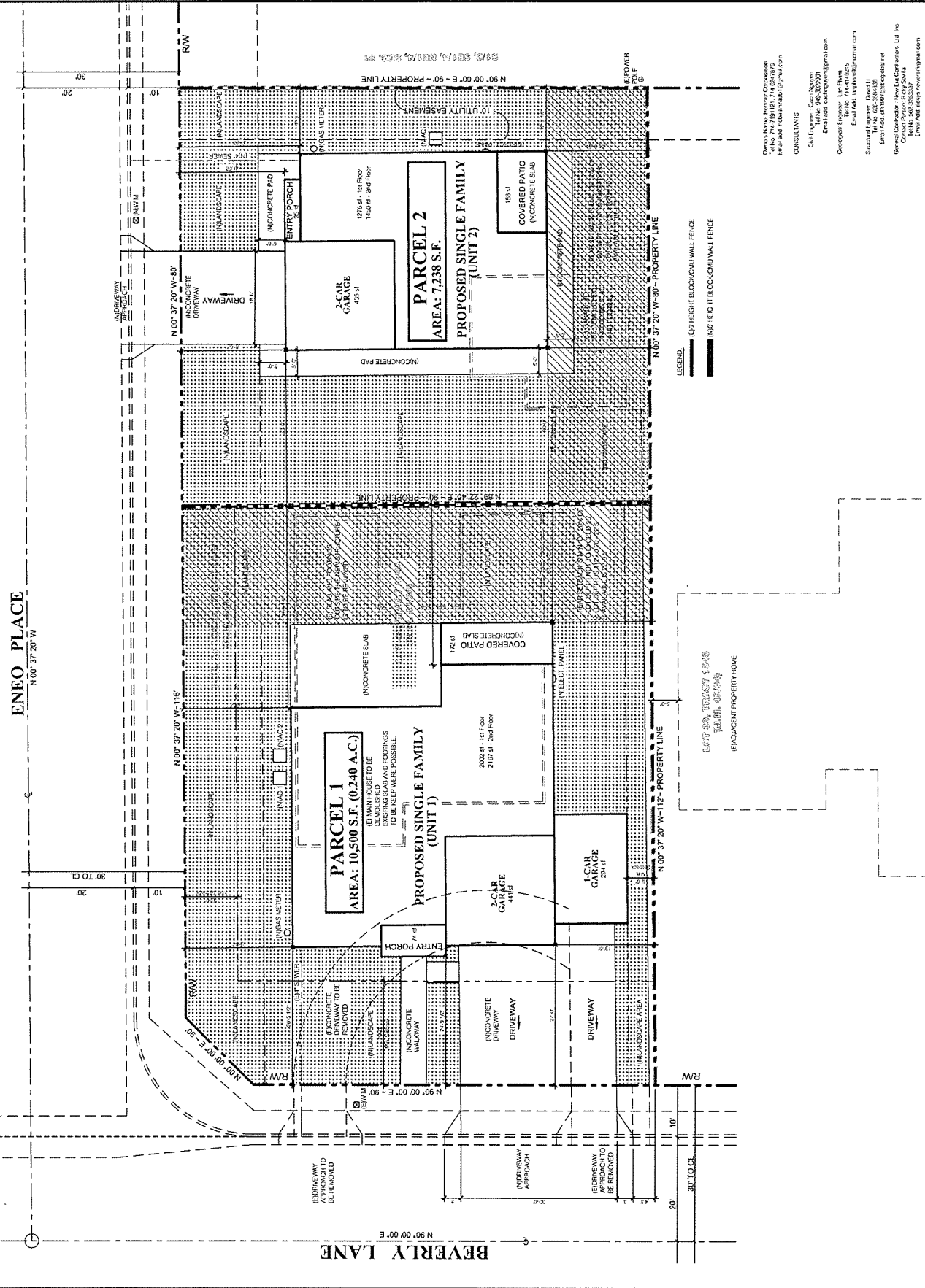
CD SET 04/13/2022

DATE	BY	DESCRIPTION
04/13/2022	JAA	ISSUED FOR PERMIT
04/13/2022	JAA	ISSUED FOR PERMIT
04/13/2022	JAA	ISSUED FOR PERMIT
04/13/2022	JAA	ISSUED FOR PERMIT
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04/13/2022	JAA	ISSUED FOR PERMIT
04/13/2022	JAA	ISSUED FOR PERMIT

SITE PLAN  
 SHEET TITLE

SHEET NO  
 1

SCALE  
 1/8" = 1'-0"



OWNER: Hemmer, Christine  
 714.780.0774  
 1000 S. WILSHIRE BLVD #4  
 LOS ANGELES, CA 90048

CONSULTANTS:  
 Civil Engineer: Cash Nguyen  
 Electrical Engineer: JAA  
 Mechanical Engineer: JAA  
 Structural Engineer: JAA  
 Foundation Engineer: JAA  
 Fire Protection Engineer: JAA  
 Sanitary Engineer: JAA  
 Surveyor: JAA  
 Title: JAA  
 Project No: 20070 Hemmer Residence  
 Sheet Title

DATE: 04/13/2022  
 DRAWN: JAA  
 CHECKED: JAA  
 PROJECT NO: 20070 Hemmer Residence  
 SHEET TITLE

SCALE  
 1/8" = 1'-0"

SHEET NO  
 1

SITE PLAN

SCALE  
 1/8" = 1'-0"

SHEET NO  
 1

SITE PLAN

SCALE  
 1/8" = 1'-0"

SHEET NO  
 1

SITE PLAN

SCALE  
 1/8" = 1'-0"

SHEET NO  
 1

SITE PLAN

SCALE  
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SHEET NO  
 1

SITE PLAN

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 1

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SHEET NO  
 1

SITE PLAN

SCALE  
 1/8" = 1'-0"

SHEET NO  
 1

SITE PLAN

SCALE  
 1/8" = 1'-0"

SHEET NO  
 1

SITE PLAN



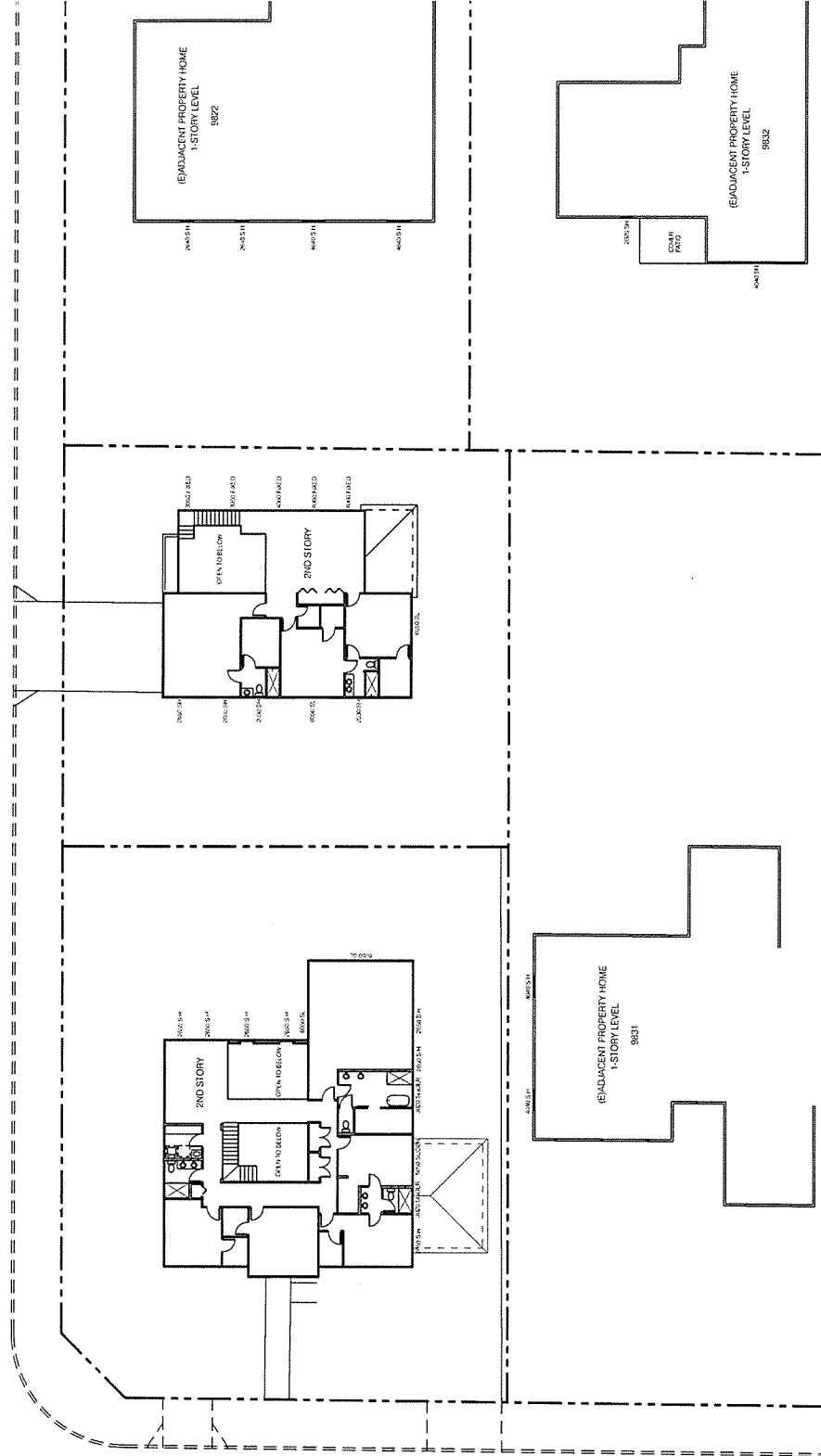
JAA DESIGN & DRAFTING SERVICES  
 2515 W. WILSON RD #4  
 FORT COCKERILL, CA 94024  
 TEL: 714.634.2142

PROJECT:  
**Henner Residence**

9811 Beverly Ln  
 Garden Grove, Ca  
 92841

**ENEQ PLACE**  
 N88°37'55"W

**BEVERLY LANE**



**REVISIONS ISSUE DATES**

NO.	DATE	DESCRIPTION
1	04/13/21	City Submittal
2	07/29/21	City Corrections Submittal
3	10/15/21	City Corrections Submittal
4	11/04/21	City Corrections Submittal
5	11/05/21	Chief Planner
6	11/05/21	City Corrections Submittal
7	01/17/22	City Submittal
8	04/13/22	City Re-Submittal

CD SET 04/13/2022

DATE	BY	DESCRIPTION
07/29/21	JAA	REVIEWED
10/15/21	JAA	REVIEWED
11/04/21	JAA	REVIEWED
11/05/21	JAA	REVIEWED
01/17/22	JAA	REVIEWED
04/13/22	JAA	REVIEWED

ADJACENT NEIGHBOR PLAN

NSP

NEIGHBORHOOD SITE PLAN

SHEET NO. 1

SCALE: 1/8" = 1'-0"



JAA DESIGN & DRAFTING SERVICES  
2515 W. Wilson Rd # 4  
San Jose, CA 95128  
415.444.1412

PROJECT  
Henner-Residence

9811 Beverly Ln  
Garden Grove, Ca  
92841

CD SET 14/09/2021

NO.	DATE	REVISION
1	04/13/21	City Submittal
2	07/27/21	City Comments Submittal
3	10/26/21	City Comments Submittal
4	11/04/21	Client Revision
5	11/09/21	City Comments Submittal

DATE: 09/26/2020  
DRAWN: JAA  
REVIEWED: JAA  
PROJECT NO: 202011-Henner-Residence  
SHEET TITLE: FLOOR PLAN

PROPOSED  
LOWER LEVEL  
FLOOR PLAN  
UNIT 1  
A1.1

**FLOOR PLAN NOTES**

1. ALL DIMENSIONS UNLESS OTHERWISE NOTED ARE IN FEET AND INCHES (ROUNDED UP TO THE NEXT 1/8").

2. FINISHES AND MATERIALS TO BE DETERMINED BY THE ARCHITECT AT THE TIME OF THE PERMIT APPLICATION.

3. ALL WORK SHALL BE IN ACCORDANCE WITH THE 2019 CALIFORNIA BUILDING CODE (CBC) AND ALL APPLICABLE LOCAL ORDINANCES.

4. ALL WORK SHALL BE IN ACCORDANCE WITH THE 2019 CALIFORNIA ELECTRICAL CODE (CEC) AND ALL APPLICABLE LOCAL ORDINANCES.

5. ALL WORK SHALL BE IN ACCORDANCE WITH THE 2019 CALIFORNIA MECHANICAL CODE (CMC) AND ALL APPLICABLE LOCAL ORDINANCES.

6. ALL WORK SHALL BE IN ACCORDANCE WITH THE 2019 CALIFORNIA PLUMBING CODE (CPC) AND ALL APPLICABLE LOCAL ORDINANCES.

7. ALL WORK SHALL BE IN ACCORDANCE WITH THE 2019 CALIFORNIA FIRE CODE (FC) AND ALL APPLICABLE LOCAL ORDINANCES.

8. ALL WORK SHALL BE IN ACCORDANCE WITH THE 2019 CALIFORNIA SAFETY CODE (SC) AND ALL APPLICABLE LOCAL ORDINANCES.

9. ALL WORK SHALL BE IN ACCORDANCE WITH THE 2019 CALIFORNIA ENERGY CODE (CEC) AND ALL APPLICABLE LOCAL ORDINANCES.

10. ALL WORK SHALL BE IN ACCORDANCE WITH THE 2019 CALIFORNIA ENVIRONMENTAL CODE (EC) AND ALL APPLICABLE LOCAL ORDINANCES.

11. ALL WORK SHALL BE IN ACCORDANCE WITH THE 2019 CALIFORNIA LAND USE CODE (LUC) AND ALL APPLICABLE LOCAL ORDINANCES.

12. ALL WORK SHALL BE IN ACCORDANCE WITH THE 2019 CALIFORNIA ZONING CODE (ZC) AND ALL APPLICABLE LOCAL ORDINANCES.

13. ALL WORK SHALL BE IN ACCORDANCE WITH THE 2019 CALIFORNIA PLANNING AND ZONING ACT (PZA) AND ALL APPLICABLE LOCAL ORDINANCES.

14. ALL WORK SHALL BE IN ACCORDANCE WITH THE 2019 CALIFORNIA DEVELOPMENTAL SERVICES ACT (DSA) AND ALL APPLICABLE LOCAL ORDINANCES.

15. ALL WORK SHALL BE IN ACCORDANCE WITH THE 2019 CALIFORNIA HOUSING ACT (HA) AND ALL APPLICABLE LOCAL ORDINANCES.

16. ALL WORK SHALL BE IN ACCORDANCE WITH THE 2019 CALIFORNIA COMMUNITY DEVELOPMENT ACT (CDA) AND ALL APPLICABLE LOCAL ORDINANCES.

17. ALL WORK SHALL BE IN ACCORDANCE WITH THE 2019 CALIFORNIA PUBLIC UTILITIES ACT (PUA) AND ALL APPLICABLE LOCAL ORDINANCES.

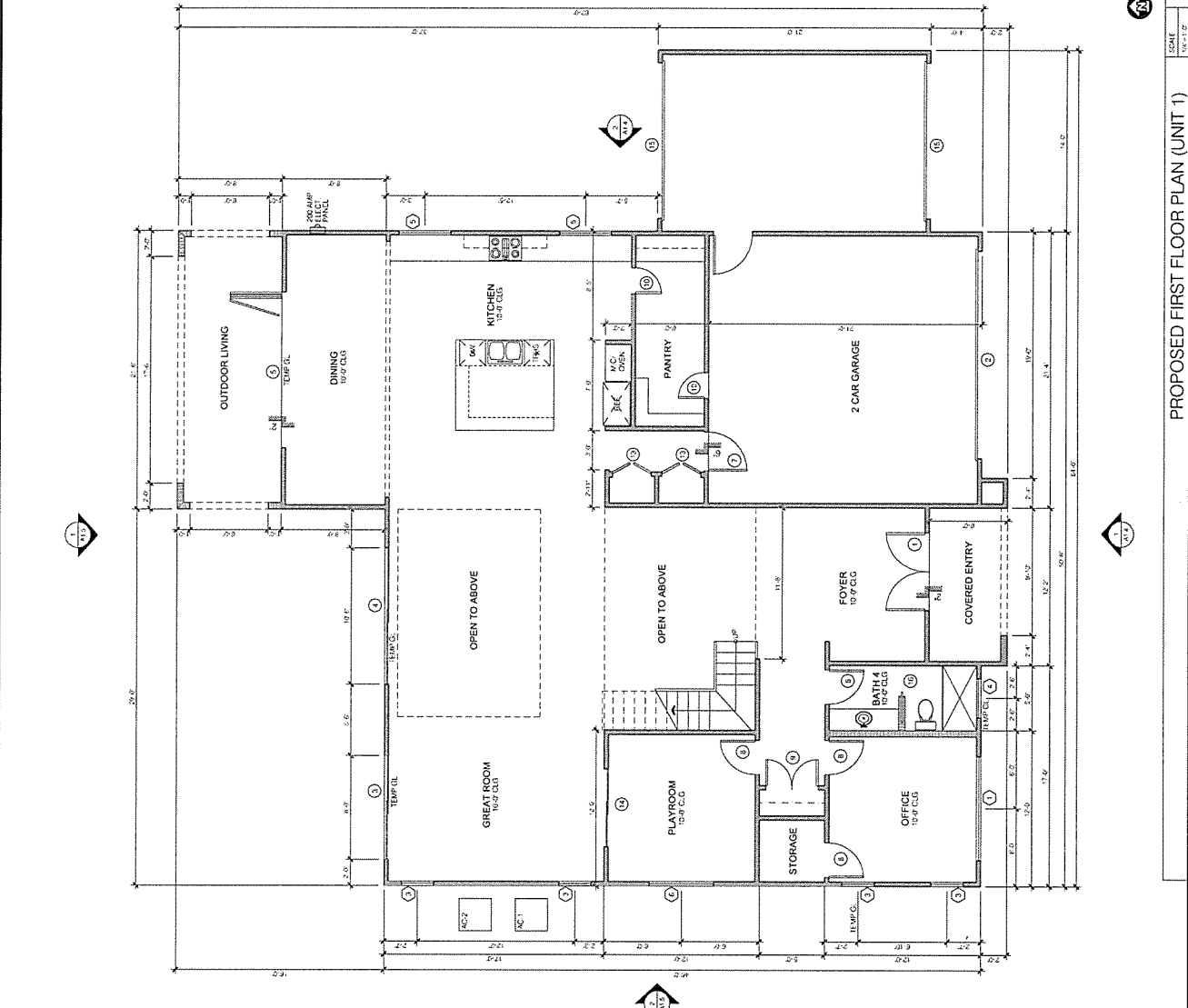
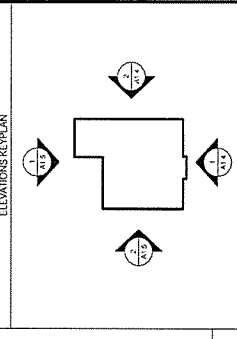
18. ALL WORK SHALL BE IN ACCORDANCE WITH THE 2019 CALIFORNIA WATER RESOURCES ACT (WRA) AND ALL APPLICABLE LOCAL ORDINANCES.

19. ALL WORK SHALL BE IN ACCORDANCE WITH THE 2019 CALIFORNIA AIR RESOURCES ACT (ARA) AND ALL APPLICABLE LOCAL ORDINANCES.

20. ALL WORK SHALL BE IN ACCORDANCE WITH THE 2019 CALIFORNIA CLIMATE CHANGE ACT (CCA) AND ALL APPLICABLE LOCAL ORDINANCES.

- FLOOR PLAN LEGEND**
1. 2x10 SIPS ON 24" MAX WALKS AT 16" O.C. MAX UNDO BY STRUCTURAL PLANS
2. 2x10 SIPS ON 24" MAX WALKS AT 16" O.C. MAX UNDO BY STRUCTURAL PLANS
3. 2x10 SIPS ON 24" MAX WALKS AT 16" O.C. MAX UNDO BY STRUCTURAL PLANS
4. 2x10 SIPS ON 24" MAX WALKS AT 16" O.C. MAX UNDO BY STRUCTURAL PLANS
5. 2x10 SIPS ON 24" MAX WALKS AT 16" O.C. MAX UNDO BY STRUCTURAL PLANS
6. 2x10 SIPS ON 24" MAX WALKS AT 16" O.C. MAX UNDO BY STRUCTURAL PLANS
7. 2x10 SIPS ON 24" MAX WALKS AT 16" O.C. MAX UNDO BY STRUCTURAL PLANS
8. 2x10 SIPS ON 24" MAX WALKS AT 16" O.C. MAX UNDO BY STRUCTURAL PLANS
9. 2x10 SIPS ON 24" MAX WALKS AT 16" O.C. MAX UNDO BY STRUCTURAL PLANS
10. 2x10 SIPS ON 24" MAX WALKS AT 16" O.C. MAX UNDO BY STRUCTURAL PLANS
11. 2x10 SIPS ON 24" MAX WALKS AT 16" O.C. MAX UNDO BY STRUCTURAL PLANS
12. 2x10 SIPS ON 24" MAX WALKS AT 16" O.C. MAX UNDO BY STRUCTURAL PLANS
13. 2x10 SIPS ON 24" MAX WALKS AT 16" O.C. MAX UNDO BY STRUCTURAL PLANS
14. 2x10 SIPS ON 24" MAX WALKS AT 16" O.C. MAX UNDO BY STRUCTURAL PLANS
15. 2x10 SIPS ON 24" MAX WALKS AT 16" O.C. MAX UNDO BY STRUCTURAL PLANS
16. 2x10 SIPS ON 24" MAX WALKS AT 16" O.C. MAX UNDO BY STRUCTURAL PLANS
17. 2x10 SIPS ON 24" MAX WALKS AT 16" O.C. MAX UNDO BY STRUCTURAL PLANS
18. 2x10 SIPS ON 24" MAX WALKS AT 16" O.C. MAX UNDO BY STRUCTURAL PLANS
19. 2x10 SIPS ON 24" MAX WALKS AT 16" O.C. MAX UNDO BY STRUCTURAL PLANS
20. 2x10 SIPS ON 24" MAX WALKS AT 16" O.C. MAX UNDO BY STRUCTURAL PLANS

Room	Area (sq ft)
Living Room	10,500
Dining Room	2,000
Kitchen	2,000
Pantry	500
Bath	500
Office	500
Playroom	500
Storage	500
Foyer	500
Garage	10,000
Outdoor Living	1,000
Outdoor Dining	1,000
Open to Above	10,000
Open to Below	10,000
Unfinished Area	5,000
Total Area	65,500
Total Building Area	75,500
Total Lot Area	100,000
Lot Coverage %	75.5%
Banking to Lot Edge	48%



**DOOR AND WINDOW SCHEDULE**

NO.	DESCRIPTION
1	6'0" x 8'0" SLIDING GLASS DOOR
2	6'0" x 8'0" SLIDING GLASS DOOR
3	6'0" x 8'0" SLIDING GLASS DOOR
4	6'0" x 8'0" SLIDING GLASS DOOR
5	6'0" x 8'0" SLIDING GLASS DOOR
6	6'0" x 8'0" SLIDING GLASS DOOR
7	6'0" x 8'0" SLIDING GLASS DOOR
8	6'0" x 8'0" SLIDING GLASS DOOR
9	6'0" x 8'0" SLIDING GLASS DOOR
10	6'0" x 8'0" SLIDING GLASS DOOR
11	6'0" x 8'0" SLIDING GLASS DOOR
12	6'0" x 8'0" SLIDING GLASS DOOR
13	6'0" x 8'0" SLIDING GLASS DOOR
14	6'0" x 8'0" SLIDING GLASS DOOR
15	6'0" x 8'0" SLIDING GLASS DOOR
16	6'0" x 8'0" SLIDING GLASS DOOR
17	6'0" x 8'0" SLIDING GLASS DOOR
18	6'0" x 8'0" SLIDING GLASS DOOR
19	6'0" x 8'0" SLIDING GLASS DOOR
20	6'0" x 8'0" SLIDING GLASS DOOR

PROPOSED FIRST FLOOR PLAN (UNIT 1)

SCALE: 1/8" = 1'-0"

1

DATE: 09/26/2020

PROJECT NO: 202011-Henner-Residence

SHEET TITLE: FLOOR PLAN

UNIT 1

A1.1



JAA DESIGN & DRAFTING SERVICES  
2915 W. Wilshire Rd #4  
Los Angeles, CA 90024  
714.401.6142

PROJECT  
**Hemmer Residence**

9811 Beverly Ln  
Garden Grove, Ca  
92841

CD SET 11/09/2021

NO.	DATE	DESCRIPTION
1	07/20/21	City Submittal
2	07/20/21	City Corrections Submittal
3	07/20/21	City Corrections Submittal
4	11/04/21	City Corrections Submittal
5	11/04/21	City Corrections Submittal
6	11/04/21	City Corrections Submittal

DATE	BY	REVISION
07/20/2021	JAA	JAA
07/20/2021	JAA	JAA
11/04/2021	JAA	JAA

PROPOSED  
SECOND LEVEL  
FLOOR PLAN  
SHEET NO. UNIT 1  
A1.2

**FLOOR PLAN NOTES**

1. ALL DIMENSIONS UNLESS OTHERWISE NOTED ARE TO FACE UNLESS INDICATED OTHERWISE.
2. FINISH FLOOR SHALL BE 1/2" THICK POLISHED CONCRETE OVER 1" THICK UNREINFORCED CONCRETE ON GRADE.
3. FINISH FLOOR SHALL BE 1/2" THICK POLISHED CONCRETE OVER 1" THICK UNREINFORCED CONCRETE ON GRADE.
4. FINISH FLOOR SHALL BE 1/2" THICK POLISHED CONCRETE OVER 1" THICK UNREINFORCED CONCRETE ON GRADE.
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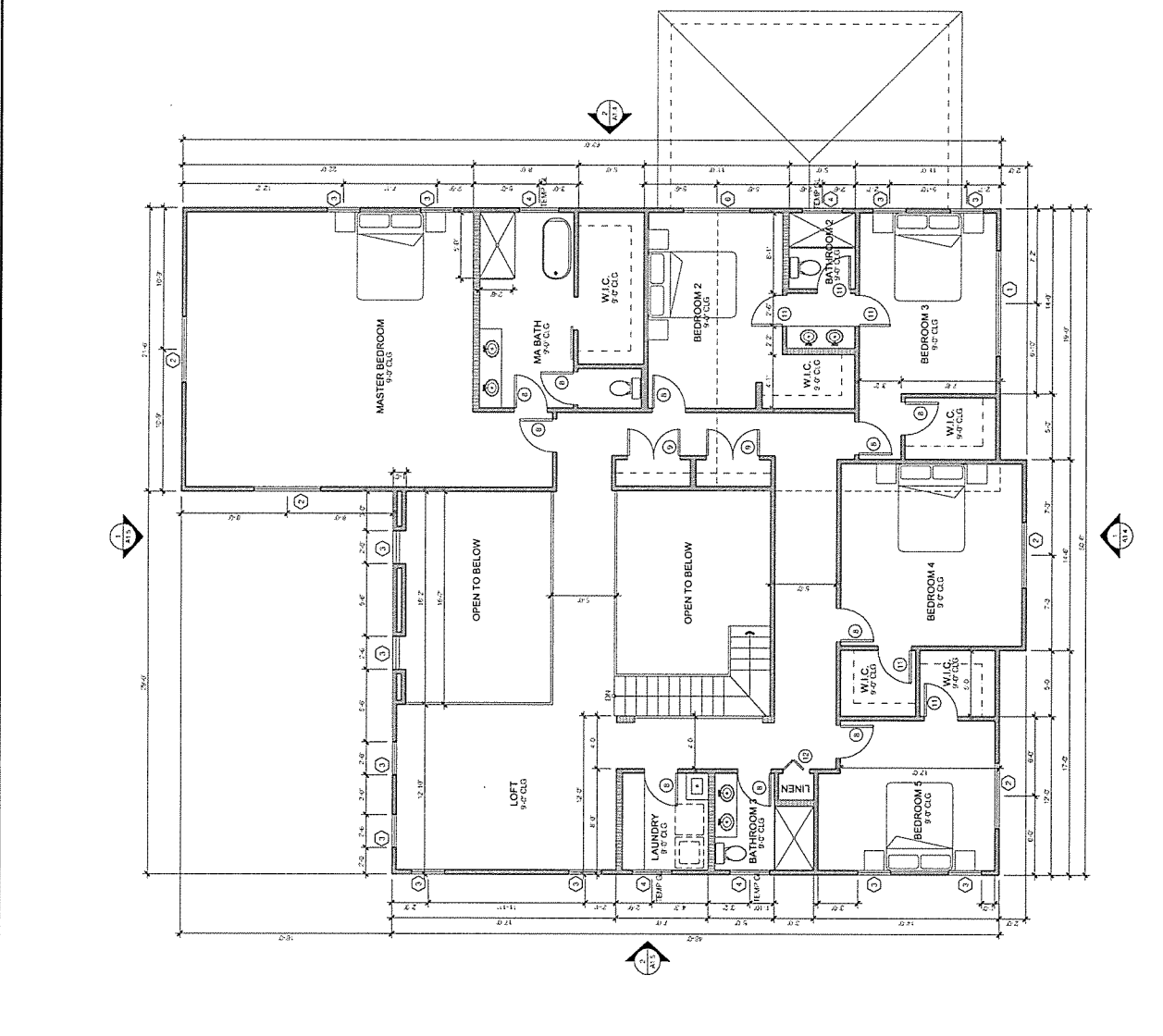
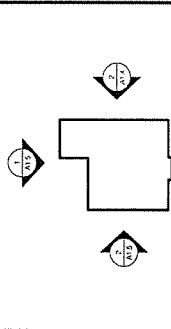
**FLOOR PLAN KEYNOTES**

1. FINISH FLOOR SHALL BE 1/2" THICK POLISHED CONCRETE OVER 1" THICK UNREINFORCED CONCRETE ON GRADE.
2. FINISH FLOOR SHALL BE 1/2" THICK POLISHED CONCRETE OVER 1" THICK UNREINFORCED CONCRETE ON GRADE.
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15. FINISH FLOOR SHALL BE 1/2" THICK POLISHED CONCRETE OVER 1" THICK UNREINFORCED CONCRETE ON GRADE.

**FLOOR PLAN LEGEND**

24" X 36" HALLS AT 15' O.C. MARKING BY STRUCTURAL PLANS  
20" X 36" HALLS AT 15' O.C. MARKING BY STRUCTURAL PLANS

Symbol	Description
[Symbol]	24" X 36" HALLS AT 15' O.C. MARKING BY STRUCTURAL PLANS
[Symbol]	20" X 36" HALLS AT 15' O.C. MARKING BY STRUCTURAL PLANS



PROPOSED SECOND FLOOR PLAN (UNIT 1) SHEET 1 OF 1

**DOOR AND WINDOW SCHEDULE**

NO.	DESCRIPTION	QTY
1	INTERIOR DOOR	1
2	INTERIOR WINDOW	1
3	INTERIOR WINDOW	1
4	INTERIOR WINDOW	1
5	INTERIOR WINDOW	1
6	INTERIOR WINDOW	1
7	INTERIOR WINDOW	1
8	INTERIOR WINDOW	1
9	INTERIOR WINDOW	1
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97	INTERIOR WINDOW	1
98	INTERIOR WINDOW	1
99	INTERIOR WINDOW	1
100	INTERIOR WINDOW	1



JAA DESIGN & DRAFTING SERVICES  
 2215 W. Wilson Rd 4  
 San Jose, CA 95128  
 TEL: 415.434.5342

PROJECT:  
**Henmer Residence**

9811 Beverly Ln  
 Garden Grove, Ca  
 92841

NO.	DATE	DESCRIPTION
1	04/03/21	City Submittal
2	07/01/21	City Construction Submittal
3	09/15/21	City Construction Submittal
4	11/04/21	City Construction Submittal
5	11/03/21	Client Revision
6	11/03/21	City Construction Submittal

CD SET 11/09/2021  
 SHEET NO. 11/09/2021  
 JAA  
 PRODUCT NO. 3002 Beverly Residence  
 SHEET TITLE

PROPOSED  
 ROOF PLAN  
 UNIT 1  
 A1.3

**TYPICAL ROOF MATERIAL**  
 MATERIAL: 30 YEAR MINIMUM LIFE EXPECTED SERVICE LIFE  
 THE LEADERSHIP IN GREEN BUILDING (LEED) GREEN BUILDRATING  
 SYSTEMS (LEED) GREEN BUILDRATING SYSTEMS (LEED) GREEN BUILDRATING  
 RETARDANT ROOF COVERING OF CLASS A MIN.

**INSTALLATION:** (SEE NOTES) (SEE NOTES) (SEE NOTES) (SEE NOTES)  
 (SEE NOTES) (SEE NOTES) (SEE NOTES) (SEE NOTES)  
 (SEE NOTES) (SEE NOTES) (SEE NOTES) (SEE NOTES)  
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**DECK:** (SEE NOTES) (SEE NOTES) (SEE NOTES) (SEE NOTES)  
 (SEE NOTES) (SEE NOTES) (SEE NOTES) (SEE NOTES)  
 (SEE NOTES) (SEE NOTES) (SEE NOTES) (SEE NOTES)  
 (SEE NOTES) (SEE NOTES) (SEE NOTES) (SEE NOTES)

**GUTTERS:** (SEE NOTES) (SEE NOTES) (SEE NOTES) (SEE NOTES)  
 (SEE NOTES) (SEE NOTES) (SEE NOTES) (SEE NOTES)  
 (SEE NOTES) (SEE NOTES) (SEE NOTES) (SEE NOTES)  
 (SEE NOTES) (SEE NOTES) (SEE NOTES) (SEE NOTES)

**DOWNSPOUTS:** (SEE NOTES) (SEE NOTES) (SEE NOTES) (SEE NOTES)  
 (SEE NOTES) (SEE NOTES) (SEE NOTES) (SEE NOTES)  
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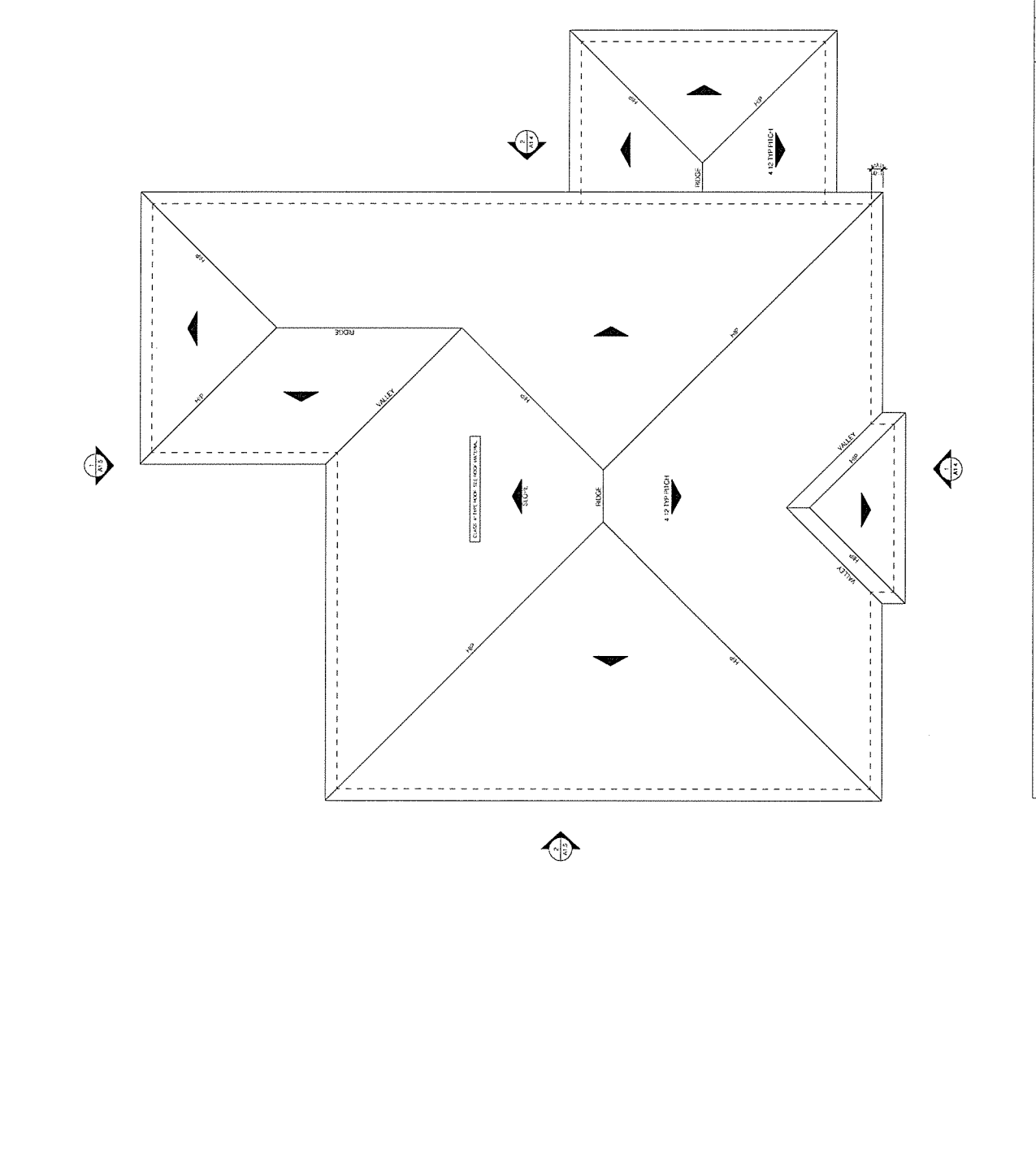
**FASCIA:** (SEE NOTES) (SEE NOTES) (SEE NOTES) (SEE NOTES)  
 (SEE NOTES) (SEE NOTES) (SEE NOTES) (SEE NOTES)  
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**WOODLAME:** (SEE NOTES) (SEE NOTES) (SEE NOTES) (SEE NOTES)  
 (SEE NOTES) (SEE NOTES) (SEE NOTES) (SEE NOTES)  
 (SEE NOTES) (SEE NOTES) (SEE NOTES) (SEE NOTES)  
 (SEE NOTES) (SEE NOTES) (SEE NOTES) (SEE NOTES)

**ROOF PLAN KEYNOTES**

**ROOF PLAN LEGEND**

**ELEVATIONS KEY PLAN**



PROPOSED ROOF PLAN (UNIT 1) DATE: 11/09/21

PERMISSION TO REPRODUCE THIS DRAWING IS GRANTED BY THE ARCHITECT FOR THE ARCHITECT'S CLIENT. THIS PERMISSION IS LIMITED TO THE PROJECT AND SITE SPECIFICALLY IDENTIFIED HEREIN. ANY OTHER REPRODUCTION OR TRANSMISSION OF THIS DRAWING WITHOUT THE WRITTEN PERMISSION OF THE ARCHITECT IS PROHIBITED. THE ARCHITECT ASSUMES NO LIABILITY FOR ANY DAMAGE OR INJURY TO PERSONS OR PROPERTY ARISING FROM THE USE OF THIS DRAWING. THE ARCHITECT'S LIABILITY IS LIMITED TO THE PROFESSIONAL SERVICES RENDERED BY THE ARCHITECT. THE ARCHITECT DOES NOT WARRANT THE ACCURACY OF ANY INFORMATION PROVIDED BY THE CLIENT. THE ARCHITECT'S LIABILITY IS LIMITED TO THE PROFESSIONAL SERVICES RENDERED BY THE ARCHITECT. THE ARCHITECT DOES NOT WARRANT THE ACCURACY OF ANY INFORMATION PROVIDED BY THE CLIENT.



JAA DESIGN & DRAFTING SERVICES  
 2515 W. Wilson Rd #4  
 Pasadena, CA 91104  
 T 714.451.5142

PROJECT:  
**Henmer Residence**

9811 Beverly Ln  
 Garden Grove, Ca  
 92741

NO.	DATE	DESCRIPTION
1	04/13/21	City Submittal
2	07/01/21	City Corrections Submittal
3	08/19/21	City Corrections Submittal
4	11/04/21	City Corrections Submittal
5	11/05/21	Client Revision
6	11/09/21	City Corrections Submittal

CD SET 11/09/2021  
 SHEET TITLE  
 PROPOSED ELEVATIONS  
 UNIT 1  
 A1.4

**ELEVATION NOTES**

CONCRETE ROOFING THE OVERHANG UNDERLAMENT. INSTALL 1/4" THICK ROOFING FLASHING AT ALL ROOF PLANE CHANGES.

WOOD FRAME  
 PROVIDE WOOD FRAME AROUND ALL WALL PENETRATIONS INCLUDING WINDOWS AND DOORS. PROVIDE 2" MINIMUM OVERLAP WITH EXISTING WALLS.

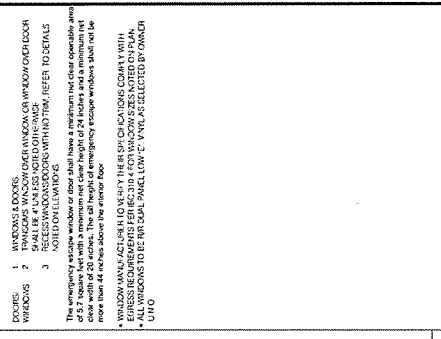
PAINTED 3/4" GYP BOARD OVER 1/2" THICK THREE COAT PORTLAND CEMENT PLASTER. PROVIDE 2 LAYERS OF GABRIEL TAPE AT JUNCTIONS.

EXTERIOR WALL CONSTRUCTION ASSEMBLY WITH A MINIMUM OF ONE 1/2" THICK GYP BOARD OVER 1/2" THICK THREE COAT PORTLAND CEMENT PLASTER WITH AN OVERLAP OF 1/2". ALL WALLS SHALL BE APPLIED OVER STUDS OF ALL EXTERIOR WALLS. TWO LAYERS OF GABRIEL TAPE SHALL BE APPLIED TO ALL EXTERIOR WALLS. PROVIDE 2" MINIMUM OVERLAP WITH EXISTING WALLS.

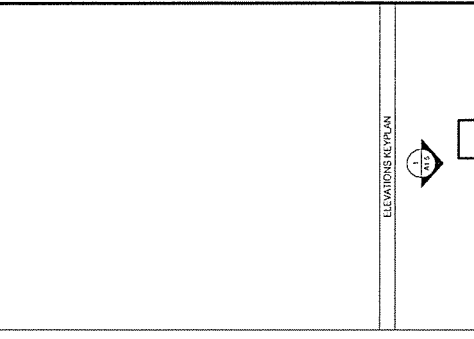
WOODS AND DOORS  
 1. WINDOWS AND DOORS  
 2. FINISHES: WINDOWS OVER WINDOW OR WINDOW OVER DOOR  
 3. RECESS WINDOWS AND DOORS WITH NOTCHES. REFER TO DETAILS

NOTED ON ELEVATIONS  
 The emergency escape window or door shall have a minimum net clear opening area of 5.7 sq ft. The height of emergency escape windows shall not be more than 44 inches above the interior floor.

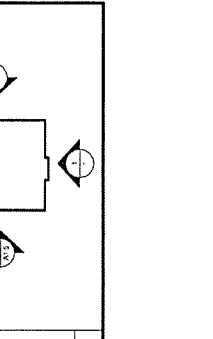
WINDOW MANUFACTURER TO VERIFY THEIR SPECIFICATIONS COMPLY WITH ALL LOCAL AND STATE CODES. ALL WINDOWS TO BE PERMANENTLY LOW E. WINDOW AS SELECTED BY OWNER U110



PROPOSED NORTH ELEVATION (UNIT 1)  
 SCALE: 1/8" = 1'-0"



PROPOSED EAST ELEVATION (UNIT 1)  
 SCALE: 1/8" = 1'-0"



PROPOSED SOUTH ELEVATION (UNIT 1)  
 SCALE: 1/8" = 1'-0"

REVISIONS ISSUE DATES  
 DATE DESCRIPTION  
 04/13/21 City Submittal  
 07/01/21 City Corrections Submittal  
 08/19/21 City Corrections Submittal  
 11/04/21 City Corrections Submittal  
 11/05/21 Client Revision  
 11/09/21 City Corrections Submittal

DATE: 07/01/2021  
 DRAWN: JAA  
 REVIEWED: JAA  
 PROJECT NO: 20217 Henmer Residence  
 SHEET TITLE  
 PROPOSED ELEVATIONS  
 UNIT 1  
 A1.4

DATE: 07/01/2021  
 DRAWN: JAA  
 REVIEWED: JAA  
 PROJECT NO: 20217 Henmer Residence  
 SHEET TITLE  
 PROPOSED ELEVATIONS  
 UNIT 1  
 A1.4





JAA DESIGN & DRAFTING SERVICES  
2935 W. WILSON RD #4  
FAYETTEVILLE, AR 72715  
717.441.5142

PROJECT:  
**Henmer Residence**

9811 Beverly Ln  
Garden Grove, Ca  
92641

NO.	DATE	DESCRIPTION
1	8/17/21	City Submittal
2	8/17/21	City Correction Submittal
3	8/17/21	City Correction Submittal
4	11/04/21	City Correction Submittal
5	11/05/21	Client Revision
6	11/05/21	City Correction Submittal

DATE	DRAWN	PROJECT NO.	PROJECT TITLE
8/17/21	JAA	2020 Henmer Residence	

PROPOSED ELEVATIONS  
SHEET NO  
UNIT 1  
A1.5

**ELEVATION NOTES**

ROOFING: CONCRETE REINFORCING THE EXISTING ROOF UNDERLAMENT. INSTALL 1/4" THICK ROOFING FLASHING AT ROOF FLASHING MATERIAL.

FASCIA: WOOD FASCIA

VAPOR BARRIER: PROTECT VAPOR BARRIER AROUND ALL WALL PENETRATIONS INCLUDING DOORS, WINDOWS AND ROOF FLASHINGS.

EXTERIOR WALLS: PAINTED A COAT (EXTERIOR FACER) OF THREE COAT PORTLAND EMULSION GROUT OVER EXISTING GROUT AND OVER EXISTING GROUT. PROVIDE 2 LAYERS OF GROUT OVER EXISTING WALLS. EXTERIOR WALL CONSTRUCTION ASSEMBLY WITH A MINIMUM OF ONE (1) INCH OF GROUT OVER EXISTING WALLS. PROVIDE 2 LAYERS OF GROUT OVER EXISTING WALLS WITH A MINIMUM OF ONE (1) INCH OF GROUT OVER EXISTING WALLS. PROVIDE 2 LAYERS OF GROUT OVER EXISTING WALLS WITH A MINIMUM OF ONE (1) INCH OF GROUT OVER EXISTING WALLS. PROVIDE 2 LAYERS OF GROUT OVER EXISTING WALLS WITH A MINIMUM OF ONE (1) INCH OF GROUT OVER EXISTING WALLS.

DOORS: 1. WINDOWS & DOORS  
2. WINDOWS WITH OVER WINDOW OR WINDOW OVER DOOR  
3. WINDOWS WITH OVER WINDOW OR WINDOW OVER DOOR

WINDOWS: 1. WINDOWS & DOORS  
2. WINDOWS WITH OVER WINDOW OR WINDOW OVER DOOR  
3. WINDOWS WITH OVER WINDOW OR WINDOW OVER DOOR

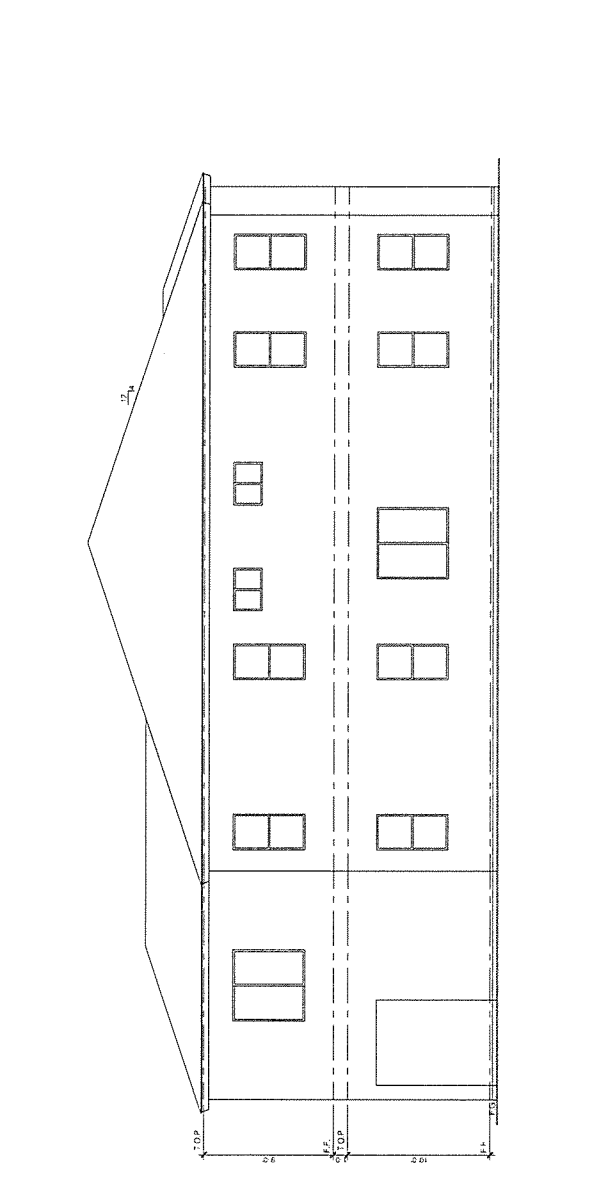
NOTED ON ELEVATION: 1. SEE WINDOW SCHEDULE FOR WINDOW SIZES REFER TO DETAILS

The emergency escape window or door shall have a minimum net clearable area of 5.7 square feet. The sill height of emergency escape windows shall not be more than 44 inches above the interior floor.

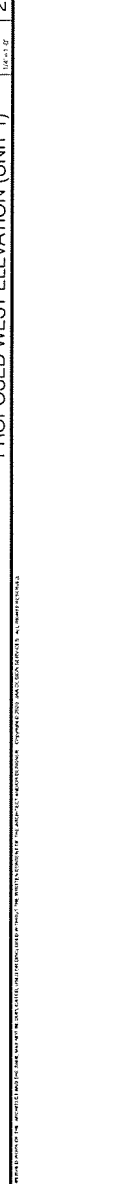
WINDOWS SHALL BE IN FULL PANEL LOW E. WINDOW AS SELECTED BY OWNER.



PROPOSED SOUTH ELEVATION (UNIT 1)



PROPOSED WEST ELEVATION (UNIT 1)



PLAN: C:\P\2021\JAA 027 821\TY RESIDENCE - 1 - 000011.DWG DATE: 11/05/21 11:45 AM DRAWN BY: JAA CHECKED BY: JAA PROJECT NO.: 2020 Henmer Residence SHEET NO.: A1.5 UNIT: 1





JAA DESIGN & DRAFTING SERVICES  
2915 W. Winston Blvd #4  
Aurora, IL 60018  
708.481.9197

PROJECT:  
**Hemmer Residence**

9811 Beverly Ln  
Garden Grove, Ca  
92841

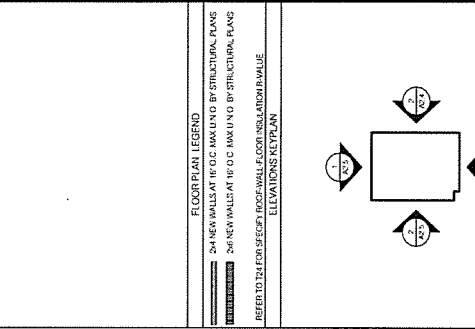
CD SET 04/13/2022

NO.	DATE	DESCRIPTION
1	04/13/21	City Submittal
2	07/01/21	City Comments Submittal
3	09/01/21	City Comments Submittal
4	11/04/21	City Comments Submittal
5	11/09/21	Client Revision
6	11/09/21	City Comments Submittal
7	01/17/22	City Submittal
8	04/13/22	City Re-Submittal

PROPOSED  
LOWER LEVEL  
FLOOR PLAN  
UNIT 2  
SHEET NO. A2.1

- FLOOR PLAN NOTES**
1. ALL FINISHES TO BE IDENTICAL TO FINISHES SHOWN ON THE GENERAL NOTES.
  2. ALL FINISHES TO BE IDENTICAL TO FINISHES SHOWN ON THE GENERAL NOTES.
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  21. ALL FINISHES TO BE IDENTICAL TO FINISHES SHOWN ON THE GENERAL NOTES.
  22. ALL FINISHES TO BE IDENTICAL TO FINISHES SHOWN ON THE GENERAL NOTES.

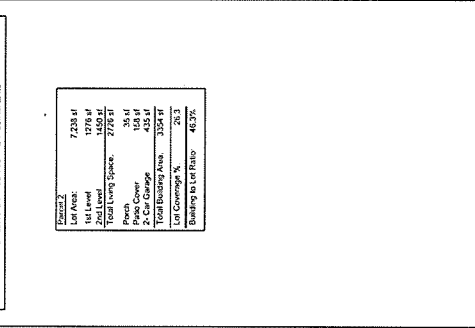
- ADDITIONAL ELOOR PLAN NOTES**
1. ALL FINISHES TO BE IDENTICAL TO FINISHES SHOWN ON THE GENERAL NOTES.
  2. ALL FINISHES TO BE IDENTICAL TO FINISHES SHOWN ON THE GENERAL NOTES.
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  22. ALL FINISHES TO BE IDENTICAL TO FINISHES SHOWN ON THE GENERAL NOTES.



- FLOOR PLAN NOTES**
1. ALL FINISHES TO BE IDENTICAL TO FINISHES SHOWN ON THE GENERAL NOTES.
  2. ALL FINISHES TO BE IDENTICAL TO FINISHES SHOWN ON THE GENERAL NOTES.
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  22. ALL FINISHES TO BE IDENTICAL TO FINISHES SHOWN ON THE GENERAL NOTES.

**DOOR AND WINDOW SCHEDULE**

NO.	DESCRIPTION	QTY	FINISH
1	600 X 2100 SLIP DOOR	1	WOOD GRAB
2	3000 X 7800 WINDOW	1	WOOD GRAB
3	3000 X 7800 WINDOW	1	WOOD GRAB
4	3000 X 7800 WINDOW	1	WOOD GRAB
5	3000 X 7800 WINDOW	1	WOOD GRAB
6	3000 X 7800 WINDOW	1	WOOD GRAB
7	3000 X 7800 WINDOW	1	WOOD GRAB
8	3000 X 7800 WINDOW	1	WOOD GRAB
9	3000 X 7800 WINDOW	1	WOOD GRAB
10	3000 X 7800 WINDOW	1	WOOD GRAB
11	3000 X 7800 WINDOW	1	WOOD GRAB
12	3000 X 7800 WINDOW	1	WOOD GRAB
13	3000 X 7800 WINDOW	1	WOOD GRAB
14	3000 X 7800 WINDOW	1	WOOD GRAB
15	3000 X 7800 WINDOW	1	WOOD GRAB
16	3000 X 7800 WINDOW	1	WOOD GRAB
17	3000 X 7800 WINDOW	1	WOOD GRAB
18	3000 X 7800 WINDOW	1	WOOD GRAB
19	3000 X 7800 WINDOW	1	WOOD GRAB
20	3000 X 7800 WINDOW	1	WOOD GRAB
21	3000 X 7800 WINDOW	1	WOOD GRAB
22	3000 X 7800 WINDOW	1	WOOD GRAB



PROPOSED FIRST FLOOR PLAN (UNIT 2)  
SCALE: 1/8" = 1'-0"  
SHEET NO. A2.1





JAA DESIGN & DRAFTING SERVICES  
 2515 W. WASHINGTON BLVD  
 ANAHEIM, CA 92801  
 714.631.6142

PROJECT:  
**Henmer Residence**

9811 Beverly Ln  
 Garden Grove, Ca  
 92841

NO.	DATE	REVISION
1	04/13/2022	City Submission
2	07/21/21	City Correction Submittal
3	10/19/21	City Correction Submittal
4	11/04/21	City Correction Submittal
5	11/09/21	Client Revision
6	11/09/21	City Correction Submittal
7	01/17/22	City Submission
8	04/13/22	City Re-Submission

DATE: 04/13/2022  
 DRAWN: JAA  
 REVIEWED: JAA  
 PROJECT NO.: 20021 Henmer Residence  
 SHEET TITLE:  
 PROPOSED ROOF PLAN

SHEET NO.: UNIT 2  
 A2.3

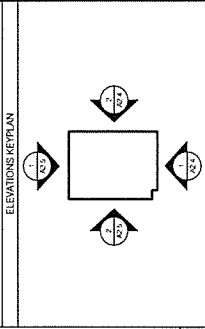
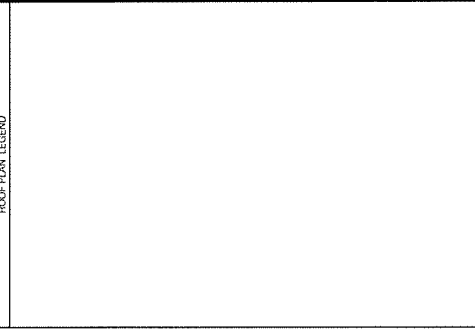
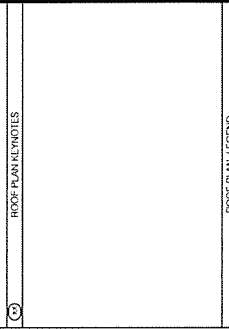
**TYPICAL ROOF MATERIAL**  
 MAKE AND MODEL TO BE DETERMINED BY THE CONTRACTOR. MAKE SURE TO OBTAIN APPROVED MATERIALS FROM THE ARCHITECT'S APPROVED SUPPLIER LIST. A ROOFING CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL JURISDICTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL JURISDICTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL JURISDICTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL JURISDICTION.

**DECK**  
 MAKE AND MODEL TO BE DETERMINED BY THE CONTRACTOR. MAKE SURE TO OBTAIN APPROVED MATERIALS FROM THE ARCHITECT'S APPROVED SUPPLIER LIST. A ROOFING CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL JURISDICTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL JURISDICTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL JURISDICTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL JURISDICTION.

**GUTTERS**  
 MAKE AND MODEL TO BE DETERMINED BY THE CONTRACTOR. MAKE SURE TO OBTAIN APPROVED MATERIALS FROM THE ARCHITECT'S APPROVED SUPPLIER LIST. A ROOFING CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL JURISDICTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL JURISDICTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL JURISDICTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL JURISDICTION.

**DOWNSPOUTS**  
 MAKE AND MODEL TO BE DETERMINED BY THE CONTRACTOR. MAKE SURE TO OBTAIN APPROVED MATERIALS FROM THE ARCHITECT'S APPROVED SUPPLIER LIST. A ROOFING CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL JURISDICTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL JURISDICTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL JURISDICTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL JURISDICTION.

**FASCIA**  
 MAKE AND MODEL TO BE DETERMINED BY THE CONTRACTOR. MAKE SURE TO OBTAIN APPROVED MATERIALS FROM THE ARCHITECT'S APPROVED SUPPLIER LIST. A ROOFING CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL JURISDICTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL JURISDICTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL JURISDICTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL JURISDICTION.





JAA DESIGN & DRAFTING SERVICES  
 2915 W. Wilson Rd 4  
 Anaheim, CA 92805  
 T 714.804.3147

PROJECT:  
**Henmer Residence**  
 9811 Beverly Ln  
 Garden Grove, Ca  
 92841

CD SET 04/13/2022

NO.	DATE	REVISION
1	04/13/21	City Submittal
2	07/21/21	City Corrections Submittal
3	11/04/21	City Corrections Submittal
4	11/05/21	Client Revision
5	11/09/21	City Corrections Submittal
6	01/17/22	City Submittal
7	04/13/22	City Re-Submittal

PROPOSED  
 ELEVATIONS  
 UNIT 2  
 A2.4

**ELEVATION NOTES**

ROOFING: CONCRETE ROOFING IS OVER ROOF UNDERLAMENT. INSTALL PER MANUFACTURER'S SET POINTS WITH 1/4" SLACK.

FASGIA: WOOD GRAIN.

WINDOR: PROTECT WINDOR BARRIERS AROUND ALL WALL EXTERIORS.

WINDOR BARRIERS: FLOOR TO CEILING WINDORS AND WINDOWS - 1/4" SLACK.

EXTERIOR WALLS: EXTERIOR WINDOR BARRIERS AND WINDOR BARRIERS SHALL BE INSTALLED OVER THREE COATS OF GYPSUM BOARD WITH THREE COATS OF GYPSUM BOARD OVER SHEET WALLS.

EXTERIOR WALL CONSTRUCTION ASSEMBLY WITH A MINIMUM OF ONE COAT OF GYPSUM BOARD OVER SHEET WALLS.

CONCRETE SHALL BE APPLIED OVER SHEET WALLS.

SLURR OF ALL EXTERIOR WALLS TWO LAYERS OF GYPSUM BOARD OR GYPSUM BOARD OVER SHEET WALLS SHALL BE APPLIED OVER WOOD FRAME SHEATHING.

DOORS: 1. UNFINISHED DOORS

WINDOWS: 2. FINISHED WINDOW OVER WINDOW OVER DOOR

3. ACCESS TO DOORS WITH NO TRAIL REFER TO DETAILS NOTED ON ELEVATION

The emergency escape window or door shall have a minimum net clear opening area of 5.7 sq. ft. for windows and 20 sq. ft. for doors. The net height of emergency escape windows shall not be more than 44 inches above the interior floor.

- Window manufacturer to verify their specifications comply with egress requirements per the IBC for window sizes noted on plan.
- All windows shall be labeled with a "2008" vinyl as indicated by owner.
- The lead resistance of glass under uniform load shall be determined in accordance with ASTM E 1308.

STONE: ADHESIVE STONE APPLIED OVER BRICK COAT. MANUFACTURER: EL DORADO STONE. INSTALL PER MANUFACTURER'S SPECIFICATIONS. E.D. K131091715.

VENTILATION: SEE MECHANICAL DRAWINGS.

**ELEVATION NOTES**

1. DOOR: WOOD GRAIN. FASGIA: WOOD GRAIN. PROTECT WINDOR BARRIERS AROUND ALL WALL EXTERIORS. FLOOR TO CEILING WINDORS AND WINDOWS - 1/4" SLACK.

2. WINDOW: PROTECT WINDOR BARRIERS AROUND ALL WALL EXTERIORS. FLOOR TO CEILING WINDORS AND WINDOWS - 1/4" SLACK.

3. WINDOW: PROTECT WINDOR BARRIERS AROUND ALL WALL EXTERIORS. FLOOR TO CEILING WINDORS AND WINDOWS - 1/4" SLACK.

4. WINDOW: PROTECT WINDOR BARRIERS AROUND ALL WALL EXTERIORS. FLOOR TO CEILING WINDORS AND WINDOWS - 1/4" SLACK.

5. WINDOW: PROTECT WINDOR BARRIERS AROUND ALL WALL EXTERIORS. FLOOR TO CEILING WINDORS AND WINDOWS - 1/4" SLACK.

6. WINDOW: PROTECT WINDOR BARRIERS AROUND ALL WALL EXTERIORS. FLOOR TO CEILING WINDORS AND WINDOWS - 1/4" SLACK.

7. WINDOW: PROTECT WINDOR BARRIERS AROUND ALL WALL EXTERIORS. FLOOR TO CEILING WINDORS AND WINDOWS - 1/4" SLACK.

8. WINDOW: PROTECT WINDOR BARRIERS AROUND ALL WALL EXTERIORS. FLOOR TO CEILING WINDORS AND WINDOWS - 1/4" SLACK.

9. WINDOW: PROTECT WINDOR BARRIERS AROUND ALL WALL EXTERIORS. FLOOR TO CEILING WINDORS AND WINDOWS - 1/4" SLACK.

10. WINDOW: PROTECT WINDOR BARRIERS AROUND ALL WALL EXTERIORS. FLOOR TO CEILING WINDORS AND WINDOWS - 1/4" SLACK.

11. WINDOW: PROTECT WINDOR BARRIERS AROUND ALL WALL EXTERIORS. FLOOR TO CEILING WINDORS AND WINDOWS - 1/4" SLACK.

12. WINDOW: PROTECT WINDOR BARRIERS AROUND ALL WALL EXTERIORS. FLOOR TO CEILING WINDORS AND WINDOWS - 1/4" SLACK.

13. WINDOW: PROTECT WINDOR BARRIERS AROUND ALL WALL EXTERIORS. FLOOR TO CEILING WINDORS AND WINDOWS - 1/4" SLACK.

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15. WINDOW: PROTECT WINDOR BARRIERS AROUND ALL WALL EXTERIORS. FLOOR TO CEILING WINDORS AND WINDOWS - 1/4" SLACK.

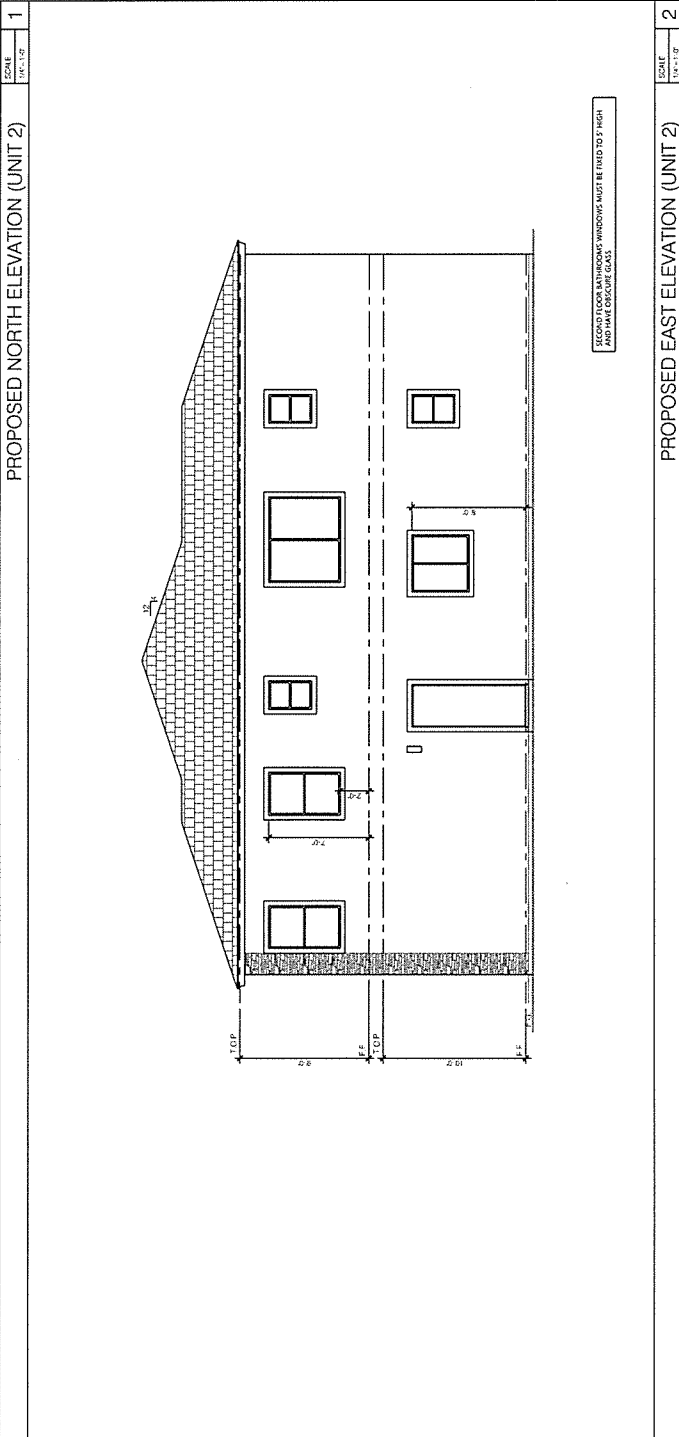
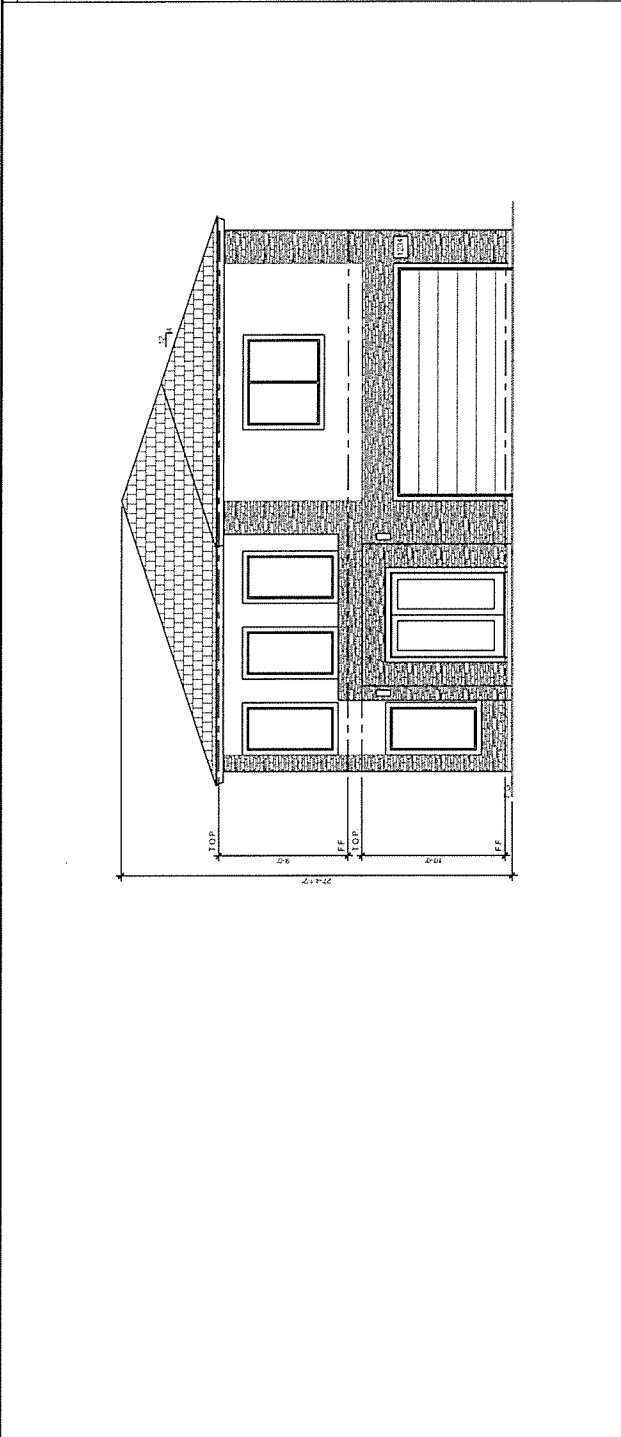
16. WINDOW: PROTECT WINDOR BARRIERS AROUND ALL WALL EXTERIORS. FLOOR TO CEILING WINDORS AND WINDOWS - 1/4" SLACK.

17. WINDOW: PROTECT WINDOR BARRIERS AROUND ALL WALL EXTERIORS. FLOOR TO CEILING WINDORS AND WINDOWS - 1/4" SLACK.

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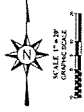






**LEGEND & ABBREVIATION:**

- T.W. TOP OF WALL
- T.C. TOP OF CURB
- T.G. TOP OF GRADE INLET
- D.V. DRAINAGE PIPE INVERT ELEVATION
- F.L. FINISH GRADE (CALIFORNIA STATE AREA)
- F.S. FINISH GRADE (FEDERAL AREA)
- F.G. FINISH GRADE (GDM AREA)
- B.F. BUILDING FINISHED FLOOR
- C.L. CENTERLINE
- P.L. PROPERTY LINE
- B.W. RIGHT-OF-WAY
- U.S. DRAINAGE
- 4529 PROPOSED ELEVATION
- OVERFLOW DRAIN/SLOPE
- PROPERTY LINE
- EXISTING CURB & GUTTER WALL
- EXISTING SIDEWALK
- DRAINAGE DIRECTION
- W/VALVE
- W/VALVE

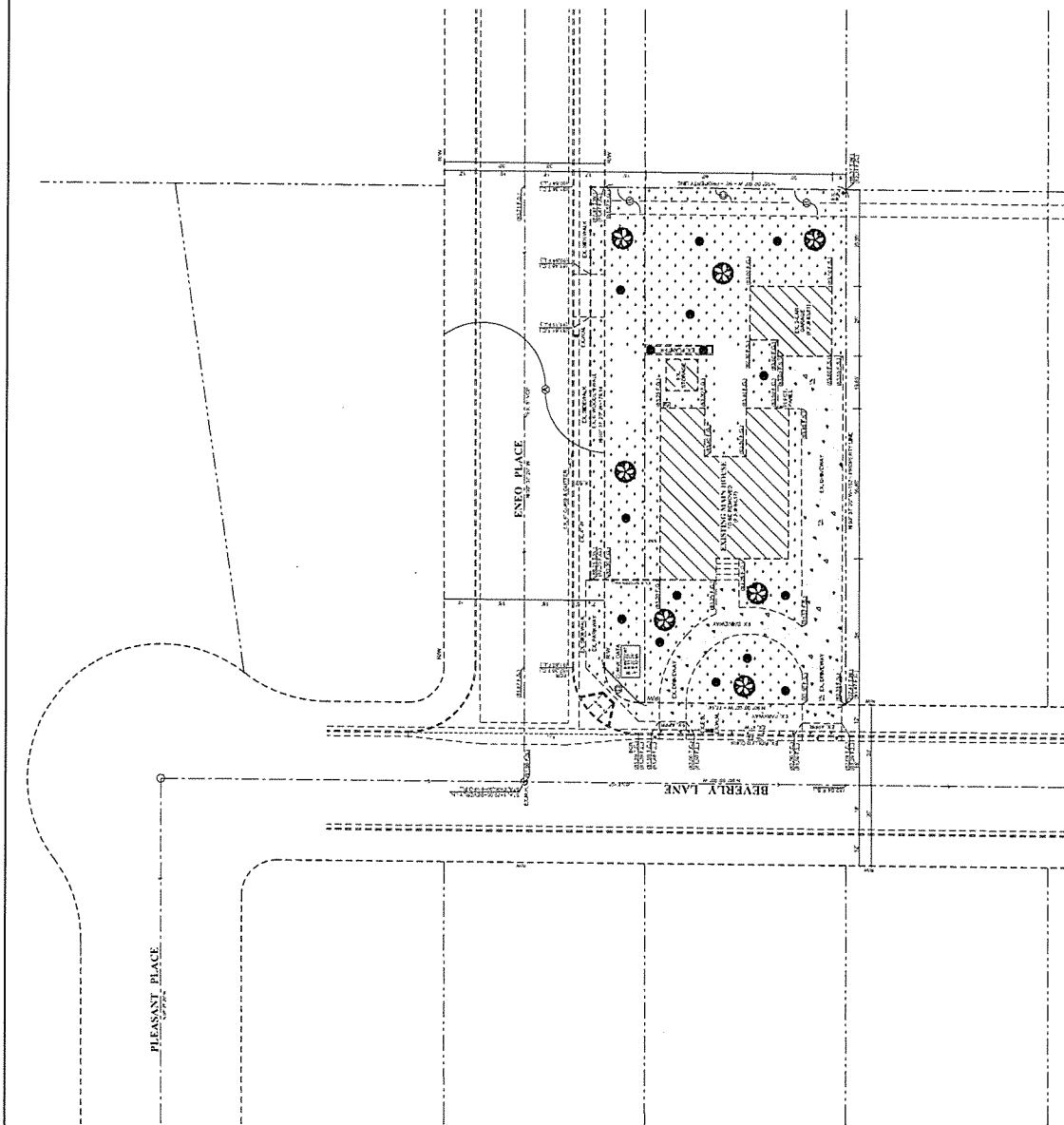


**EASEMENTS:**

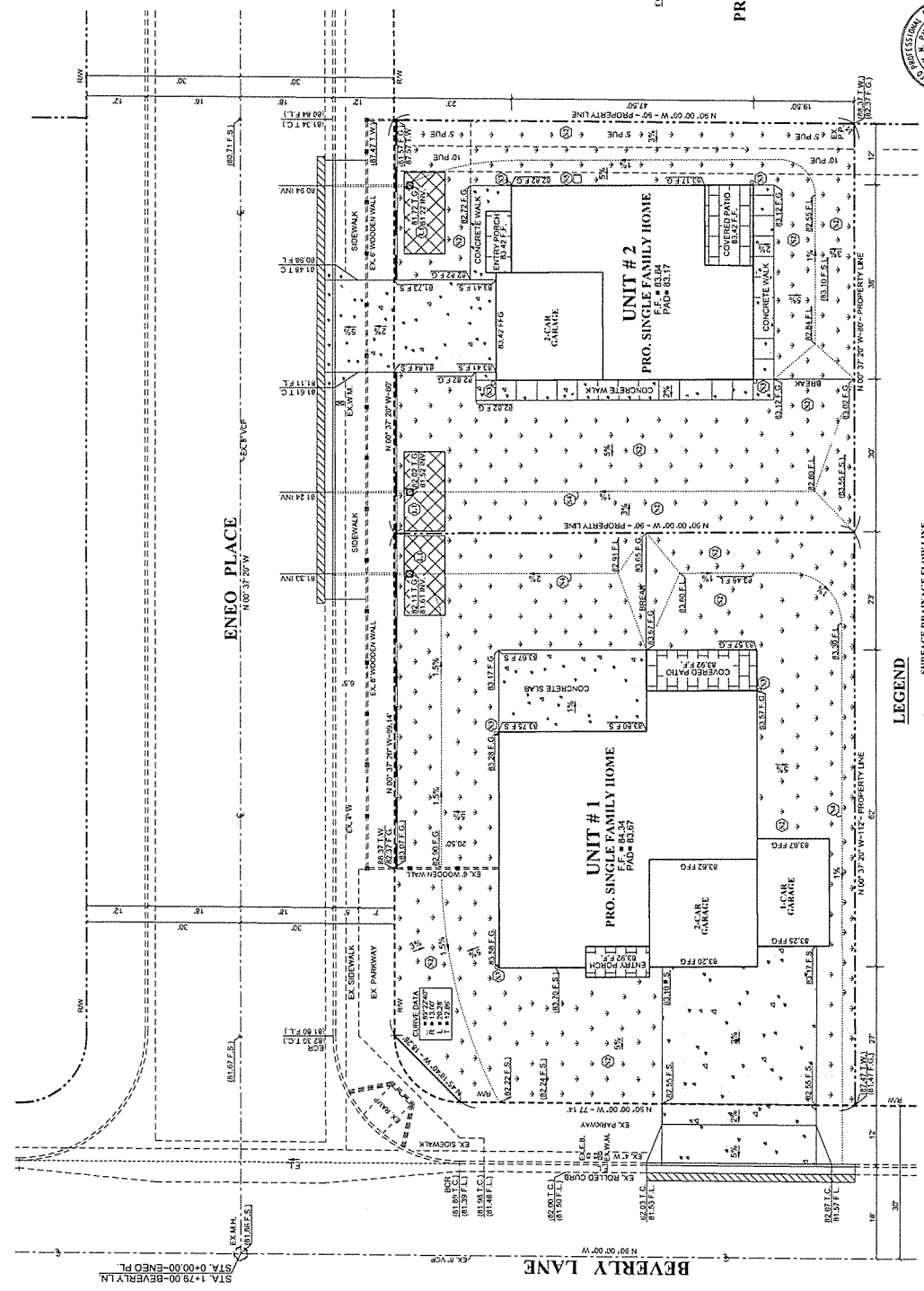
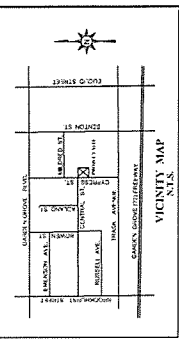
- ⊗ Existing easement for public street purposes conveyed to the County of Orange by Deed recorded July 15, 1954 in book 2771, page 180 of Official Records
- ⊗ Existing 10 foot easement for Southern California Edison Company recorded December 28, 1951 in book 2287, page 503 of Official Records.
- ⊗ Existing 5 feet easement for The Pacific Telephone and Telegraph Company recorded December 17, 1951 in book 2265, page 24 of Official Records.
- ⊗ Proposed corner cut-off for public access ramp to dedicated to City of Garden Grove.

**BASIS OF BEARINGS:**

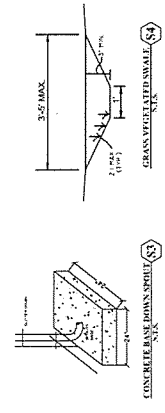
The enclosing of Beverly Lane being N60°00'00"E in the Tract 1545 (Book 45, Page 34) is used for Basis of Bearing of this project.



REVISIONS		BENCH MARK	LEGAL DESCRIPTION	OWNER/DEVELOPER	SOIL ENGINEER	CIVIL ENGINEER	PLANS WERE PREPARED UNDER THE SUPERVISION OF:	PLAN NUMBER	
<p><b>DIG ALERT</b>                        LOCAL, FULL DEPTH                      AT LEAST TWO DAYS                      BEFORE ANY EXCAVATION WORK</p>		<p>3. A BENCH MARK WAS FOUND AT THE CORNER OF PLEASANT PLACE AND BEVERLY LANE. THE BENCH MARK IS A BRONZE CITY OF GARDEN GROVE MARK WITH THE NUMBER 117. THE BENCH MARK IS LOCATED IN THE SOUTHWEST CORNER OF THE BLOCK AND IS SET EAST OF THE BLOCK. THE BENCH MARK IS AT THE CORNER OF PLEASANT PLACE AND BEVERLY LANE.</p>	<p>LOT 166 SUPERSEDED BY LEGAL DESCRIPTION IN THE CITY OF GARDEN GROVE, CALIFORNIA. THE LEGAL DESCRIPTION OF THE PROPERTY IS: BEVERLY LANE, A CALIFORNIA CORPORATION, 9811 Beverly Lane, Garden Grove, CA 92643. CONTACT: HARRY BILAKA, PHONE: (714) 751-1111. EMAIL: CITY@HARRYBILAKA.COM</p>	<p><b>HENNER CORP</b>                      A CALIFORNIA CORPORATION                      9811 Beverly Lane, Garden Grove, CA 92643                      Contact: Harry Bilaka                      Phone: (714) 751-1111                      Email: CITY@HARRYBILAKA.COM</p>	<p><b>LAN &amp; ASSOCIATES</b>                      12011 Harbor Boulevard                      Garden Grove, CA 92643                      RCE, M.E., Engineer 31023                      Tel: (714) 44-6915                      Email: lan@lanand.com</p>	<p><b>LAN &amp; ASSOCIATES</b>                      12011 Harbor Boulevard                      Garden Grove, CA 92643                      RCE, M.E., Engineer 31023                      Tel: (714) 44-6915                      Email: lan@lanand.com</p>	<p>LAN &amp; ASSOCIATES                      12011 Harbor Boulevard                      Garden Grove, CA 92643                      RCE, M.E., Engineer 31023                      Tel: (714) 44-6915                      Email: lan@lanand.com</p>	<p><b>G-0000</b>                      Address: 9811 Beverly Lane, Garden Grove CA 92841</p>	<p>SHEET 4 OF 4</p>



- LEGEND**
- SURFACE DRAINAGE FLOW LINE
  - CONCRETE GUTTER/DRIVEWAY PAVEMENT
  - POROUS CONCRETE PAVEMENT (BMPINF-6)
  - INFILTRATION TRENCH (BMPINF-2)
  - BUILDING STRUCTURES
  - TRASH BIN
  - WATER FLOW



PROPOSED TWO SINGLE FAMILY RESIDENCES  
9811 Beverly Lane, Garden Grove CA 92841

TREATMENT CONTROL BMPs

- (1) 18" x 180" INFILTRATION TRENCH (BMPINF-2)
- (2) EFFICIENT IRRIGATION OF LANDSCAPED AREA
- (3) CONCRETE BASE DOWNSPOUT
- (4) GRASS VEGETATED SWALE
- (5) TRASH BIN



CIVIL ENGINEER  
J.W. & ASSOCIATES  
10111 W. 14th St.  
Garden Grove, CA 92841  
Tel: (714) 414-8211  
Email: jwhume@jwa.com

PLANS REVIEWED FOR CONFORMANCE WITH  
LANS BUREAU  
K.C. WICKES/ENGINEER/SEAL

CITY OF GARDEN GROVE  
CONCEPTUAL WATER QUALITY MANAGEMENT PLAN  
Address: 9811 Beverly Lane, Garden Grove CA 92841



**B-122**

SHEET 1 OF 4

STD. PLAN NUMBER

DATE BY

REVISIONS

City Engineer R.C.E. 52125 Exp. 12-31-16

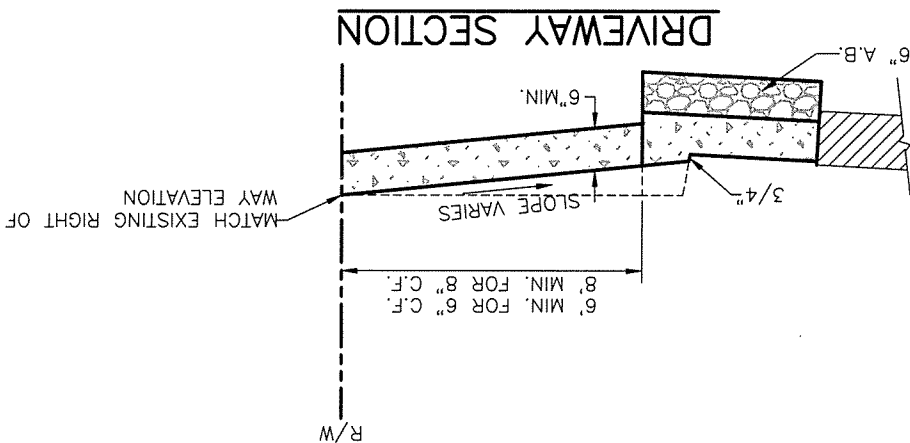
Date 12-8-15

Approved

City of Garden Grove California  
**GARDEN GROVE**

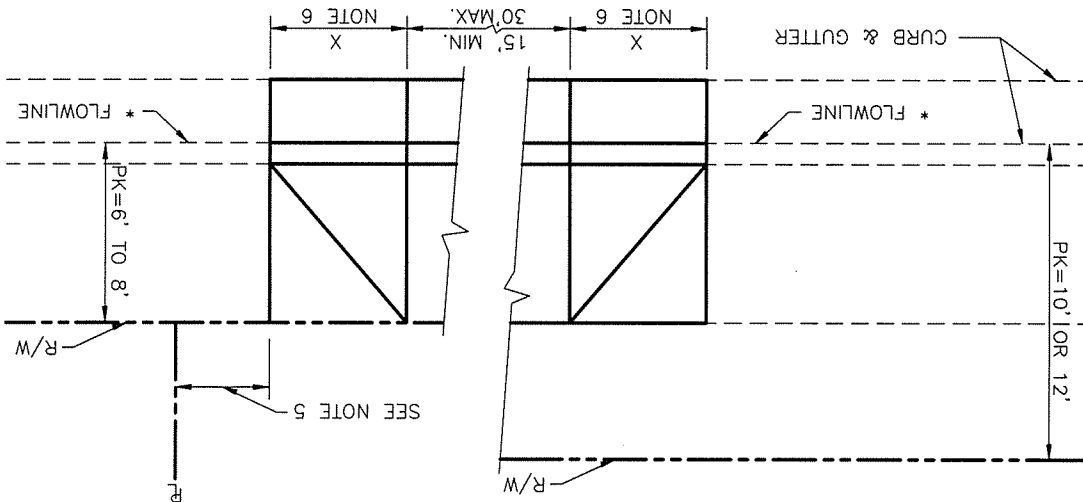
**LOCAL STREET DRIVEWAY OPTION #1**

SEE SHEET 2 FOR CONSTRUCTION NOTES.

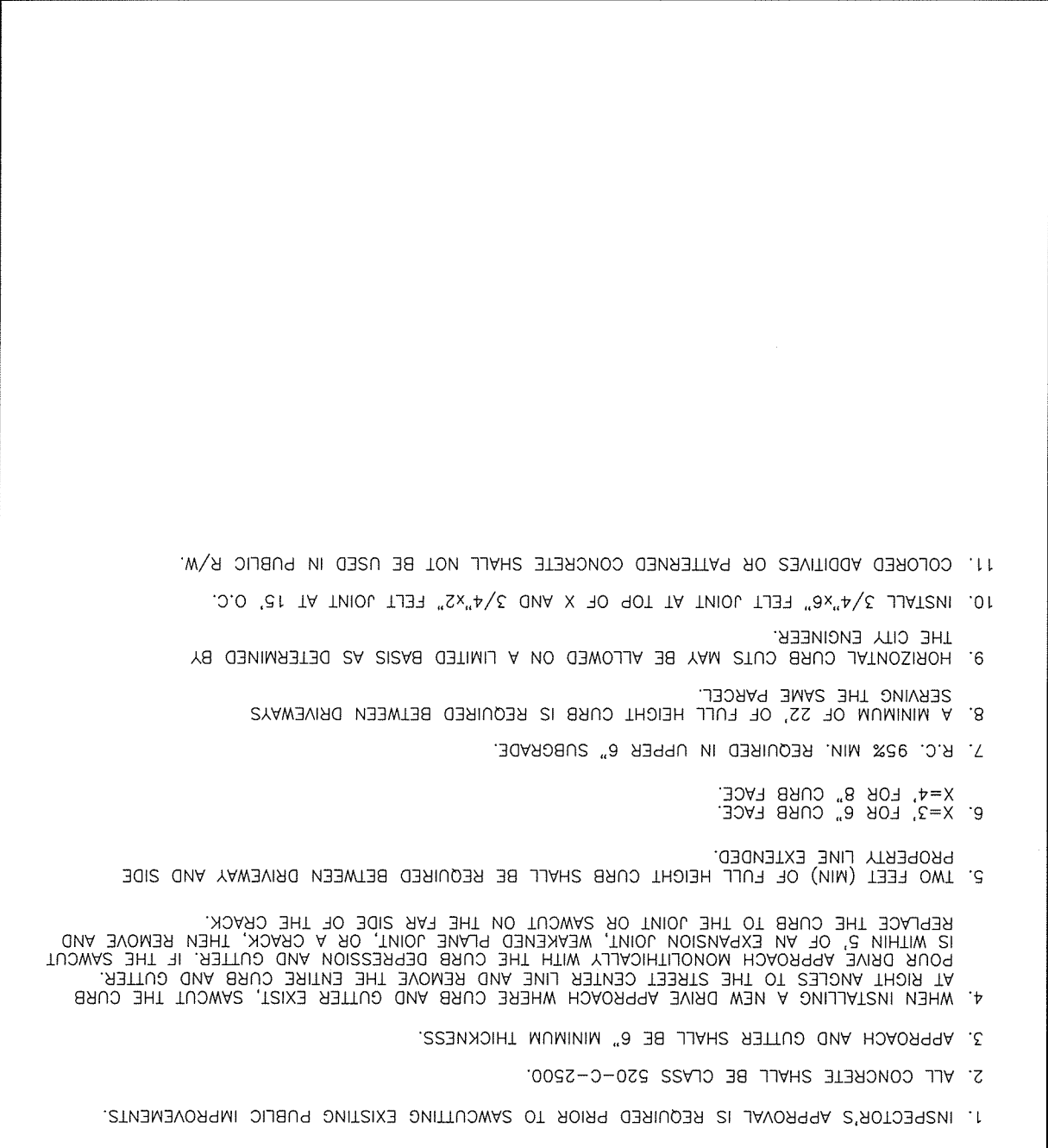


**PLAN - OPTION #1**  
 N.T.S.

SEE NOTE 4 IF EXISTING CURB & GUTTER IS TO BE REMOVED FOR CONSTRUCTION OF NEW DRIVEWAY  
 \* A POSITIVE FLOWLINE SHALL BE MAINTAINED. EXISTING CURB & GUTTER SHALL BE REMOVED AND REPLACED AS DETERMINED BY THE CITY ENGINEER, TO ACHIEVE PROPER DRAINAGE.



City of Garden Grove California  
**G** GARDEN GROVE  
 LOCAL STREET DRIVEWAY OPTION #1



**NOTES:**

1. INSPECTOR'S APPROVAL IS REQUIRED PRIOR TO SAWCUTTING EXISTING PUBLIC IMPROVEMENTS.

2. ALL CONCRETE SHALL BE CLASS 520-C-2500.

3. APPROACH AND GUTTER SHALL BE 6" MINIMUM THICKNESS.

4. WHEN INSTALLING A NEW DRIVE APPROACH WHERE CURB AND GUTTER EXIST, SAWCUT THE CURB AT RIGHT ANGLES TO THE STREET CENTER LINE AND REMOVE THE ENTIRE CURB AND GUTTER. POUR DRIVE APPROACH MONOLITHICALLY WITH THE CURB DEPRESSION AND GUTTER. IF THE SAWCUT IS WITHIN 5' OF AN EXPANSION JOINT, WEAKENED PLANE JOINT, OR A CRACK, THEN REMOVE AND REPLACE THE CURB TO THE JOINT OR SAWCUT ON THE FAR SIDE OF THE CRACK.

5. TWO FEET (MIN) OF FULL HEIGHT CURB SHALL BE REQUIRED BETWEEN DRIVEWAY AND SIDE PROPERTY LINE EXTENDED.

6. X=3' FOR 6" CURB FACE.  
 X=4' FOR 8" CURB FACE.

7. R.C. 95% MIN. REQUIRED IN UPPER 6" SUBGRADE.

8. A MINIMUM OF 22' OF FULL HEIGHT CURB IS REQUIRED BETWEEN DRIVEWAYS SERVING THE SAME PARCEL.

9. HORIZONTAL CURB CUTS MAY BE ALLOWED ON A LIMITED BASIS AS DETERMINED BY THE CITY ENGINEER.

10. INSTALL 3/4"x6" FELT JOINT AT TOP OF X AND 3/4"x2" FELT JOINT AT 15' O.C.

11. COLORED ADDITIVES OR PATTERNED CONCRETE SHALL NOT BE USED IN PUBLIC R/W.

RESOLUTION NO. 6042-22

RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF GARDEN GROVE APPROVING SITE PLAN NO. SP-112-2022 AND TENTATIVE PARCEL MAP NO. PM-2021-112 FOR PROPERTY LOCATED AT 9801 AND 9811 BEVERLY LANE, ASSESSOR'S PARCEL NOS 133-132-01 AND 133-132-02.

BE IT RESOLVED that the Planning Commission of the City of Garden Grove, in a regular session assembled on May 19, 2022 hereby approves Site Plan No. SP-112-2022 and Tentative Parcel Map No. PM-2021-112, for properties located at the northeast corner of Beverly Lane and Eneo Place, at 9801 and 9811 Beverly Lane, Assessor's Parcel No. 133-132-01 and 133-132-02.

BE IT FURTHER RESOLVED in the matter of Site Plan No. SP-112-2022 and Tentative Parcel Map No. PM-2021-112, the Planning Commission of the City of Garden Grove does hereby report as follows:

1. The subject case was initiated by Henry Balanza c/o HENMER CORP.
2. The applicant is requesting Site Plan and Tentative Parcel Map approval to reconfigure two (2) existing adjacent parcels to create two (2) new lots, for the purpose of constructing a new two-story single-family home on each lot. Lot 1 will have a lot size of 10,500 square feet. Lot 2 will have a lot size of 7,238 square feet. The existing single-family dwelling will be demolished to accommodate the proposed development.
3. Pursuant to the California Environmental Quality Act ("CEQA"), the City of Garden Grove Planning Commission hereby determines that this project is categorically exempt from the California Environmental Quality Act (CEQA) pursuant to Section 15303 (New Construction or Conversion of Small Structures) and Section 15315 (Minor Land Divisions) of the CEQA Guidelines (14 Cal. Code Regs., Sections 15315 and 15303).
4. The subject site comprises two (2) adjoining Assessor Parcels: 133-132-01 and 133-132-02, under two (2) different addresses: 9801 and 9811 Beverly Lane, respectively. Both parcels have a General Plan Land Use Designation of Low Density Residential and are zoned R-1-7 (Single-Family Residential). 9801 Beverly Lane, or Lot A, is a vacant, narrow strip of land located at the corner of Beverly Lane and Eneo Place. 9811 Beverly Lane, or Lot B, is an interior lot abutting Lot A to the east, with the lot fronting onto Beverly Lane. Lot B is improved with a one-story, single-family dwelling that consists of four (4) bedrooms and a detached two-car garage that will be demolished to accommodate the proposed subdivision.
5. Existing land use, zoning, and General Plan designation of property in the vicinity of the subject property have been reviewed.



6. Report submitted by the City staff was reviewed.
7. Pursuant to a legal notice, a public hearing was held on May 19, 2022, and all interested persons were given an opportunity to be heard.
8. The Planning Commission gave due and careful consideration to the matter during its meeting on May 19, 2022; and

BE IT FURTHER RESOLVED, FOUND AND DETERMINED that the facts and reasons supporting the conclusion of the Planning Commission, as required under Municipal Code Section 9.04.030 are as follows:

FACTS:

The subject properties are located within a single-family residential (R-1) zoned neighborhood, at the northeast corner of Beverly Lane and Eneo Place. The subject site is comprised two (2) adjoining Assessor Parcels: 133-132-01 and 133-132-02, under two (2) different addresses: 9801 and 9811 Beverly Lane, respectively. Both parcels have a General Plan Land Use Designation of Low Density Residential and are zoned R-1-7 (Single-Family Residential). The subject site is surrounded by properties improved with one and two-story residential dwellings.

9801 Beverly Lane, or Lot A, is a vacant, narrow strip of land located at the corner of Beverly Lane and Eneo Place. 9811 Beverly Lane, or Lot B, is an interior lot abutting lot A to the east, with the lot fronting onto Beverly Lane. Lot B is improved with a one-story, single-family dwelling that consists of four (4) bedrooms and a detached two-car garage. The dwelling was rented out and used as an adult residential care facility until January 2022.

The applicant, who owns both lots, wishes to reconfigure the two (2) existing lots to create two (2) new lots for the purpose of constructing a new single-family dwelling on each lot. The existing dwelling will be demolished to facilitate the proposed development. Although the project site already consists of two (2) parcels, the proposed lot orientation will be significantly different from the existing configuration; thus, a new Parcel Map is required. Section 9.40.030 of the Municipal Code requires a Tentative Parcel Map approval whenever a parcel, or contiguous parcels, of land is/are proposed to be subdivided for the purpose of creating four or fewer lots. Section 9.32.030 of the code requires a Site Plan approval for any building to be constructed as part of a single-family subdivision. In addition, since the proposed housing project requires the demolition of an existing rental unit, the California Department of Housing and Community Development (HCD) has orally advised City staff that Government Code §66300(d) applies and requires the City to impose specified replacement housing and affordability requirements as conditions of approval.

Upon approval of the proposed Tentative Parcel Map, Lot 1 will be 10,500 square feet, while Lot 2 will be 7,238 square feet. One (1) two-story single-family home will be constructed on each lot.

FINDINGS AND REASONS:

**SITE PLAN**

1. The Site Plan complies with the spirit and intent of the provisions, conditions, and requirements of the Municipal Code and other applicable ordinances and is consistent with the General Plan.

The subject site is zoned R-1 (Single-Family Residential), and is located within an R-1 zoned neighborhood that is improved with one and two-story single-family dwellings. The Low Density Residential designation is intended to create, maintain, and enhance residential areas characterized by detached, single-family homes on a single parcel. The R-1 (Single-Family Residential) zone is intended to provide for the establishment and promotion of single-family detached residences on individual lots and compatible associated activities.

A new two-story, single-family home will be constructed on each lot, subject to the approval of the Site Plan. The proposed project is compatible with the character of the surrounding single-family neighborhood as it is designed to meet all of the development standards for R-1 zone, and all other applicable ordinances. This includes, but is not limited to, compliance with setbacks, parking, lot coverage, building height, and the minimum lot size and lot width requirements of the zone.

In addition, the proposal is consistent with several goals and policies as set forth by the General Plan.

*Policy LU-2.4: Assure that the type and intensity of land use shall be consistent with that of the immediate neighborhood* - The properties are zoned R-1, and are located within a single-family residential-zoned neighborhood. The proposed map is for the purpose of building a single-family home on each new lot, similar with the development pattern of the immediate neighborhood.

*LU-IMP-2B: New development shall be similar in scale to the adjoining residential neighborhood to preserve its character* - The proposed project is to construct two (2) new single-family homes, similar to neighboring properties that are also improved with single-family homes.

*Goal H-2: Housing supply to accommodate housing needs at all affordability levels* – Although the proposed project includes the demolition of a “protected” unit, the applicant will replace the “protected” unit with one unit for lower income households as required by Government Code section 66300(d). The project has been conditioned as such.

*Policy H-3.1: Maintain land use policies and regulations that create capacity for development of a range of residential development types that can fulfill local housing needs, including low-density single-family uses projects* – The subject site consists of two (2) parcels with one legal nonconforming lot. Although the Code allows nonconforming lots to be developed upon approval of a variance, the physicality of the nonconforming lot would make it impossible to develop. Under the current map, only one (1) dwelling unit will be allowed despite having two (2) separate parcels. The proposed Tentative Parcel Map and Site Plan is necessary since it will not only create two (2) new lots that conform to the R-1 zone development standards, but also encourage the production of one additional housing unit.

*Policy H-3.8: New Housing. Critically analyze the location of any proposed new housing to determine suitability for healthy living conditions* – The proposed project is compatible with the character of the surrounding single-family neighborhood as it is designed to meet all of the development standards for R-1 zone, and all other applicable ordinances. This includes, but is not limited to, compliance with setbacks, parking, lot coverage, building height, and the minimum lot size and lot width requirements of the zone.

2. The proposed development does not adversely affect essential on-site facilities such as off-street parking, loading and unloading areas, traffic circulation, and points of vehicular and pedestrian access

Lot 1 will be located at the corner of Beverly Lane and Eneo Place with the lot fronting onto Beverly Lane. A new thirty-foot (30'-0") driveway approach will be installed along Beverly Lane to allow vehicle access from the right-of-way to Lot 1. Lot 1 will be provided with a three-car garage and three open parking spaces, for a total of six (6) parking spaces.

Lot 2 will abut Lot 1 to the north, and will front onto Eneo Place. Lot 2 will be accessed from an eighteen-foot (18'-0") wide drive approach along the Eneo Place. Lot 2 will be provided with a two-car garage and two open parking spaces, for a total of four (4) parking spaces.

The City's Traffic Engineering Section has reviewed the proposed project, and all appropriate conditions of approval have been incorporated to minimize any adverse impacts to surrounding streets.

3. The development, as proposed, will not adversely affect essential public facilities such as streets and alleys, utilities, and drainage channels.

The streets in the area are adequate to accommodate the development once the developer provides the necessary improvements for the project. Required improvements include new driveway approaches, new sidewalks, new curbs and gutter on both Beverly Lane and Eneo Place. Existing utilities and drainage channels in the area are otherwise adequate to accommodate the development. The proposed development will provide landscaping and proper grading of the site, thereby improving drainage in the area. New utility services will be provided underground to the maximum extent possible, and all above-ground equipment will be screened.

The Public Works Department has reviewed the project, and has incorporated all the appropriate conditions of approval to minimize any adverse impacts.

4. The development does have a reasonable degree of physical, functional, and visual compatibility with neighboring uses and desirable neighborhood characteristics.

The subject site is located in an area improved with existing single-family residences. The proposed project will be compatible with the surrounding neighborhood. Architecturally, the residential units have been designed with enhanced facades to be aesthetically complimentary with the neighboring properties. A variety of architectural detailing, materials, massing, and rooflines help create visual intrigue.

The project has been designed in accordance with the R-1 (Single-Family Residential) development standards. In particular, each lot will have a single-family home that complies with the development standards of the R-1 zone, including, but not limited to minimum lot sizes, setbacks, parking, lot coverage, and maximum building height.

The City's Community and Economic Development Department has reviewed the proposed project, and all appropriate conditions of approval have been incorporated to ensure physical, functional, and visual compatibility with the surrounding neighborhood.

5. Through the planning and design of buildings and building replacement, the provision of open space landscaping and other site amenities will attain an attractive environment for the occupants of the property.

The project is designed to comply with the fifty-percent (50%) lot coverage requirement of the R-1 zone, ensuring each parcel maintains the required amount of usable open space. Additionally, the conditions of approval will ensure that the landscaping requirements of the Municipal Code, including the City's Landscape Water Efficiency Guidelines, will be complied with.

### **TENTATIVE PARCEL MAP**

1. The proposed map is consistent with the General Plan.

The property has a General Plan land use designation of Low Density Residential. The proposed map is consistent with the provisions of the General Plan Low Density Residential land use designation. This designation is intended to create, maintain, and enhance residential areas characterized by detached, single-family homes on a single parcel. The proposed Tentative Parcel Map will create two (2) parcels by combining the square footage of the two existing lots and establishing a new shared lot line. Each new lot will be improved with one single-family home. The proposal is consistent with several goals and policies as set forth by the General Plan.

*Policy LU-2.4: Assure that the type and intensity of land use shall be consistent with that of the immediate neighborhood* - The properties are zoned R-1, and are located within a single-family residential zoned neighborhood. The proposed map is approved for the purpose of building a single-family home on each new lot, similar with the development pattern of the immediate neighborhood.

*LU-IMP-2B: New development shall be similar in scale to the adjoining residential neighborhood to preserve its character* - The proposed map will accommodate the construction of new single-family homes on each lot, similar to neighboring properties that are also improved with single-family homes.

*Goal H-2: Housing supply to accommodate housing needs at all affordability levels* - Although the proposed project includes the demolition of a "protected" unit, the applicant will replace the "protected" unit with one for lower income households as required by the SB 330. The project has been conditioned as such.

*Policy H-3.1: Maintain land use policies and regulations that create capacity for development of a range of residential development types that can fulfill local housing needs, including low-density*

*single-family uses* – The subject site consists of two (2) parcels with one (1) legal nonconforming lot. Although the Code allows nonconforming lots to be developed upon approval of a variance, the physicality of the nonconforming lot would make it impossible to develop. Under the current map, only one (1) dwelling unit will be allowed to build at this location despite having two (2) separate parcels. The proposed Tentative Parcel Map is necessary since it will not only create two (2) new lots that conform to the R-1 zone development standards, but it will also encourage the production of one additional housing unit at the subject site.

*Policy H-3.8: New Housing. Critically analyze the location of any proposed new housing to determine suitability for healthy living conditions* – The proposed project is compatible with the character of the surrounding single family neighborhood as it is designed to meet all of the development standards for R-1 zone, and all other applicable ordinances. This includes, but is not limited to, compliance with setbacks, parking, lot coverage, building height, and the minimum lot size and lot width requirements of the zone.

2. The design and improvement of the proposed subdivision is consistent with the General Plan.

The design and improvement of the proposed map are consistent with the Low Density Residential General Plan land use designation. The creation of a two (2) lot subdivision for the purpose of constructing one single-family home on each lot is consistent with the General Plan. As the zoning code is implementing the General Plan, being in compliance to the zoning requirement is equivalent to being consistent with the General Plan. In particular, the project complies with the minimum lot size requirement of 7,200 square feet of the R-1 zone. Additionally, the configuration of the lots and design of the homes is compatible with the existing residential neighborhood. With the conditions of approval, the design and improvement of the subject site is consistent with the spirit and intent of the General Plan.

3. The site is physically suitable for the proposed type of development.

The site is adequate in size and shape to accommodate the proposed project. Each new parcel complies with the minimum lot size of 7,200 square feet, the minimum lot width of sixty feet (60'-0") for interior lot and sixty-five feet (65'-0") for corner lot, and all of the other development standards of the R-1 zone. The placement and size of the proposed single-family homes complies with the development standards for the R-1 zone, including, but not limited to: setbacks, parking, lot

coverage, and open space provisions.

4. The design of the subdivision and the proposed improvements are not likely to cause substantial environmental damage or substantially and avoidably injure fish or wildlife or their habitat.

The subject site is located in a developed urban area, and the proposed improvements are not anticipated to impact any existing fish or wildlife habitat.

5. The requirements of the California Environmental Quality Act have been satisfied.

The requirements of the California Environmental Quality Act have been satisfied. The project was determined to be exempt pursuant to Section 15303(a) (New Construction or Conversion of Small Structures) and Section 15315 (Minor Land Divisions) of the California Environmental Quality Act.

6. The site is physically suitable for the proposed density of the development.

The combined square footage of the site is adequate in size and shape to accommodate the creation of two (2) new parcels that comply with the minimum lot size and width requirements of the R-1-7 zone. The proposed design of the residential lot includes the placement of one single-family home on each lot, which complies with the density requirement of the General Plan. The placement of the new single-family home on each lot complies with the R-1 development standards. The project complies with the minimum parking, open space, setbacks, lot coverage, and building height requirements of the R-1 zone.

7. The design of the subdivision and the proposed improvements are not likely to cause serious public health problems.

As long as the conditions of approval are adhered to for the life of the project, the design of the proposed map, and improvements, are not likely to cause serious public health problems. Various City divisions, including the Traffic Division, Water Division, Engineering Division, Police Department, Building and Safety, and the Planning Services Division, have reviewed the proposed development, and have applied conditions of approval to minimize against any potential negative impacts that the project may have on the community. The conditions of approval for on- and off-site improvements will safeguard the public health.

8. The design of the subdivision and the proposed improvements will not conflict with easements of record or easements established by court judgment acquired by the public at large for access through or use of property within the proposed subdivision; or, if such easements exist, that alternate easements for access or for use will be provided, and that these will be substantially equivalent to the ones previously acquired by the public.

The design of the proposed map and improvements will not conflict with easements of record, or easements established by court judgment acquired by the public at large for access through or use of property within the proposed subdivision. The project has been designed to avoid development over existing easements.

9. The design and improvement of the proposed subdivision are suitable for the uses proposed, and the subdivision can be developed in compliance with the applicable zoning regulations.

The design of the proposed use, and the subdivision are developed in compliance with the applicable zoning requirements. The design of the subdivision is suitable for the low-density residential project, and complies with the spirit and intent of the General Plan, and the Subdivision Map Act. The project has also been designed to comply with the R-1 development standards, and complies with the minimum lot size, and minimum lot width requirement.

10. The design of the subdivision provides, to the extent feasible, for future passive or natural heating and cooling opportunities in the subdivision (Gov. Code Sec. 66473.1).

To the extent feasible, the project has been designed in accordance with Government Code Section 66473.1, such as to allow for passive or natural heating opportunities in the subdivision design, to encourage the orientation of structures to take advantage of shade and prevailing breezes, to allow solar access for passive heating and opportunities for placement of shade trees and other vegetation for cooling.

11. The design, density and configuration of the subdivision strikes a balance between the effect of the subdivision on the housing needs of the region and of public service needs of City residents and available fiscal and environmental resources.

The project has been reviewed in relation to the housing needs and goals of the City, and is compatible with the existing residential projects in the



vicinity. The site is located in an area with existing single-family residences. The property is currently improved with a single-family home and detached garage that are to be demolished to facilitate the proposed development. A new, two-story, single-family home, will be constructed on each lot. The proposed Tentative Parcel Map will be compatible with the surrounding area since the lots are designed to comply with the minimum lot size required by the Zoning Code. The project complies with the density requirements of the General Plan, and complies with all applicable R-1 development standards.

12. That the character of the subdivision is compatible with the design of existing structures and that the lot sizes of the subdivision are substantially the same as the lot sizes within the general area.

The proposed design of the residential lot includes the placement of one single-family home on each lot, which complies with the density requirement of the General Plan. The placement of the new single-family home on each lot complies with the R-1 development standards. The project complies with the minimum parking, open space, setbacks, lot coverage and building height requirements of the R-1 zone.

13. The subject property is not located within a state responsibility area or a very high fire hazard severity zone, the proposed is served by local fire suppression services, and the proposed subdivision meets applicable design, location, and ingress-egress requirements.

The subject site is not in a state responsibility area, or a very-high fire hazard severity zone. The project will be served by all applicable local fire suppression services. Both proposed parcels will meet all applicable design requirements of the City, and Orange County Fire Authority. The project complies with all ingress and egress requirements.

14. The discharge of waste from the proposed subdivision into the existing sewer system will not result in violation of existing requirements prescribed by the California Regional Water Quality Control Board. The conditions of approval for on- and off-site improvements will ensure permitted capacity of the public sewer system is not exceeded.

The design of the proposed use, and the subdivision are developed in compliance with the applicable zoning requirements. The design of the subdivision is suitable for the low-density residential project, and complies with the spirit and intent of the General Plan, and the Subdivision Map Act. The project has also been designed to comply with the R-1 development standards, and complies with the minimum lot size, and minimum lot width requirement.

INCORPORATION OF FACTS AND FINDINGS SET FORTH IN STAFF REPORT

In addition to the foregoing, the Planning Commission incorporates herein by this reference, the facts and findings set forth in the staff report.

BE IT FURTHER RESOLVED that the Planning Commission does conclude:

1. The Site Plan and Tentative Parcel Map possess characteristics that would justify the request in accordance with Municipal Code Section No. 9.32.030.D.3 (Site Plan) and Section 9.40.060 (Tentative Maps).
2. In order to fulfill the purpose and intent of the Municipal Code and thereby promote the health, safety, and general welfare, the attached Conditions of Approval (Exhibit "A") shall apply to Site Plan No. SP-112-2022 and Tentative Parcel Map No. PM-2021-112

Adopted this 19th day of May 2022

**EXHIBIT "A"**

**Site Plan No. SP-112-2022**

**Tentative Parcel Map No. PM-2021-112**

9801 AND 9811 Beverly Lane

(Assessor's Parcel Nos. 133-132-01 and 133-132-02)

**CONDITIONS OF APPROVAL**

**General Conditions**

1. The applicant and each owner of the property shall execute, and the applicant shall record a "Notice of Agreement with Conditions of Approval and Discretionary Permit of Approval," as prepared by the City Attorney's Office, on the property. Proof of such recordation is required prior to issuance of building permits.
2. All Conditions of Approval set forth herein shall be binding on and enforceable against each of the following, and whenever used herein, the term "applicant" shall mean and refer to each of the following: the project applicant, Henry Balanza c/o HENMER CORP., the current owner of the property, and the future owner(s) and tenants(s) of the property, and each of their respective successors and assigns. All conditions of approval are required to be adhered to for the life of the project, regardless of property ownership. Any changes of the Conditions of Approval require approval by the Planning Commission.
3. Approval of this Site Plan and the Tentative Parcel Map shall not be construed to mean any waiver of applicable and appropriate zoning and other regulations; and wherein not otherwise specified, all requirements of the City of Garden Grove Municipal Code shall apply.
4. Minor modifications to the Site Plan, the Tentative Parcel Map, and/or these Conditions of Approval, which do not materially change the scope or intensity of the project and which will not result in impacts that have not previously been addressed, may be approved by the Community and Economic Development Director, in his or her discretion. Proposed modifications to the project and/or these Conditions of Approval determined by the Community and Economic Development Director not to be minor in nature shall be subject to approval of new and/or amended land use entitlements by the applicable City hearing body.
5. The approved site plan, floor plan, and use of the subject property, as represented by the Applicant, are an integral part of the decision approving this Site Plan and Tentative Parcel Map. If major modifications are made to the approved floor plan, site plan, or other related changes that result in the

intensification of the project or create impacts that have not been previously addressed, the proper entitlements shall be obtained reflecting such changes.

6. All conditions of approval shall be implemented at the applicant's expense, except where specified in the individual condition.

### **Public Works Engineering Division**

#### Project Design

7. A geotechnical study prepared by a registered geotechnical engineer is required. The report shall analyze the liquefaction potential of the site and make recommendations. The report shall analyze sub-surface issues related to the past uses of the site, including sub-surface tanks and basement and septic facilities. Any soil or groundwater contamination shall be remediated prior to the issuance of a building permit per the requirements of the Orange County Health Department and the mitigation requirements of governing regulatory requirements. The report shall make recommendations for foundations and pavement structural section design of interior streets and parking spaces. The report shall also test and analyze soil conditions for LID (Low Impact Development) principles and the implementation of water quality for storm water runoff, including potential infiltration alternatives, soil compaction, saturation, permeability and groundwater levels.
8. Prior to the issuance of any grading or building permits, the applicant shall submit to the City for review and approval a final design Water Quality Management Plan that:
  1. Addresses required mitigation Site Design Best Management Practices (BMPs) based upon the latest Santa Ana Regional Water Quality Control Board (SARWQCB) approved Drainage Area Management Plan (DAMP) as identified in the geotechnical report recommendations and findings, including, but not limited to, infiltration minimizing impervious areas, maximizing permeability, minimizing directly connected impervious areas, creating reduced or "zero discharge" areas, and conserving natural areas as required by the latest adopted County of Orange Technical Guidance Document (TGD).
  2. BMP's shall be sized per the requirements of the latest Technical Guidance Documents.
  3. Incorporates the applicable Routine Source Control BMPs as defined in the DAMP.
  4. Incorporates structural and Treatment Control BMPs as defined in the DAMP.

Generally describes the long-term operation and maintenance requirements for the Treatment Control BMPs.

5. Identifies the entity that will be responsible for long-term operation and maintenance of the Treatment Control BMPs.
6. Describes the mechanism for funding the long-term operation and maintenance of the Treatment Control BMPs.
7. Provides a hydrological analysis with scaled map as well as hydrologic and hydraulic calculations to size storm drains per the Orange County RDMD standards.
9. Parkway culverts shall be designed per City of Garden Grove Standard Plan B-209. Storm drain lateral pipe connections to city maintained storm drains within City right of way shall be RCP with a minimum diameter of 18-inches.
10. Grading and Street improvement plans prepared by a registered Civil Engineer are required. As required under Section 107 of the California Building Standards Code (CBC), the grading plan shall be based on a current survey of the site, including a boundary survey, topography on adjacent properties up to 30 feet outside the boundary, and designed to preclude cross-lot drainage. Minimum grades shall be 0.50% for concrete flow lines and 1.25% for asphalt. The grading plan shall also include water and sewer improvements. The grading plan shall include a coordinated utility plan showing all existing utility facilities, easements and proposed utility facilities. All on-site improvements shall be tied by horizontal dimensional control to the property boundary as established by survey. A minimum uninterrupted 20-foot wide throat access to the site is required from the street for the multi residential projects and shall meet the requirements of the California Fire Code throughout the site. Vehicle maneuvering, as demonstrated by Auto Turn along private streets and access ways, shall be demonstrated on the grading plan. Street improvement plans shall conform to all format and design requirements of the City Standard Drawings & Specifications.
11. All vehicular access drives to the site shall be provided in locations approved by the City Traffic Engineer. (Policies and Procedures – TE-17)
12. The applicant shall dedicate right-of-way and corner cut-off from the existing right-of-way line fronting the project to the ultimate right-of-way for future ultimate street improvements. Applicant shall record said easement in a format conforming to City policy guidelines under the approval authority of the City Engineer and City Attorney. The applicant shall submit to the Engineering Division an updated title report along with copies of the recorded instruments listed in the title report, as well as reference maps and exhibits used to prepare legal description. Dedication exhibits shall include closure calculations along with

- a detailed plat for review and approval of the street dedication exhibits by the City Engineer.
13. The applicant shall coordinate with Planning Services Division and Orange County Fire Authority to identify proper emergency vehicle access to the site and shall provide the Engineering Division a copy of the approval letters upon first submittal of the grading and street improvement plans.
  14. Any new drive approaches to the site shall be constructed in accordance with Garden Grove Standard B-122 as they conform to land use and roadway designation.
  15. The grading plan shall depict an accessibility route for the ADA pathway in conformance with the requirements of the Department of Justice standards, latest edition and section 1110A of the California Building Code.
  16. Any new or required block walls and/or retaining walls shall be shown on the grading plans, both in plan-view and cross sections. Cross sections shall show vertical and horizontal relations of improvements (existing and proposed) on both sides of property lines. Required wall heights shall be measured vertically from the highest adjacent finished grade. Block walls shall be designed in accordance with City of Garden Grove Standard B-504, B-505, B-506 & B-508 or designed by a professional registered engineer. In addition, the following shall apply:
    - a. The color and material of all proposed block walls, columns, and wrought iron fencing shall be approved by the Planning Services Division prior to installation.
    - b. Openings for drainage through walls shall be shown in section details and approved by the City Engineer. Cross-lot drainage is not allowed.
    - c. Prior to issuance of a building permit, the applicant shall obtain the letter of permission or an encroachment permit for the temporary work from Caltrans and the owner of the adjacent northerly property where the project matchup and construction of a CMU wall will need to occur.
  17. The applicant shall remove any existing substandard driveway approaches, curbs, sidewalks, ADA ramps, pavement sections, tree well and landscaping, and construct Eneo Place frontage improvements as identified below. All landscape, irrigation, sidewalk, and lighting improvements installed within the public rights-of-way shall require the approval of the City Engineer, Street Division, and Planning Services Division shall be maintained by the applicant for the life of the Project.

- a. All proposed public improvements within the public right-of-way on Eneo Place shall be part of the grading improvement plans. All work shall be per City standards and specifications.
  - b. Existing substandard driveway on Eneo Place shall be removed and replaced with new curb, gutter, landscape, and sidewalk per City standards and specifications.
  - c. The new driveway approach to the site shall be constructed in accordance with Garden Grove Standard B-122.
  - d. The owner/contractor shall verify the removal and replacement sections of the sidewalk concrete panels with public works inspector prior to start of construction and replace it with the new sidewalk panels in accordance with City of Garden Grove Standard B-105.
  - e. Construct curb and gutter when replacing the existing driveway approach along the property frontage on Eneo Place in accordance with City Standard Plan B-114.
  - f. The applicant shall locate all existing public utilities across the property frontage and within the property boundary of the project prior to commencement of grading operation and mobilization.
18. The applicant shall coordinate with the Planning Services Division and Public Works Street Division before placing any type of tree within public right-of-way and proposed landscape area. The applicant shall remove any existing substandard driveway approaches, curbs, sidewalks, ADA ramps, pavement sections, tree well and landscaping, and construct Beverly Lane frontage improvements as identified below. All landscape, irrigation, sidewalk, and lighting improvements installed within the public rights-of-way shall be maintained by the applicant and shall require the approval of the City Engineer, Street Division, and Planning Services Division.
- a. All proposed public improvements within the public right-of-way on Beverly Lane shall be part of the grading improvement plans. All work shall be per City standards and specifications.
  - b. Existing substandard driveway approached on Beverly Lane shall be removed and replaced with new curb, gutter, landscape, and sidewalk per City standards and specifications.
  - c. The new driveway approach to the site shall be constructed in accordance with Garden Grove Standard B-122.

- d. Construct curb and gutter when replacing the existing driveway approach along the property frontage on Beverly Lane in accordance with City Standard Plan B-114.
  - e. The applicant shall locate all existing public utilities across the property frontage and within the property boundary of the project prior to commencement of grading operation and mobilization.
  - f. The applicant shall coordinate with the Public Works Street Division before placing any type of tree within public right-of-way and proposed landscape area.
19. For projects subdividing via a parcel map, the applicant shall complete the following:
- a. Prior to recordation of a final parcel map, the surveyor/engineer preparing the map shall tie the boundary of the map into the Horizontal Control System established by the County Surveyor in a manner described in Sections 7-9-330 and 7-9-337 of the Orange County Subdivision Code and Orange County Subdivision Manual, Subarticle 18. The surveyor/engineer shall submit record information to the City on Auto Cad DWG format.
  - b. Prior to recordation of a final parcel map, the surveyor/engineer preparing the map shall submit to the County Surveyor a digital graphics file of said map in a manner described in Sections 7-9-330 and 7-9-337 of the Orange County Subdivision Code and Orange County Subdivision Manual, Subarticle 18. The surveyor/engineer shall submit record information to the City on Auto Cad DWG format.
  - c. Prior to issuance of a grading permit, the applicant shall submit to the Planning Services Division an updated title report, along with copies of the recorded instruments listed in the title report, reference maps used to prepare legal description and the plat for review and approval of the parcel map.
  - d. All subdivision mapping shall be concurrently reviewed by the City Engineering Division and the County of Orange Survey Department. The applicant shall forward all plan check comments received from the County of Orange Survey Department to the City of Garden Grove's Engineering Division upon receipt from the county.
  - e. Prior to the issuance of the street improvement encroachment permit and grading permit, the applicant shall provide subdivision completion bonds for all work constructed under the street improvements and grading permit for review and approval by the City Engineer, City Attorney, and City Finance Department (Risk Management). Alternate forms of security may



be considered, solely at the discretion of the City Engineer and with the concurrence of the City Attorney and City Finance Department (Risk Management)

20. Any proposed new landscaping in public right-of-way shall be approved by Planning Services Division and maintained by the owner for the life of the Project.
21. Driveway widths shall be in accordance with City's Traffic Engineering Policy TE-8 (Driveway Opening Policy).
22. Sight Distance Standards shall be in accordance with City's Traffic Engineering Policy TE-13. All structures and walls shall be designed to ensure proper vision clearance for cars entering or leaving the driveway and parking areas. No structure, wall or fence shall cause an exceedance of the applicable site distance standards set forth in City Traffic Engineering Policy TE 13.
23. The Site Plan and Tentative Map shall comply with the completed Development Review and Comment Sheet prepared pursuant to City's Traffic Engineering Policy TE-17 and provided to the applicant.
24. Traffic Impact Mitigation Fees / Transportation Facilities Fees shall be in accordance with Garden Grove City Council Resolution 9401-16 and Chapter 10.110 of the Garden Grove Municipal Code.
25. The design, placement, and construction of off-street parking areas and spaces, driveways, maneuvering areas, and garages and carports shall conform to the standards set forth in Section 7-9-70.3 of the Orange County Code of Ordinances (Off-street parking requirements for residential uses).

#### Permit Issuance

26. The applicant shall be subject to Traffic Mitigation Fees, In-lieu Park Fees, Drainage Facilities Fees, Water Assessment Fees, and other applicable mitigation fees identified in Chapter 9.44 of the Garden Grove Municipal Code, along with all other applicable fees duly adopted by the City. The amount of said fees shall be calculated based on the City's current fee schedule at the time of permit issuance.
27. A separate street permit is required for work performed within the public right-of-way.
28. Grading fees shall be calculated based on the current fee schedule at the time of permit issuance.
29. The applicant shall identify a temporary parking site(s) for construction crew and construction trailers office staff prior to issuance of a grading permit. No

construction parking is allowed on local streets. Construction vehicles should be parked off traveled roadways in a designated parking area. Parking areas, whether on-site or off-site, shall be included and covered by the erosion control plans.

30. Prior to issuance of a grading permit, the applicant shall submit and obtain City approval of a work-site traffic control plan for all the proposed improvements within public right-of-way. The plan shall be subject to the review and approval of the City Traffic Engineer.
31. Chapter 9.48 of the Garden Grove Municipal Code (Undergrounding of Utilities) applies to the project. In accordance therewith, the applicant is required to underground all existing and proposed on-site utility facilities and off-site utility facilities serving the property which the applicant is developing or redeveloping and which (1) are located within the public right-of-way adjacent or peripheral to the property or project, or (2) are otherwise providing service to the property or project. All existing improvements and utilities shall be shown as part of the grading submittal package in the topography section.

#### Project Construction/Operation

32. The applicant shall coordinate with City's Public Works Department (Engineering, Water Services and Streets Division) and set up appointments for preconstruction inspections for all the on-site and off-site improvements prior to commencement of grading operation and mobilization.
33. In accordance with the Orange County Storm Water Program manual, the applicant and/or its contractors shall provide dumpsters on-site during construction unless an Encroachment Permit is obtained for placement in street.
34. The applicant and its contractors shall be responsible for protecting all existing horizontal and vertical survey controls, monuments, ties (centerline and corner) and benchmarks located within the limits of the project. If any of the above require removal, relocation or resetting, the Contractor shall, prior to any construction work, and under the supervision of a California licensed Land Surveyor, establish sufficient temporary ties and benchmarks to enable the points to be reset after completion of construction. Any ties, monuments and benchmarks disturbed during construction shall be reset per Orange County Surveyor Standards after construction. Applicant and its contractors shall also re-set the tie monuments where curb or curb ramps are removed and replaced or new ramps are installed. The Applicant and its contractors shall be liable for, at applicant's expense, any resurvey required due to the negligence of applicant or applicant's contractors or agents in protecting existing ties, monuments, benchmarks or any such horizontal and vertical controls. Temporary Benchmarks shall not be used for Vertical control. Benchmarks shall be to the National Geodetic Vertical Datum (NGVD).

35. Heavy construction truck traffic and hauling trips, and any required lane closures, shall occur outside peak travel periods. Peak travel periods are considered to be from 7 a.m. to 9 a.m. and 4 p.m. to 6 p.m.
36. Prior to grading or building permit closeout and/or the issuance of a certificate of use or a certificate of occupancy, the applicant shall:
  - a. Demonstrate that all structural best management practices (BMPs) described in the Project WQMP have been constructed and installed in conformance with approved plans and specifications.
  - b. Demonstrate that the applicant is prepared to implement and maintain all non-structural BMPs described in the Project WQMP.
  - c. Demonstrate that an adequate number of copies of the approved Project WQMP are available on-site.
  - d. Submit for review and approval by the City an Operations and Maintenance (O&M) Plan for all structural BMPs.

#### **Public Works Environmental**

37. The applicant shall replace any street trees along Beverly Lane and Enoe Place that were removed as a result of the replacement of the sidewalk and/or widening of the tree wells with 24-inch box sized "Platanus Ornamentalis" or "Platanus Orientalis" trees.

#### **Public Works Water and Sewer Services Division**

38. It is the applicant's responsibility to field verify the actual location of all utilities. Applicant may request copies of as-built drawings and atlas maps from the City or access the digital files on the City's web site.
39. New water service installations 2" and smaller, shall be installed by the City of Garden Grove at owner's/developer's expense. Installation shall be scheduled upon payment of applicable fees, unless otherwise noted. Fire services and larger water services 3" and larger shall be installed by applicant's contractor per City Standards.
40. Water meters shall be located within the City right-of-way and shall be located on the same street as the assigned address for the residence. Fire services and large water services 3" and larger shall be installed by applicant's contractor possessing a Class A or C-34 license, per City water standards and inspected by approved Public Works inspection.

41. If a separate irrigation connection is proposed, a Reduced Pressure Principle Device (RPPD) backflow prevention device shall be installed for meter protection. Installation shall be per City Standards and shall be tested by a certified backflow device tester immediately after installation. Cross-connection inspector shall be notified for inspection after the installation is completed. Property owner shall have RPPD device tested once a year thereafter by a certified backflow device tester and the test results shall be submitted to Public Works, Water Services Division. Property owner must open a water account upon installation of RPPD device.
42. A composite utility site plan shall be part of the water plan approval.
43. There shall be a minimum 15-foot clearance of building footings from water main. Clearances less than 15 feet shall be reviewed and approved by Water Engineering.
44. New utilities shall have a minimum 5-foot horizontal and a minimum 1-foot vertical clearance from water main and appurtenances.
45. Any new or existing water valve located within new concrete driveway or sidewalk construction shall be reconstructed per City Standard B-753.
46. City shall determine if existing water services(s) is/are usable and meets current City Standards. Any existing meter and service located within new driveway(s) shall be relocated at applicant's expense.
47. Water meters and boxes shall be installed by City forces upon payment of applicable fees and after new water system (including water services) pass all bacteriological and pressure tests.
48. No permanent structures, trees or deep-rooted plants shall be placed over sewer main or water main.
49. Location and number of fire hydrants shall be as required by Water Services Division and the Orange County Fire Authority (OCFA).
50. Applicant shall install new sewer lateral with clean out at right-of-way line. Lateral in public right-of-way shall be 4" min. dia., extra strength VCP with wedgelock joints.
51. Applicant shall abandon any existing unused sewer lateral(s) at street right-of-way on the property owner's side. The sewer pipe shall be capped with an expansion sewer plug and encased in concrete. Only one sewer connection per lot is allowed.

52. All perpendicular crossings of the sewer, including laterals, shall maintain a vertical separation of a minimum of 12" below the water main, outer diameter to outer diameter. All exceptions to the above require a variance from the State Water Resources Control Board.
53. If water main is exposed during installation of sewer lateral, a 20-foot section of the water main shall be replaced with a 20-foot PVC C-900 DR-14 Class 305 water pipe, size in kind and centered at the crossing.

**Orange County Fire Authority**

54. The applicant shall comply with all applicable Orange County Fire Authority (OCFA) requirements, including but not limited to the Fire Master Plan.

**Community and Economic Development Department**

**Building and Safety Division**

55. All works shall comply with the latest California Building Standards Code (CBC) at time of building permit application, including but not limited to:
  - a. Soil report complying with CBC Chapter 18 is required at time of permit application.
  - b. Solar ready shall be provided for each unit per Energy Standards.
  - c. Future E.V, charging shall be provided in each garage.
  - d. At least ten percent (10%), but not less than one (1) unit, shall comply with CBC Section 1102A.3.
  - e. Fire/sound separations shall be provided between units and shall comply with CBC Chapter 7. Plans when submitted for plan checks shall include complete construction details and tested/rated assemblies.
  - f. Openings/through penetrations/membrane penetrations shall be protected and shall comply with CBC Section 714. Applicant shall provide complete details and rated assemblies.
56. Plans when submitting for construction permit shall be reviewed and stamped by a registered design professional for compliance with accessibilities, exiting/egress and fire/sound rated constructions.

### **Planning Services Division**

57. The applicant shall submit detailed plans showing the proposed location of utilities and mechanical equipment to the Community and Economic Development Department, Planning Services Division for review and approval prior to submitting plans into the Building and Safety Division Plan Check process. The project shall also be subject to the following:
- a. All on-site and off-site utilities (off-site refers to the areas within public right-of-way to the center line of the streets adjacent to the subject property) within the perimeter of the site and to the centerline of the adjacent streets shall be installed or relocated underground. All on-site and off-site utilities pertaining to the improvements proposed under this Site Plan shall be installed or relocated underground.
  - b. All above-ground utility equipment (e.g. electrical, gas, telephone, cable TV) shall not be located in the street setback, within the common areas, or any parking areas, and shall be screened to the satisfaction of the Community and Economic Development Director.
  - c. No roof-mounted mechanical equipment, including, but not limited to, dish antennas, shall be permitted unless a method of screening complementary to the architecture of the building is approved by the Community and Economic Development Department prior to the issuance of building permits. Screening shall block visibility of any roof-mounted mechanical equipment from view of public streets and surrounding properties.
  - d. All ground or wall-mounted mechanical equipment shall be screened from view from any place on or off the site.
  - e. No exterior piping, plumbing, or mechanical ductwork shall be permitted on any exterior façade and/or be visible from any public right-of-way or adjoining property. Roof rain gutters are permitted. The rain gutters shall follow the natural architecture lines of the building.
58. All landscaping shall be consistent with the landscape requirements of the Landscape Water Efficiency Guidelines (Appendix A), per Title 9 of the Municipal Code. The applicant shall submit a separate and complete Water Efficient Landscape Plan for each property. The water efficient landscape submittals shall include landscape plans, irrigation plans, soils report, grading plans, and all other applicable documentation. The landscape plans shall include type, size, location, and quantity of all plant material. The landscape plans are also subject to the following:

- a. A complete, permanent, automatic remote control irrigation system shall be provided for all landscaping areas shown on the plan. The sprinklers shall be of drip or micro-spray system sprinkler heads for water conservation.
- b. The plans shall provide a mixture of a minimum of ten percent (10%) of the trees at 48-inch box, ten percent (10%) of the trees at 36-inch box, fifteen percent (15%) of the trees at 24-inch box, and sixty percent (60%) of the trees at 15-gallon, the remaining five percent (5%) may be of any size. These trees shall be incorporated into the landscaped frontages of all streets. Where clinging vines are considered for covering walls, drought tolerant vines shall be used.
- c. No trees shall be planted closer than five feet (5'-0") from any public right-of-way. Trees planted within ten feet (10'-0") of any public right-of-way shall be planted in a root barrier shield. All landscaping along street frontages adjacent to driveways shall be of the low-height variety to ensure safe sight clearance. All trees planted on the subject property, whether for screening the project from the neighboring lots or for aesthetic or selling/marketing purposes, shall have an irrigation system installed in order maintain the trees.
- d. The landscape treatments along Beverly Lane and Eneo Place, and the area designated as public rights-of-way, shall incorporate a mixture of ground cover, flowerbeds, and shrubs. The height of the plant material, and any fences located within the front setback areas shall not exceed three feet (3'-0") in height, in order to ensure visibility to the site from the public rights-of-way.
- e. Landscape treatments and irrigation shall be installed within the front, side, and rear setback areas of both properties. The landscaping shall incorporate a mixture of ground cover, flowerbeds, shrubs, and trees. The Community and Economic Development Department shall review the type and location of all proposed trees.
- f. The applicant shall be responsible for all installation and permanent maintenance of all landscaping on the properties. Said responsibility shall extend to the parkway landscaping, sidewalk, curb, and pavement of the site. All planting areas are to be kept free of weeds, debris, and graffiti.
- g. All above-ground utilities (e.g. water backflow devices, electrical transformers, irrigation equipment) shall be shown on the landscaping plan in order to ensure proper landscape screening will be provided.
- h. The landscape plans shall incorporate and maintain, for the life of the project, means and methods to address water run-off, also identified as Low Impact Development (LID) provisions, which address water run-off. This is

to also be inclusive of any application of Water Quality Management Plans (WQMP), Drainage Area Management Plans (DAMP), and any other water conservation measures applicable to this type of development.

- i. The applicant or the property owner shall be responsible for installing and maintaining the landscaping
59. Hours and days of construction and grading shall be as set forth in the City of Garden Grove Municipal Code Chapter 8.47 as adopted, except that:
- a. Monday through Saturday - not before 7 a.m. and not after 8 p.m. (of the same day).
  - b. Sunday and Federal Holidays may work same hours, but subject to noise restrictions as stipulated in Chapter 8.47 of the Municipal Code.
60. Construction activities shall adhere to SCAQMD Rule 403 (Fugitive Dust) that includes dust minimization measures, the use of electricity from power poles rather than diesel or gasoline powered generators, and the use methanol, natural gas, propane or butane vehicles instead of gasoline or diesel powered equipment, where feasible. Also, use of solar, low emission water heaters, and low sodium parking lot lights, shall be required to ensure compliance with Title 24.
61. During construction, if paleontological or archaeological resources are found, all attempts will be made to preserve in place or leave in an undisturbed state in compliance with applicable law. In the event that fossil specimens or cultural resources are encountered on the site during construction and cannot be preserved in place, the applicant shall contact and retain, at applicant's expense, a qualified paleontologist or archaeologist, as applicable, acceptable to the City to evaluate and determine appropriate treatment for the specimen or resource, and work in the vicinity of the discovery shall halt until appropriate assessment and treatment of the specimen or resource is determined by the paleontologist or archeologist (work can continue elsewhere on the project site). Any mitigation, monitoring, collection, and specimen/resource treatment measures recommended by the paleontologist/archaeologist shall be implemented by the applicant at its own cost.
62. Any and all correction notice(s) generated through the plan check and/or inspection process is/are hereby incorporated by reference as conditions of approval and shall be fully complied with by the owner, applicant, and all agents thereof.
63. The development is subject to the following stipulations:
- a. Each property shall maintain the ability to park the required number of vehicles within the required garages at all times. Unless otherwise required



- to be permitted by state law, the enclosed garages shall not be converted to any other use.
- b. There shall be no business activities, day care, or garage sales conducted within or from the enclosed garages.
  - c. Each unit shall have phone jacks and cable-TV outlets in all rooms, with the exception of the laundry area, hallways, and bathrooms.
  - d. Garages shall not be rented or leased separately from the dwelling units and shall not be made unavailable to the occupants of the units.
  - e. Residents shall not park or store vehicles anywhere on the site except within the garage of their dwelling unit. However, the parking spaces in front of the garage doors may be utilized by the residents and guests for temporary parking.
  - f. Trash containers shall be stored within designated storage areas only and not within the garage parking area. The placement of trash containers for pick-up, and the duration of time prior to and after trash collection of those trash containers, is subject to the Garden Grove Sanitary District requirements. The applicant shall provide each individual unit with a trash storage area to accommodate at least three trash containers. The area for each container shall be a minimum of thirty-eight inches (3'-2") by thirty-eight inches (3'-2"). The trash areas shall be paved and accessed by gates and a walkway for ease of taking trash containers to and from the street.
  - g. Each residence shall be utilized as one (1) dwelling unit. No portion of any residence shall be utilized or rented as a separate dwelling unit. This condition shall not be interpreted to prohibit an accessory dwelling unit, or junior accessory dwelling unit, constructed and permitted in accordance with applicable law.
  - h. Fences and walls located within the front yard areas, or adjacent to driveways shall not exceed thirty-six inches (3'-0") in height. Wood fencing located adjacent to any street, parking area, or driveway is prohibited. The developer shall work with the Community and Economic Development Department in order to ensure proper vision clearance for cars entering or leaving the driveway and parking areas. The existing fences located along the front setback areas of both lots shall be removed.
  - i. Any addition to the residence, including the construction of any accessory structures, shall comply with the R-1 zone development standards. Room additions and accessory structures shall maintain consistency with the architecture of the house, including building material, design, and roof pitch.

- j. The facades of the units shall be designed with sound attenuation features including the use of dual paned windows. These features shall be approved by the Community and Economic Development Department prior to the issuance of building permits.
  - k. Mechanical equipment, including air conditioning units, Jacuzzi spa equipment, sump pump, etc., shall not be located closer than three feet (3'-0") to any property line. The equipment shall only be located in an enclosed rear or side yard, and only if the above distance stipulation is met. If units will not have an air conditioning condensing unit installed, a place shall be designated for the location of such on the property that allows for a ground-mounted unit, minimizes noise intrusion to adjacent units, and allows for ease of installation per the distance separation requirements noted immediately above. No wall or window mounted or roof type of air conditioning system is permitted.
  - l. All lighting structures/fixtures shall be placed so as to confine direct rays to the subject property. All exterior lights shall be reviewed and approved by the Planning Services Division. Lighting adjacent to residential properties shall be restricted to low, decorative type, wall-mounted lights, or a ground lighting system. Lighting in the common areas shall be directed, positioned, or shielded in such a manner so as not to unreasonably illuminate the window area of nearby residents.
  - m. At no time shall any structure, fireplace, architectural feature, or otherwise, be closer than three feet to any property line. Any roof eaves or similar roof overhangs intruding into the three-foot setback requirement, shall comply with the U.B.C. concerning method of construction.
  - n. The second floor windows shall, to the extent feasible, be oriented away from the existing single-family homes and/or incorporate view-obscuring measures such as the use of high windows, window alignment, and obscure glass window glazing.
64. As part of the submittal drawings for plan check, the Developer shall submit detailed drawings showing the exterior of all buildings, architectural details, and window and door treatments. The plans shall indicate landscape materials, wall materials, and building materials proposed for the project. Trim shall be provided around all windows and doors. A detail of the trim shall be provided in the construction drawings. The garage doors shall be automatic roll-up type doors. The materials and colors shall be complimentary between the two units.
65. Final Parcel Map No. PM-2021-112 shall be approved by the City and shall be recorded by the applicant prior to issuance of building permits for the proposed single-family homes.

66. A minimum six foot (6'-0"), not to exceed seven feet (7'-0"), high block wall shall be maintained along the perimeter property lines of each parcel at all times, excluding the front setback area, as measured from the on-site finished grade. All existing block walls shall be modified, as necessary, to comply with the minimum block wall height requirement. The type, texture, and color of the block wall shall be approved by the Planning Services Division, and shall match any existing block wall that will remain.
67. The applicant/property owner shall submit signed letters acknowledging receipt of the decision approving Site Plan No. SP-112-2022 and Tentative Parcel Map No. PM-2021-112, and his/her agreement with all conditions of approval.
68. The applicant shall, as a condition of project approval, at its sole expense, defend, indemnify and hold harmless the City, its officers, employees, agents and consultants from any claim, action, or proceeding against the City, its officers, agents, employees and/or consultants, which action seeks to set aside, void, annul or otherwise challenge any approval by the City Council, Planning Commission, or other City decision-making body, or City staff action concerning Site Plan No. SP-112-2022 and Tentative Parcel Map No. PM-2021-112. The applicant shall pay the City's defense costs, including attorney fees and all other litigation related expenses, and shall reimburse the City for court costs, which the City may be required to pay as a result of such defense. The applicant shall further pay any adverse financial award, which may issue against the City including, but not limited to, any award of attorney fees to a party challenging such project approval. The City shall retain the right to select its counsel of choice in any action referred to herein.
69. In accordance with Garden Grove Municipal Code Sections 9.32.160 and 9.40.070.A, respectively, the rights granted pursuant to Site Plan No. SP-112-2022 and Tentative Parcel Map No. PM-2021-112 shall be valid for a period of two (2) years from the effective date of this approval. Unless a time extension is granted pursuant to Section 9.32.030.D.9 of the Municipal Code, the rights conferred by Site Plan No. SP-112-2022 shall become null and void if the subject development and construction necessary and incidental thereto is not commenced within two (2) years of the expiration of the appeal period and thereafter diligently advanced until completion of the project. In the event construction of the project is commenced, but not diligently advanced until completion, the rights granted pursuant to Site Plan No. SP-112-2022 shall expire if the building permits for the project expire.
70. The Conditions of Approval set forth herein include certain development impact fees and other exactions. Pursuant to Government Code §66020(d), these Conditions of Approval constitute written notice of the amount of such fees. The applicant is hereby notified that the 90-day protest period, commencing from the effective date of approval of Site Plan No. SP-112-2022 and Tentative Parcel Map No. PM-2021-112, has begun.

71. The applicant shall comply with the Migratory Bird Treaty Act (MBTA), and Sections 3503, 3503.5 and 3515 of the California Fish and Game Code, which require the protection of active nests of all bird species, prior to the removal of any on-site landscaping, including the removal of existing trees.
72. Because the existing single-family residential dwelling unit on the subject site will be demolished in order to construct the proposed project, this housing unit was occupied by tenants at the time of application submittal, and the household income level of the last renter household in occupancy is unknown and/or cannot be verified by the applicant, the provisions of paragraph (2) of subdivision (d) of Government Code section 66300 pertaining to "protected units" apply to the proposed housing development. Therefore, unless the California Department of Housing and Community Development (HCD) verifies that these provisions do not apply to this project, the Applicant shall comply with all applicable requirements of Section 66300(d) and related City requirements, which include the following:
  - a. Number of Total Units Required. The project shall include at least as many total dwelling units as the greatest number of permitted dwelling units that existed on the project site within the five-year period preceding the application submittal date.
  - b. Number of Replacement Protected Units Required. The project shall replace all previously demolished protected units and all existing protected units that will be demolished as part of the project. All replacement protected unit calculations resulting in fractional units shall be rounded up to the next whole number.
  - c. Projects Involving Demolition of Occupied Protected Units. If any existing protected units to be demolished are occupied on the date of application submittal, the housing development project shall provide at least the same number of replacement dwelling units of equivalent size to be made available at affordable rent or affordable housing cost to, and occupied by, persons and families in the same or lower income category as those households in occupancy. If a project site containing occupied protected units to be demolished also contains unoccupied protected units that will be demolished as part of the project, or previously contained protected units that were demolished within the five-year period preceding the application submittal date, the housing development project shall also provide at least the same number of dwelling units of equivalent size to be made available at affordable rent or affordable housing cost to, and occupied by, persons and families in the same or lower income category as the last household in occupancy. If the income of the existing or last household in occupancy of any protected units is not known, it shall be rebuttably presumed that lower income renter households occupied these protected units in the same

proportion of lower income renter households to all renter households within the City of Garden Grove, as determined by the most recently available data from the United States Department of Housing and Urban Development's Comprehensive Housing Affordability Strategy database, and replacement protected dwelling units shall be provided in that same percentage.

- d. Projects Only Involving Vacated or Demolished Protected Units. If all protected units on the site of a housing development project are vacant or have been demolished within the five-year period preceding the application submittal date, the housing development project shall provide at least the same number of dwelling units of equivalent size as existed at the highpoint of those units in the five-year period preceding the application submittal date to be made available at affordable rent or affordable housing cost to, and occupied by, persons and families in the same or lower income category as those persons and families in occupancy at that time, if known. If the incomes of the persons and families in occupancy at the highpoint is not known, it shall be rebuttably presumed that low-income and very low income renter households occupied these protected units in the same proportion of low-income and very low income renter households to all renter households within the City of Garden Grove, as determined by the most recently available data from the United States Department of Housing and Urban Development's Comprehensive Housing Affordability Strategy database, and replacement protected dwelling units shall be provided in that same percentage.
- e. Single-Family Projects Involving a Single Protected Unit. Notwithstanding subsections c and d, above, if a housing development project consists of a single residential unit on a site with a single protected unit, that protected unit may be replaced with a unit of any size at any income level.
- f. Location of Protected Units. Except as provided in the next sentence, dwelling units required to replace protected units shall be located on the same parcel or parcels as other units in the proposed new housing development project. Notwithstanding the preceding sentence, subject to approval of the Community and Economic Development Director, an applicant may locate a replacement unit on a different parcel in the City zoned for residential use, provided that (i) the development of units on different parcels is proposed as part of, and in the same, application as the remainder of the housing development project, (ii) the other parcel is zoned for residential use and all objective general plan, zoning, and other standards and requirements are met, and (iii) the applicant demonstrates that no residential tenants on the other parcel have been or will be displaced as a result of development of the replacement unit.
- g. Timing of Construction of Replacement Units. All replacement units shall be constructed concurrently with or prior to other units in the housing

development project. The City shall not issue a certificate of occupancy for any other units in a housing development project until certificates of occupancy have been issued for the replacement unit(s).

- h. Affordability. If a replacement unit will be a rental dwelling unit, it shall be subject to a recorded affordability restriction for at least 55 years. If a replacement unit will be a for-sale unit, the provisions of paragraph (2) of subdivision (c) of Government Code Section 65915 shall apply, and the applicant shall either (i) agree to ensure that the replacement unit is initially occupied by a person or family of very low or low income, and it is offered at an affordable housing cost, as that cost is defined in Section 50052.5 of the Health and Safety Code, and is subject to an equity sharing agreement with the City, or (ii) ensure that replacement unit is sold to a qualified nonprofit housing corporation.
- i. Regulatory Agreement Required. The record owner(s) of the property shall enter into an affordable housing regulatory agreement with City. The regulatory agreement shall be approved by the City and recorded prior to issuance of a building permit for any structure in the housing development. The regulatory agreement shall run with the land and bind all future owners and successors in interest. Unless otherwise authorized by the City Manager, the regulatory agreement shall include provisions addressing or requiring the following:
  - i. Identification of Affordable Units. The number, affordability level, unit size mix, and location requirements for the affordable units shall be set forth in the regulatory agreement.
  - ii. Timing of Construction. The regulatory agreement shall require that the affordable units be constructed concurrently with or prior to other units in the housing development project.
  - iii. Affordability Period for Affordable Units. The regulatory agreement shall require that the affordable units remain affordable to, and be occupied by, persons and families of the required income level at an affordable rent or affordable housing cost, as applicable, for the minimum period of time required by law.
  - iv. Annual Tenant Income Verification and Certification. For projects containing rental affordable units, the regulatory agreement shall include uniform provisions requiring the owner to annually obtain written certifications from, and to verify that, each tenant household occupying an affordable unit meets the applicable income and eligibility requirements established for the affordable unit.

- v. Eligibility of Initial Buyers of For-Sale Affordable Units. For projects containing for-sale affordable units, the regulatory agreement shall include uniform provisions requiring the owner to verify that the initial buyer(s) of each affordable unit be of the applicable income level and shall require the initial buyer(s) of each affordable unit to occupy the affordable unit at all times until resale of the affordable unit.
- vi. Equity Sharing Provisions. For projects containing for-sale affordable units, the regulatory agreement shall contain provisions establishing an equity sharing arrangement consistent with the provisions set forth in Subsection (c)(2) of Government Code Section 65915. Upon resale, the seller of the unit shall retain the value of any improvements, the down payment, and the seller's proportionate share of appreciation. The City shall recapture any initial subsidy and its proportionate share of appreciation
- vii. Annual Compliance Report. Each regulatory agreement shall contain provisions requiring the owner to submit an annual compliance report containing specified information to the City in a form reasonably satisfactory to City Manager and to annually certify that the affordable units are in compliance with the requirements of the regulatory agreement.
- viii. Maintenance Standards. The regulatory agreement shall contain uniform provisions governing the owner's maintenance obligations and the City's rights in the event the owner fails to adhere to its maintenance obligations.
- ix. Recordkeeping Requirements. The regulatory agreement shall contain uniform provisions requiring the owner to maintain affordable unit sales documents, tenant leases, income certifications, and other books, documents, and records related to the sale or rental of the affordable units and operation of the project for a period of not less than five (5) years after creation of each such record; to allow the City to inspect any such books, documents, or records and to conduct an independent audit or inspection of such records at a location that is reasonably acceptable to the City Manager upon prior written notice; and to permit the City and its authorized agents and representatives to access the property and examine the housing units and to interview tenants and employees for the purpose of verifying compliance with the regulatory agreement.
- x. Marketing Program. The regulatory agreement shall contain provisions requiring the owner (i) to prepare and obtain the City's approval of a marketing program for the leasing or sale of the affordable units prior to the issuance of a certificate of occupancy for any portion of the

project, (ii) to thereafter market the leasing or sale of the affordable units in accordance with the marketing program, and (iii) to provide City with periodic reports with respect to the leasing or sale of the affordable units.

- xi. Management Plan. The regulatory agreement shall contain uniform provisions regarding property management and management responsibilities and shall require the owner to prepare and obtain the City's approval of a management plan for the project prior to the issuance of a certificate of occupancy for any portion of the project, which sets forth in detail the owner's property management duties, including, but not limited to, a plan to manage and maintain the site and the affordable units; procedures for the selection of tenants of rental affordable units, including a description of how the owner plans to certify the eligibility of tenant households; procedures for annually verifying income and recertifying the eligibility of tenants of rental affordable units; the standard form(s) of rental agreement(s) the owner proposes to enter into with tenants of affordable units; procedures for the collection of rent; procedures for eviction of tenants; procedures for ensuring that the required number and unit size mix of rental affordable units in is maintained and that "floating" affordable units do not become congregated to a certain area of the building or project; the owner's procedures for complying with its monitoring and recordkeeping obligations; the rules and regulations of the property and manner of enforcement; a security system and crime prevention program.
- xii. Provisions regarding Section 8 Certificates. For projects containing rental affordable units, the regulatory agreement shall include uniform provisions regarding the acceptance of federal certificates for rent subsidies pursuant to the existing program under Section 8 of the United States Housing Act of 1937, or its successor (i.e., "Section 8 certificates"), which shall include the following requirements and limitations:
  1. The owner shall accept as tenants persons who are recipients Section 8 certificates on the same basis as all other prospective tenants; provided, the owner shall not rent one of the affordable units to a tenant household holding a Section 8 certificate unless none of the housing units not restricted to occupancy by the affordability covenants are available. If the only available housing unit is an affordable unit, the owner shall no longer designate the housing unit rented to a tenant household holding a Section 8 certificate as an affordable unit, shall designate the next-available housing unit as an affordable unit, and shall make available, restrict occupancy to, and rent such newly designated affordable unit



to a qualified tenant at the applicable affordable rent pursuant to the affordability covenants, such that at all times reasonably possible all of the required affordable units shall not be occupied by tenants holding Section 8 certificates.

2. Furthermore, in the event the owner rents an affordable unit to a household holding a federal certificate, the rental agreement (or lease agreement, as applicable) between the owner, as landlord, and the tenant shall expressly provide that monthly rent charged shall be the affordable rent required for the affordable unit (not fair market rent) and that the rent collected directly from such tenant holding a federal certificate shall be not more than the specified percentage of the tenant's actual gross income pursuant to the applicable federal certificate program regulations; i.e., the rent charged to such tenant under the rental agreement shall be the affordable rent chargeable under the affordability covenant and not fair market rent for the area, as would otherwise be permitted under the applicable federal certificate program.
  3. The owner shall not apply selection criteria to Section 8 certificate holders which are more burdensome than criteria applied to any other prospective tenants.
  4. If and to the extent these restrictions conflict with the provisions of Section 8 of the United States Housing Act of 1937 or any rules or regulations promulgated thereunder, the provisions of Section 8 of the United States Housing Act of 1937 and all implementing rules and regulations thereto shall control.
- xiii. Annual Monitoring Fee. Each regulatory agreement shall contain a provision requiring the Owner to reimburse City for the estimated reasonable costs incurred by the City in monitoring the owner's compliance with, and otherwise administering, the regulatory agreement, including, but not limited to, City's review of annual compliance reports and conduct of inspections and/or audits.
- j. Benefits to be Provided to All Occupants of Protected Units.
- i. Right to Remain in Occupancy Pending Demolition. The record owner of an occupied protected unit that will be demolished as part of a housing development project shall allow the existing occupants of the protected unit to occupy the unit until six months before the start of construction activities with proper notice, pursuant to the Relocation Assistance Law. The record owner shall deliver a notice of intent to terminate residency to the Director and to the occupant household.

- ii. Right to Return if Demolition Does Not Proceed. The record owner of an occupied protected unit that will be demolished as part of a housing development project shall allow any existing occupants of the protected unit that are required to leave the unit to return at their prior rental rate if the demolition does not proceed and the property is returned to the rental market. The notice of intent to terminate residency shall include notice of this right. In addition, this right shall be memorialized in a written agreement, covenant, or other document that is enforceable by the occupant(s) of the protected unit, the form of which shall be subject to review and approval by the Director.
- iii. Benefits to be Provided to Occupants of Protected Units that are Lower Income Households.
  - A. Right of First Refusal for a Comparable Unit in New Housing Development Project. The record owner of a protected unit that will be demolished as part of a housing development project shall agree to provide existing occupants of the protected unit to be demolished as part of a housing development project that are lower income households with a right of first refusal for a comparable dwelling unit available in the new housing development affordable to the household at an affordable rent or affordable housing cost. A comparable dwelling unit is a dwelling unit containing the same number of bedrooms as the demolished protected unit, except that where the protected unit is a single-family home that contains four or more bedrooms, a comparable unit is a unit containing three bedrooms. The right of first refusal shall be memorialized in a written agreement, covenant, or other document that is enforceable by the occupant(s) of the protected unit, the form of which shall be subject to review and approval by the Director. The foregoing shall not apply to (i) a housing development project that consists of a single residential unit located on a site where a single protected unit is being demolished, or (ii) units in a housing development in which 100 percent of the units, exclusive of a manager's unit or units, are reserved for lower income households, unless the occupant of the protected unit qualifies for residence in the new development and providing a comparable unit to the occupant would not be precluded due to unit size limitations or other requirements of one or more funding sources of the housing development project.
  - B. Relocation Benefits.
    1. The applicant and/or the record owner of a protected unit that will be demolished as part of a housing development project shall provide existing occupants of the protected unit

to be demolished as part of a housing development project that are lower income households with relocation benefits consistent with the requirements of the Relocation Assistance Law. By way of example, said relocation benefits may include, without limitation, advisory assistance in finding comparable new housing, payment of moving expenses, and rental assistance payments.

2. The applicant shall engage a qualified third-party contractor or consultant (a "relocation consultant") approved by the Director to determine the eligibility of occupants for benefits pursuant to this Section, prepare a relocation plan, and oversee the provision of the required relocation benefits.
3. The applicant's relocation consultant shall prepare a written relocation plan consistent with the provisions of the Relocation Assistance Law, which plan shall be subject to review and approval by the Director. The relocation plan shall include, without limitation, provisions addressing the following: (i) determination of eligibility requirements; (ii) identification of eligible occupants; (iii) occupant interviews and needs assessments; (iv) an evaluation of the availability of comparable replacement housing within the relevant geographic area; (v) identification of specific replacement housing options; (vi) the provision for relocation advisory services to affected occupants; (vii) a description of the relocation benefits available to eligible occupants; (viii) a process for the provision of benefits and the submission of benefit claims by eligible occupants; (ix) a process for occupants to appeal benefit determinations; and (x) procedures for providing the benefits required pursuant to Government Code Section 66300(d), including copies of the required notices, agreements, and other forms needed to implement the provision of said benefits.
4. Prior to the issuance of a grading or building permit for the housing development project, the relocation consultant shall provide a letter to the Director certifying that the relocation process has been completed and that all required relocation benefits have been provided.

# COMMUNITY AND ECONOMIC DEVELOPMENT DEPARTMENT STAFF REPORT

<b>AGENDA ITEM NO.:</b> C.3.	<b>SITE LOCATION:</b> West side of Pala Drive, North of Acacia Avenue, and West side of Industry Street, South of Lampson Avenue, at 12641 Industry Street and 12691 Pala Drive
<b>HEARING DATE:</b> May 19, 2022	<b>GENERAL PLAN:</b> Industrial (I)
<b>CASE NOS.:</b> Site Plan No. SP-110-2022, Variance No. V-036-2022, Lot Line Adjustment No. LLA-029-2022	<b>ZONE:</b> M-P (Industrial Park)
<b>PROPERTY OWNER:</b> Prologis, L.P., ATTN: Eddie Pang	<b>CEQA DETERMINATION:</b> Exempt: Section 15302 – Replacement or Reconstruction
<b>APPLICANT:</b> Same as Owner	<b>APN NOS.:</b> 215-011-11 & 215-111-16

## **REQUEST:**

The applicant is requesting Site Plan approval to build a new 149,335 gross square-foot industrial building, necessitating the demolition of the 65,880 square-foot building at 12641 Industry Street, and the 83,100 square-foot building at 12691 Pala Drive. In conjunction with this request, the applicant is also requesting approval of a Variance request to deviate from the thirty-five foot (35'-0") maximum building height limit of the M-P (Industrial Park) zone by two feet and two inches (2'-2"), and a Lot Line Adjustment request to relocate the property line between 12641 Industry Street and 12691 Pala Drive.

## **BACKGROUND:**

The subject site is comprised of two (2) parcels, located at 12691 Pala Drive, and 12641 Industry Street, totaling 9.02 acres. The property at 12691 Pala Drive (Assessor's Parcel No. 215-011-11), on the west side of Pala Drive, north of Acacia Avenue, is currently improved with an 83,100 square-foot industrial building, originally constructed in 1973. The property at 12641 Industry Street (Assessor's Parcel No. 215-011-16), on the west side of Industry Street, south of Lampson Avenue, is currently improved with a 65,880 square-foot industrial building, and a 36,338 square-foot building (12601 Industry Street), both also constructed in 1973. Both properties have a General Plan Land Use designation of Industrial (I), and are zoned Industrial Park (M-P). The subject site abuts industrial uses on M-P zoned properties to the north, south, west, and east, across Pala Drive and Industry Street.

In 1969, the City considered Lot Split No. LS-102-69 for the subdivision of industrial properties in the M-P zone, south of Lampson Street. While the Lot Split was approved, it was not exercised. In 1973, Lot Split No. LS-109-73 was approved by the City to subdivide property to create the lot at 12601-12641 Industry Street, and to dedicate Industry Street itself. The buildings at 12691 Pala Drive and 12641 Industry Street were approved in 1973, with construction finalizing in 1974. Lot Split

No. LS-111-74 was also approved in 1974, adjusting the lot lines approved under LS-109-73. Lastly, in 1974, Conditional Use Permit No. CUP-107-74 was approved for a freestanding monument sign on the subject Industry Street property.

Both properties have a long history of being occupied by manufacturing uses. Both 12601 and 12641 Industry Street were most recently occupied by the same textile dyeing company until 2019. According to business license records, the building at 12691 Pala Drive was most recently occupied by a garment manufacturing company until early 2021. Consequently, all buildings have remained vacant since.

Now, the applicant is requesting to demolish the 83,100 square foot building at 12691 Pala Drive, and the 65,880 square foot building at 12641 Industry Street. The 36,338 square foot building at 12601 Industry Street will remain. A new 149,335 gross square-foot industrial building will be constructed in virtually the same location as the demolished buildings. A Variance is also requested to allow for the new building to exceed the maximum height permitted in the M-P zone by two feet and two inches (2'-2"). The purpose and intent of the project is to construct a singular, larger, contemporary building with taller interior ceiling heights, meeting the current market demand for industrial facilities. The applicant is also requesting to move the lot line that is currently between the two properties approximately 262'-0" to the north, such that the existing building at 12601 Industry Street would reside on its own parcel. A reciprocal access agreement will be recorded along the new property line to maintain circulation access for both properties.

**PROJECT STATISTICS:**

	<b>New Building</b>	<b>Building to Remain</b>	<b>Municipal Code</b>
<u>Minimum Lot Size</u>	304,049 sq. ft. (6.98 acres)	88,862 sq. ft. (2.04 acres)	15,000 sq. ft. (0.34 acres)
<u>Setbacks</u>			
Front (East)	20'-0"	20'-0"	10'-0"
Rear (West)	44'-0"	83'-0"	0'-0"
Side (North)	40'-0"	65'-0"	0'-0"
Side (South)	76'-3"	35'-2"	0'-0"
<u>Parking</u>	154 spaces	89 spaces	150 spaces (New) 73 spaces (Remaining)
<u>Building Height</u>	*37'-2"	30'-0"	35'-0"
<u>Building Area</u>	149,335 sq. ft.	36,338 sq. ft.	N/A
<u>Floor Area Ratio</u>	0.49	0.41	1.0 Maximum
<u>Landscaping Area</u>	36,534 sq. ft. (11.97%)	8,932 sq. ft. (10.05%)	10%

\*Variance requested under V-036-2022

**DISCUSSION:**

SITE PLAN:

Site Design and Circulation

The project will consist of constructing a 149,335 gross square-foot industrial building on a 6.98-acre property. The industrial building will be located approximately in the center of the site, with parking and landscaping surrounding, and fronting toward both Pala Drive and Industry Street. The entirety of the street frontage along Pala Drive and Industry Street, save for driveway and pedestrian access points, will be landscaped.

The building features two entrances: one on the northeast corner, fronting Industry Street, and one on the southeast corner, fronting Pala Drive. At the Pala Drive entrance will be a 3,000 square-foot office area. Directly above that area is another 3,000 square-foot mezzanine office area. At the Industry Street entrance, on the northern side of the building, is another 2,000 square-foot office area. The remaining 141,335 square feet of the building will consist of open floor area. No tenant has been identified to date. All interior improvements, including any subdivision of the building, will be completed by the tenant(s) at a later date.

	<b>1<sup>st</sup> Floor</b>	<b>Mezzanine</b>
<u>Office</u>	5,000 sq. ft.	3,000 sq. ft.
<u>Industrial Floor</u>	141,335 sq. ft.	

Vehicle traffic can access the site via two (2) new driveways on Pala Drive, or via two (2) new driveways on Industry Street. A two-way drive aisle provides the vehicular circulation on-site, wrapping around all sides of the building, connecting the four (4) driveways, and the truck docking area. The drive aisle also provides reciprocal access to the property directly to the north, at 12601 Industry Street. Standard parking spaces are provided along the north, south, and east sides of the proposed building. To the west, in the rear of the building, parallel parking spaces will be adjacent to the drive aisle. The City's Engineering Division has reviewed the on- and off-site vehicle circulation, and has not raised any concerns with the project design.

A single row of eighteen (18) truck bays will flank the eastern side of the building, between the two main entrances. The docking bays connect directly into the open floor area, toward the center of the building. Outside, a truck turn-around and parking area is provided adjacent to the loading bays. Primary access to the trucking area will be provided from Pala Drive, to the south, but secondary access is also provided from Industry Street.

The design of the building will also provide new pedestrian access from both Pala Drive and Industry Street. The accessible path-of-travel from Pala Drive will cross the drive aisle before reaching the southern entrance of the building. Access from Industry Street will pass through a landscaped area before reaching the north entrance of the building. This pedestrian access also connects to the accessible

parking spaces in the parking lot. Bicycle parking will also be provided near the entrances of the building.

### Parking and Traffic

Parking requirements from Municipal Code Section 9.16.040.150.D. for "Industrial Uses" stipulate one (1) parking space is required per 1,000 square feet of gross floor area for buildings in excess of 100,000 square feet. Incidental offices associated with the industrial use that do not exceed 30% of the gross floor area do not require additional parking.

The proposed building is approximately 149,335 square feet in size. Of that floor area, the office space totals approximately 8,000 square feet, or approximately 5% of the gross floor area. This does not exceed 30% of the gross floor area, and therefore does not require additional parking. In total, 150 parking spaces are required for the use (149,335 sq. ft. / 1,000). The subject site provides 154 striped parking spaces, a surplus of four (4) spaces.

The building at 12601 Industry Street is approximately 36,338 square feet. Parking standards for an industrial use of that size requires two (2) parking spaces per 1,000 square feet of gross floor area. Only 4,791 square feet of the gross floor area is dedicated to office use, or 13.2%. This does not exceed 30% of the gross floor area, and therefore does not require additional parking. In total, 73 parking spaces are required for the use ((36,338 sq. ft. / 1,000) x 2). The subject site provides 89 striped parking spaces, a surplus of sixteen (16) spaces.

### Landscaping

The M-P zone requires a minimum of 10% of the total site area to be dedicated for landscaping. The proposed site design will provide a total of approximately 36,534 square feet of landscaping on-site (11.97% of the overall site). This meets the Code requirement. The landscaping is provided in a variety of areas across the site, including adjacent to the parking areas so as to limit their visual impacts. The on-site landscaping design will consist of a mixture of trees, shrubs, and groundcover.

Furthermore, the M-P zone requires a minimum ten-foot (10'-0") landscaped setback along all non-arterial street frontages, excluding the driveway access points. Neither Industry Street nor Pala Drive have been identified as arterial streets. The entirety of the street frontages along Pala Drive and Industry Street are landscaped, with a minimum dimension of twenty feet (20'-0") wide. Additionally, one (1) tree must be provided for every thirty feet (30'-0") of street frontage. In the parking lot, a minimum of one (1) tree is required for every ten (10) parking spaces. The proposed project complies with both tree planting requirements.

The applicant is required to submit a landscape and irrigation plan to the City that complies with the landscaping requirements of Title 9 of the Municipal Code, including the Landscape Water Efficiency Guidelines. All landscaping shall be watered by means of an automatic irrigation system meeting the City's Landscape Water

Efficiency Guideline requirements. A separate landscape application will be submitted, and a building permit will be obtained for the proposed landscaping.

### Building Architecture

Characterized by a rectangular footprint, flat roof, and large, vertical windows accentuating the corners of the building, the building takes on a contemporary design. The main entrances in the corners of the buildings feature vertical windows and storefronts glazed in green hues. Additional windows will be added above the loading bays, helping illuminate the interior of the building, and helping to add contrast against the concrete walls. A parapet, with a simple decorative coping will encircle the roof on all sides.

At the highest point, the roof stands approximately thirty-seven feet (37'-2") tall, above the maximum allowable building height of thirty-five feet (35'-0") for the M-P zone. As a result, a Variance is requested, and discussed below. The building parapet extends to a maximum height of forty-two feet (42'-0") at the highest point. The Municipal Code allows for architectural features that do not add usable square footage, like a parapet, to extend up to fifteen feet (15'-0") beyond the maximum building height, up to a maximum building height of fifty feet (50'-0").

The building will be constructed in a tilt-up concrete style. Various scores in the concrete walls, and various paint colors add visual intrigue. The neutral color scheme consists of shades of white, grey, and green colors, which contributes to the building's contemporary design. Window and door trim are all constructed of metal, trimmed black to add contrast. The building design and color is similar to other Prologis industrial buildings found nationwide.

### VARIANCE:

The applicant is requesting consideration of a Variance from Section 9.16.040.030 (Industrial—General Requirements) of Title 9 of the Municipal Code for a deviation to the maximum building height permitted in the M-P (Industrial Park) zone. The maximum building height permitted in the M-P zone is thirty-five feet (35'-0"). The topmost portion of the roof, as measured from average finished grade, will be 37'-2." The roof will slope downwards to lower building heights to accommodate roof drainage. The topmost portion of the roof parapet will be 42'-0". The Municipal Code allows for parapets to extend up to fifteen feet (15'-0") beyond the maximum building height, up to fifty feet (50'-0"). Therefore, the parapet does not require a Variance. Only a Variance for the roof height will be considered.

Pursuant Garden Grove Municipal Code Section 9.32.030.D.6, in order to grant a property owner's request for a Variance, the Planning Commission must make each of the following five (5) findings:

1. That there are exceptional or extraordinary circumstances or conditions applicable to the property involved or to the intended use or development of the property that do not apply generally to other property in the same zone or neighborhood.



Approval of this Variance will allow the project to deviate from the maximum building height of thirty-five feet (35'-0") in the M-P (Industrial Park) zone by two feet and two inches (2'-2"), in order to facilitate the construction of the new building. With exception to the requested Variance, the project meets all other Municipal Code development standards such as, but not limited to, building setbacks, parking, and landscaping. There are exceptional or extraordinary circumstances or conditions applicable to the property involved that do not apply generally to other similar properties in the immediate vicinity, within the same zone, or other similarly zoned properties throughout the City.

The subject site is surrounded and constrained by previously developed properties to the north, west, and south. To accommodate the edge conditions created by the presence of these existing developments, the proposed elevations adjacent to the building in the northwest and southeast corners are lower to accommodate the positive flow of stormwater to the existing and proposed drainage inlets. Building height is measured from the average level of the building at grade. By lowering the adjacent grade, the average level of the building was also lowered, creating a taller measurement to the top of the building roof. This drainage condition coupled with the calculation process for determining building height, results in a building height that is measured slightly higher than the existing buildings.

Furthermore, increased building heights lead to more vertical racking, enhanced fire sprinkler systems, and more expansive commodity storage types, which may attract high quality tenants in the community. The current market demand for industrial buildings is calling for minimum interior ceiling height clearances of thirty-two feet (32'-0"), taller than buildings built in the past. Vertical expansion is favored over horizontal, resulting in the need for taller buildings to meet vertical racking space requirements. This provides sufficient space for the minimum standard vertical racking systems, as well as meeting storage capacity needs. The additional building height requested of two feet and two inches (2'-2") provides for five feet and two inches (5'-2") of space to accommodate fire sprinkler suppression system requirements, as well as all necessary structural building requirements.

Lastly, the property is irregularly shaped, with frontages along two culs-de-sac at Pala Drive and Industry Street. Some of the lot area are triangular slivers, and generally unusable space adjacent to the cul-de-sacs. This effectively reduces the usable area of the lot, preventing a larger building footprint. Horizontal expansion, with a larger building footprint is unfeasible. The most feasible option is to add vertical capacity, resulting in a taller building.

2. That such Variance is necessary for the preservation and enjoyment of a substantial property right possessed by other property in the same vicinity and zone, but which is denied to the subject property.

Approval of this Variance will allow the project to deviate from the maximum building height of thirty-five feet (35'-0") in the M-P (Industrial Park) zone by two

feet and two inches (2'-2"), in order to facilitate the construction of the new building. With exception to the requested Variance, the project meets all other Municipal Code development standards such as, but not limited to, building setbacks, parking, and landscaping. Other properties within immediate vicinity of the subject lot, and within other similarly zoned properties throughout the City, have existing similar type buildings that exceed the maximum thirty-five foot (35'-0") building height allowed M-P zone.

Within the vicinity of the subject lot, an industrial building located at 12821 Knott Street, which is a similarly zoned property (Industrial Planned Unit Development), provides a building height of thirty-nine feet (39'-0") to the top of the roof, and forty feet (40'-0") to the top of the parapet, exceeding the thirty-five foot (35'-0") building height requirement found in the M-P zone. Another building at 12752 Monarch Street was granted a Variance (V-033-2021) in May 2021 to allow for a building height of approximately thirty-seven feet (36'-9"). This property is also located in the same neighborhood and zoning district as the subject property.

Accordingly, approval of the proposed Variance will not set a precedent, and will allow the applicant to enjoy a substantial property right possessed by other property owners located in the M-P zone, and other similar zoned properties in the City. With exception to the requested Variance to deviate from the maximum building height limitation, the proposed project complies with all other development standards of the M-P zone.

3. That the granting of a Variance will not be materially detrimental to the public welfare or injurious to the property or improvements in such zone or neighborhood in which the property is located.

Approval of this Variance will allow the project to deviate from the maximum building height of thirty-five feet (35'-0") in the M-P (Industrial Park) zone, in order to facilitate the construction of the new building. The approval of this Variance will allow a 37'-2" building height (to the topmost portion of the roof), which is slightly more than two feet (2'-2") above the maximum building height of thirty-five feet (35'-0") allowed in the M-P zone.

The Variance will not affect surrounding properties, which are developed with existing industrial uses. The subject lot is not within close proximity to sensitive uses (e.g., residential, schools, parks). Other properties within the vicinity of the subject lot, and within other similar zoned properties throughout the City, have existing similar type buildings that exceed the maximum building height of the M-P zone. Nearby, an industrial building located at 12821 Knott Street, which is a similar zoned property (Industrial Planned Unit Development), provides a building height of thirty-nine feet (39'-0") to the top of the roof, and forty feet (40'-0") to the top of the parapet, exceeding the thirty-five foot (35'-0") building height requirement found in the M-P zone. Another building at 12752 Monarch Street was granted a Variance (V-033-2021) in May 2021 to allow for a building height of approximately thirty-seven feet (36'-9").

The project will be required to comply with all applicable building, and life-safety codes and regulations to ensure that there are no adverse impacts on public health, safety, or welfare. Furthermore, the proposal has been reviewed by all City departments in order to ensure compliance with all applicable Municipal Code provisions. Provided the project complies with the Conditions of Approval, the approval of the Variance will not be materially detrimental to the public welfare or injurious to the property or improvements in such zone or neighborhood in which the property is located.

4. That the granting of such Variance will not adversely affect the City's General Plan.

The General Plan does not explicitly impose a building height limitation. Therefore, the proposed building height Variance will not adversely affect the General Plan.

The Industrial (I) General Plan Land Use Designation is intended to create, maintain, and enhance industrial areas characterized by general industrial uses, such as warehousing and distribution or business parks, and more intensive industrial uses, such manufacturing, fabrication, assembly, processing, trucking, warehousing and distribution, and servicing. The proposed building is consistent with the intent of the Industrial Land Use Designation. Specifically, the project meets the intent of the general plan by furthering the following General Plan goals, policies, and implementation programs:

Goal LU-7: The City values its industrial areas as an important contributor to a well-planned community and for the jobs and economic impacts they provide. The proposed project will replace the existing buildings with a contemporary building that meets the needs of the current industrial market by providing increased vertical racking space for additional interior storage capacity. The proposed project will ensure that the City maintains healthy and competitive industrial areas contributing to a well-planned and well-maintained community.

Implementation Program LU-IMP-7B: Require improved maintenance and rehabilitation of industrial buildings and sites, as necessary. The project Conditions of Approval will remain in perpetuity for the life of the development. The Conditions of Approval will help ensure that the property is well-maintained, and will not be detrimental to the surrounding neighborhood in which the property is located. Additionally, the project proposes enhanced exterior elevations which are aesthetically pleasing, which can foster further investments in high quality, contemporary industrial areas around the community.

The proposed project will be consistent with the spirit and intent of the General Plan, furthering its goals, policies, and implementation programs. The project would also help meet the community's need for competitive industrial facilities in its industrial zoned areas. Therefore, the granting of the requested Variance will not adversely affect the City's General Plan.

5. That approval of the Variance is subject to such conditions as will assure that it does not constitute a grant of special privileges inconsistent with the limitations upon other properties in the vicinity and zone in which the subject property is situated.

Provided the Conditions of Approval are adhered to for the life of the project, approval of the subject Variance will not grant a special privilege that is inconsistent with the limitations upon other properties located within the vicinity or zone in which the subject property is situated.

To construct the proposed building, the Variance to deviate from the maximum building height of thirty-five feet (35'-0") allowed in the M-P (Industrial Park) zone is necessary to facilitate the development and improvement of the property, which otherwise meets all other Municipal Code development standards. This includes, but is not limited to, building setbacks, parking, and landscaping. Other properties within the immediate area of the subject lot, and within other similarly zoned properties throughout the City, have existing similar type buildings that exceed the maximum thirty-five foot (35'-0") building height allowed M-P zone.

Within the immediate vicinity of the subject site, an industrial building located at 12821 Knott Street, which is a similarly zoned property (Industrial Planned Unit Development) provides a building height of thirty-nine feet (39'-0") to the top of the roof, and forty feet (40'-0") to the top of the parapet, which exceeds the building height requirement of the M-P zone. A Variance (V-033-2021) was more recently granted for the redevelopment of an industrial building located 12752 Monarch Street, providing for a maximum building height of approximately thirty-seven feet (36'-9").

Accordingly, approval of the proposed Variance will not grant a special privilege that is inconsistent with the limitations upon other property owners located in the Industrial Park and other similar zoned properties in the City. With exception to the requested Variance to deviate from the maximum building height limitation, the proposed project complies with all other development standards of the M-P zone.

The industrial building market has evolved over the past years to necessitate a greater interior ceiling height. Current market demands are calling for a minimum thirty-two foot (32'-0") interior ceiling height to provide increased operations and storage capacity. Increased building heights lead to more vertical racking, enhanced fire sprinkler systems, and more expansive commodity storage types, which attract high quality tenants. Property acquisition in a built-out community to accommodate horizontal expansion is impractical, and the proposed project is otherwise consistent with the standards and intent of the M-P (Industrial Park) zone. Conditions applicable to the subject property and its intended development do not readily apply to other property in similar zones, the same zone, and neighborhood. Accordingly, approval of the proposed Variance will not set a precedent, and will allow the applicant to enjoy a substantial property right possessed by other property owners located in similar properties in the City.

In addition, Condition of Approval No. 84 states, "The rights granted the applicant pursuant to Variance No. V-036-2022 shall continue in effect for only so long as the improvements authorized and contemplated by Site Plan No. SP-110-2022, and these Conditions of Approval (as they may be amended from time to time) continue to exist on the Site. In the event that that Site Plan No. SP-110-2022 is not exercised within one year of approval (or the length of any extension approved by the City), or the improvements authorized and contemplated by Site Plan No. SP-110-2022 are demolished and not re-established within one year of demolition, Variance No. V-036-2022 shall cease to be effective or grant the applicant any rights to construct other improvements inconsistent with the then-currently applicable development standards."

Staff believes the record contains sufficient facts to support approval of the applicant's Variance request. Accordingly, Staff has prepared for the Commission's consideration a draft resolution approving the proposed Variance request, in conjunction with the Site Plan and Lot Line Adjustment requests, subject to the specified Conditions of Approval.

**LOT LINE ADJUSTMENT:**

Currently, the buildings at 12691 Pala Drive, and 12641 Industry Street are conjoined, with no direct access, and separated by a property line. To accommodate the new building, this property line will be moved approximately 262'-0" to the north. The new property line will reside in the drive aisle between the new building, and the existing building at 12601 Industry Street.

As a result of the lot line adjustment, the subject site with the new building will ultimately total 6.98 acres. The property at 12601 Industry Street will ultimately consist of 2.04 acres. Both new properties meet the minimum 15,000 square-foot lot sizes required in the M-P zone. After the Lot Line Adjustment, both the new and existing buildings will comply with the development standards pertaining to the M-P zone, save for the requested Variance.

Lastly, a new driveway and vehicle access aisle will be provided along the new lot line between the two subject parcels. The drive aisle will provide access to both the new building to the south, and the existing building to the north. A reciprocal easement agreement, or equivalent agreement, will be recorded between the two properties to maintain access to the properties in perpetuity. The project has been conditioned as such.

**CALIFORNIA ENVIRONMENTAL QUALITY ACT (CEQA):**

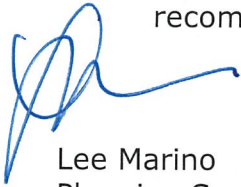
CEQA's Class 2 exemption applies to replacement or reconstruction of existing structures (CEQA Guidelines §15302.). A project can qualify for a Class 2 exemption if a commercial building is replaced with a new structure of substantially the same size, purpose, and capacity (CEQA Guidelines §15302.).

The project involves the demolition of a 65,880 square-foot industrial building at 12641 Industry Street, and an 83,100 square-foot industrial building at 12691 Pala Drive. The two buildings are currently adjoined, and compose a singular 148,980 square-foot structure. The project will replace the existing two buildings with an industrial building with a 146,335 square-foot footprint. By replacing a 148,980 square-foot industrial building with another industrial building of a slightly smaller footprint, the project replaces a commercial building with a new structure of substantially the same size, purpose, and capacity. It can be determined that the project can be exempted from further CEQA action under the Class 2 exemption.

**RECOMMENDATION:**

Staff recommends that the Planning Commission take the following action:

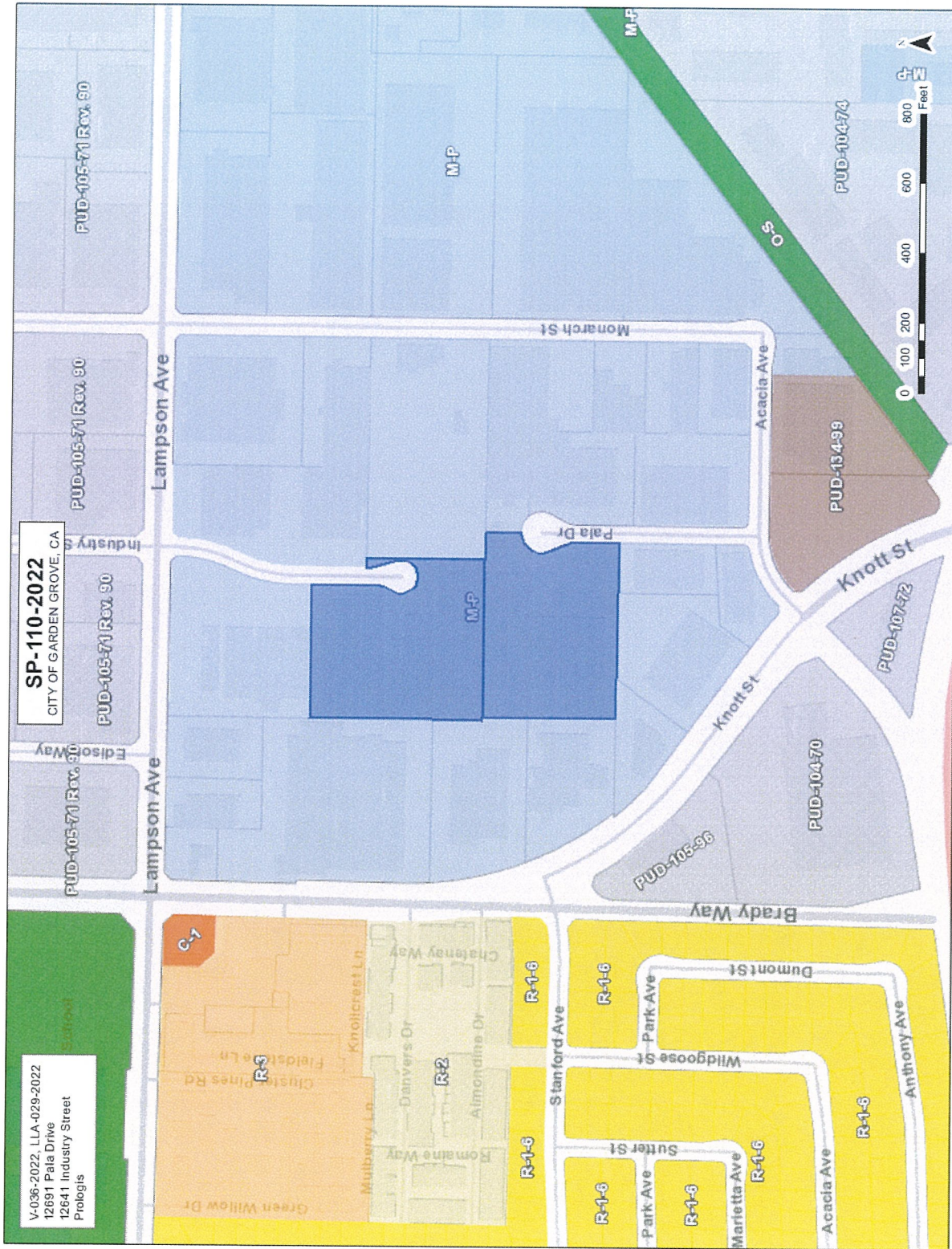
- Adopt Resolution No. 6040-22 approving Site Plan No. SP-110-2022, Variance No. V-036-2022, and Lot Line Adjustment No. LLA-029-2022, subject to the recommended Conditions of Approval.



Lee Marino  
Planning Services Manager




By: Priit Kaskla  
Assistant Planner




V-036-2022, LLA-029-2022  
12691 Pala Drive  
12641 Industry Street  
Prologis

SP-110-2022  
CITY OF GARDEN GROVE, CA



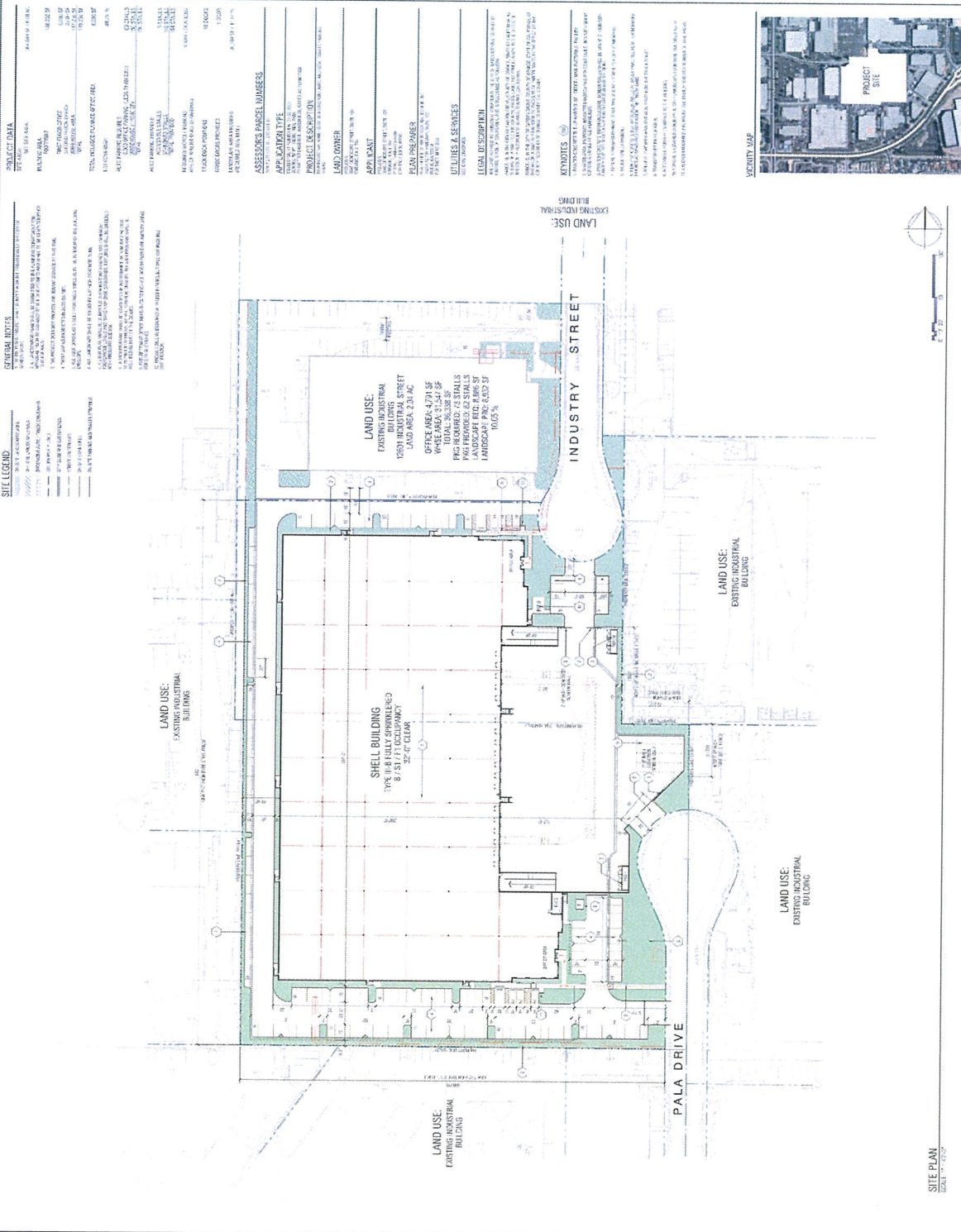
**Office of Architectural Design**  
 1321 Adams Boulevard, Suite 100  
 Torrance, CA 90501  
 310-541-6070  
 310-541-6072

**PALA DRIVE DEVELOPMENT**  
 LAX 11903  
 13011 PALA DRIVE  
 GARDEN GROVE, CA



13011 PALA DRIVE, SUITE 100  
 GARDEN GROVE, CA 92641  
 714-942-2222  
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A1-1P

SITE PLAN  
 SCALE: 1" = 20'





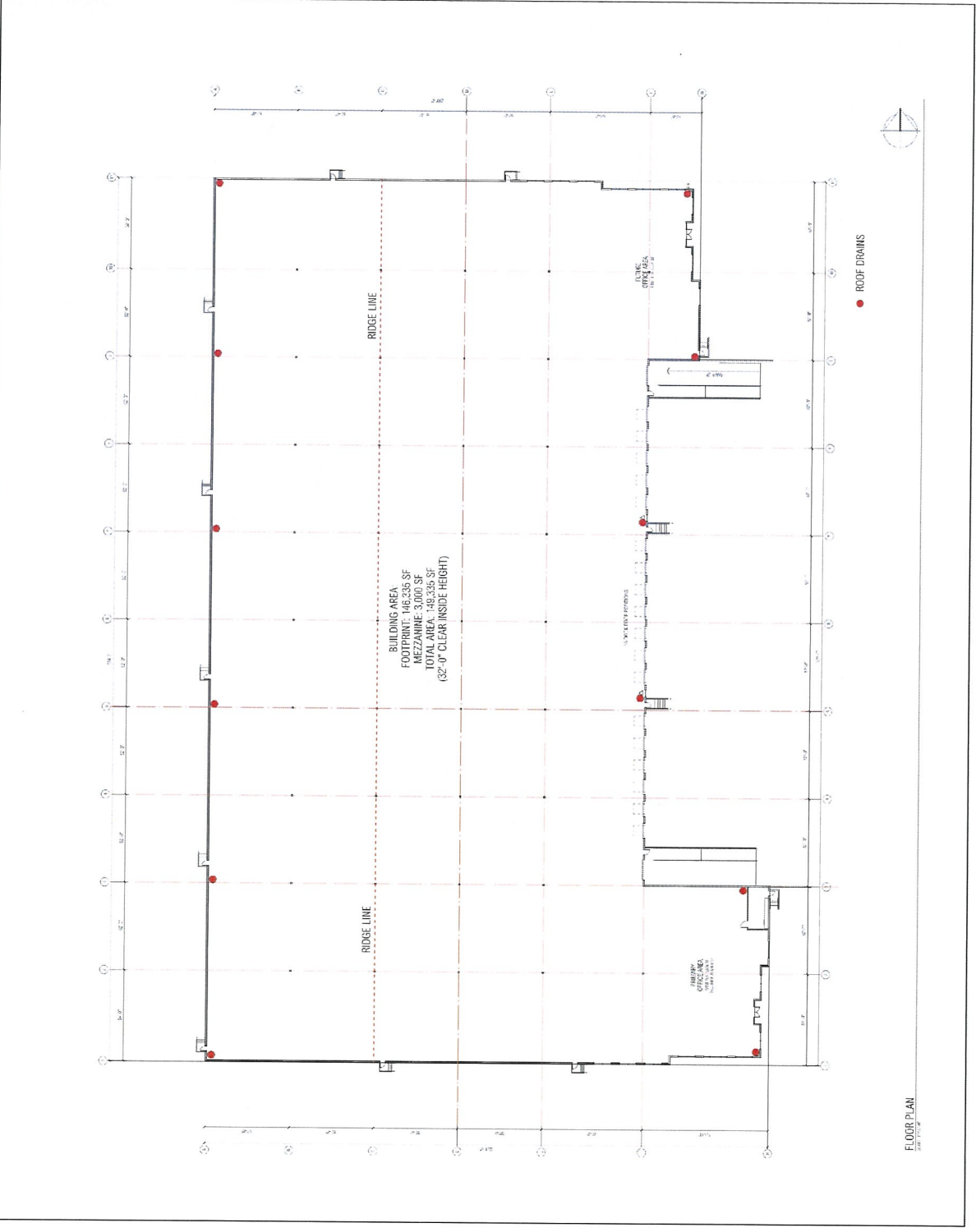
**RGA**  
 Office of Architectural Design  
 12311 Alton Parkway, Suite 100  
 Irvine, CA 92618  
 714.951.6000  
 714.951.6002

PALA DRIVE  
 DEVELOPMENT  
 LAX 11503  
 1201 PALA DRIVE  
 GARDEN GROVE, CA

**PROLOGIS**  
 20000 BLOSSOM STREET  
 SUITE 100  
 GARDEN GROVE, CA 92640  
 714.951.6000

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A2-1P



**RG&A**  
 Office of Architectural Design  
 1521 Piedmont Avenue, Suite 100  
 Berkeley, CA 94704  
 415-863-3100  
 415-863-3102

PALA DRIVE  
 DEVELOPMENT  
 LAX 11503  
 1201 PALA DRIVE  
 GARDEN GROVE, CA

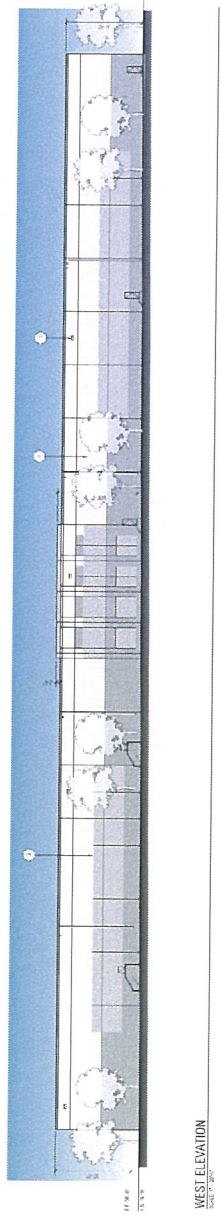
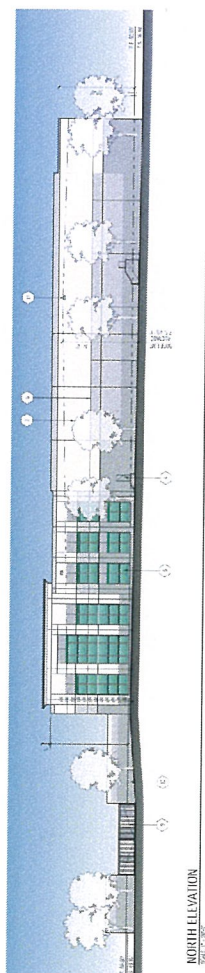
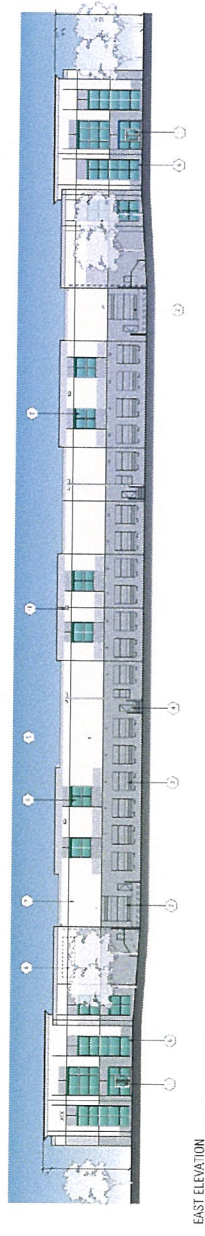
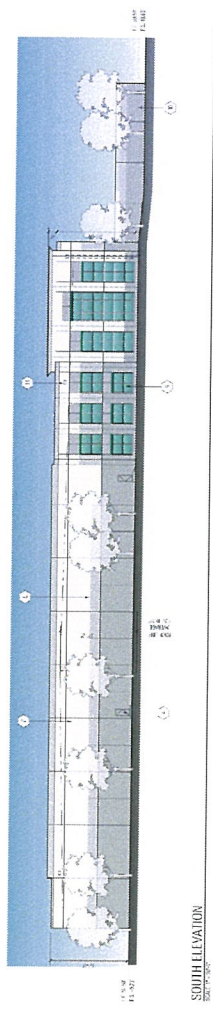
**PROLOGIS**  
 PROLOGIS LOGISTICS  
 20000 SHELTON BLVD  
 FORT WORTH, TX 76155  
 PHONE: 817-424-5131  
 CONTACT: TERRY HANSEN

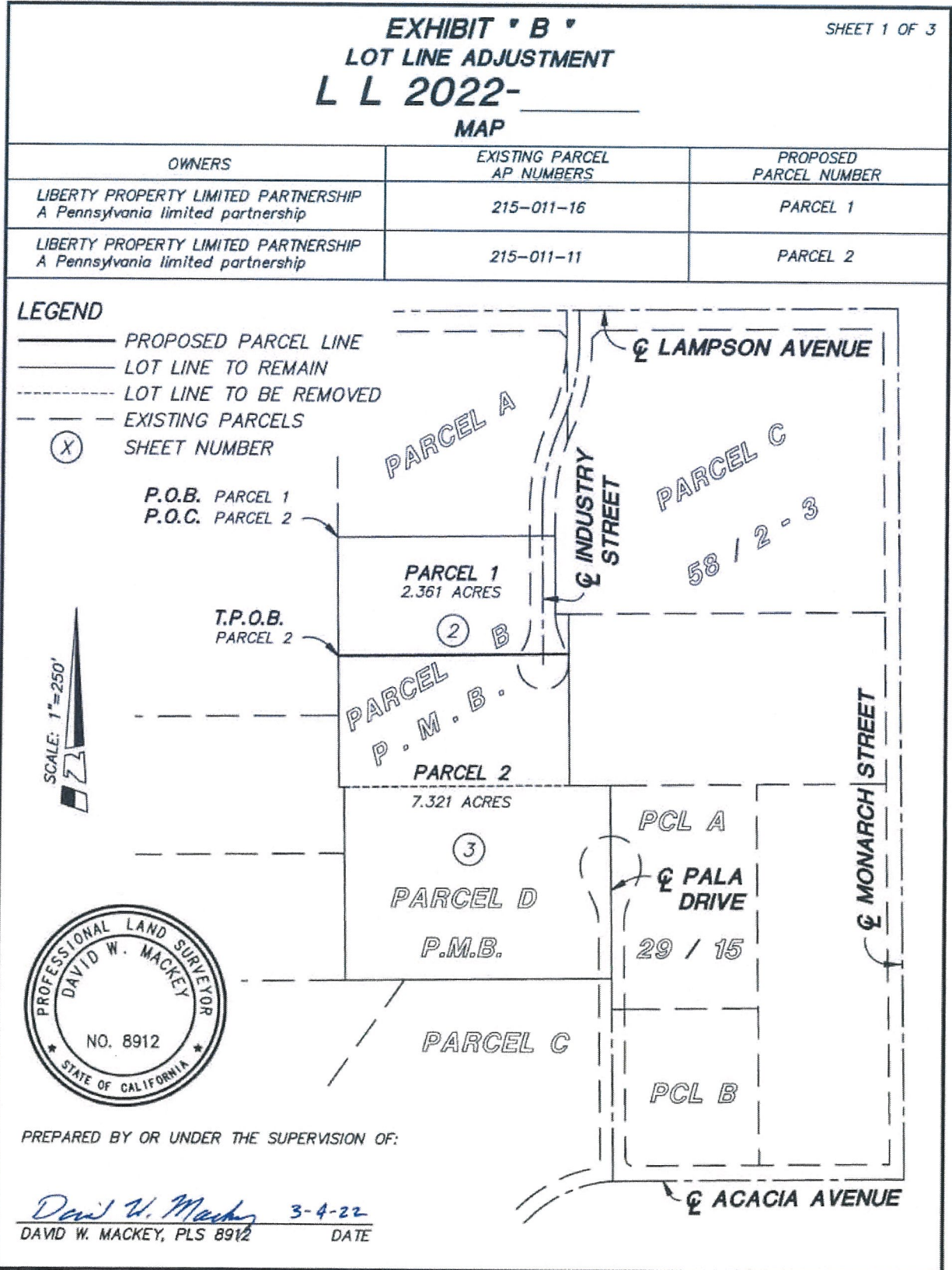
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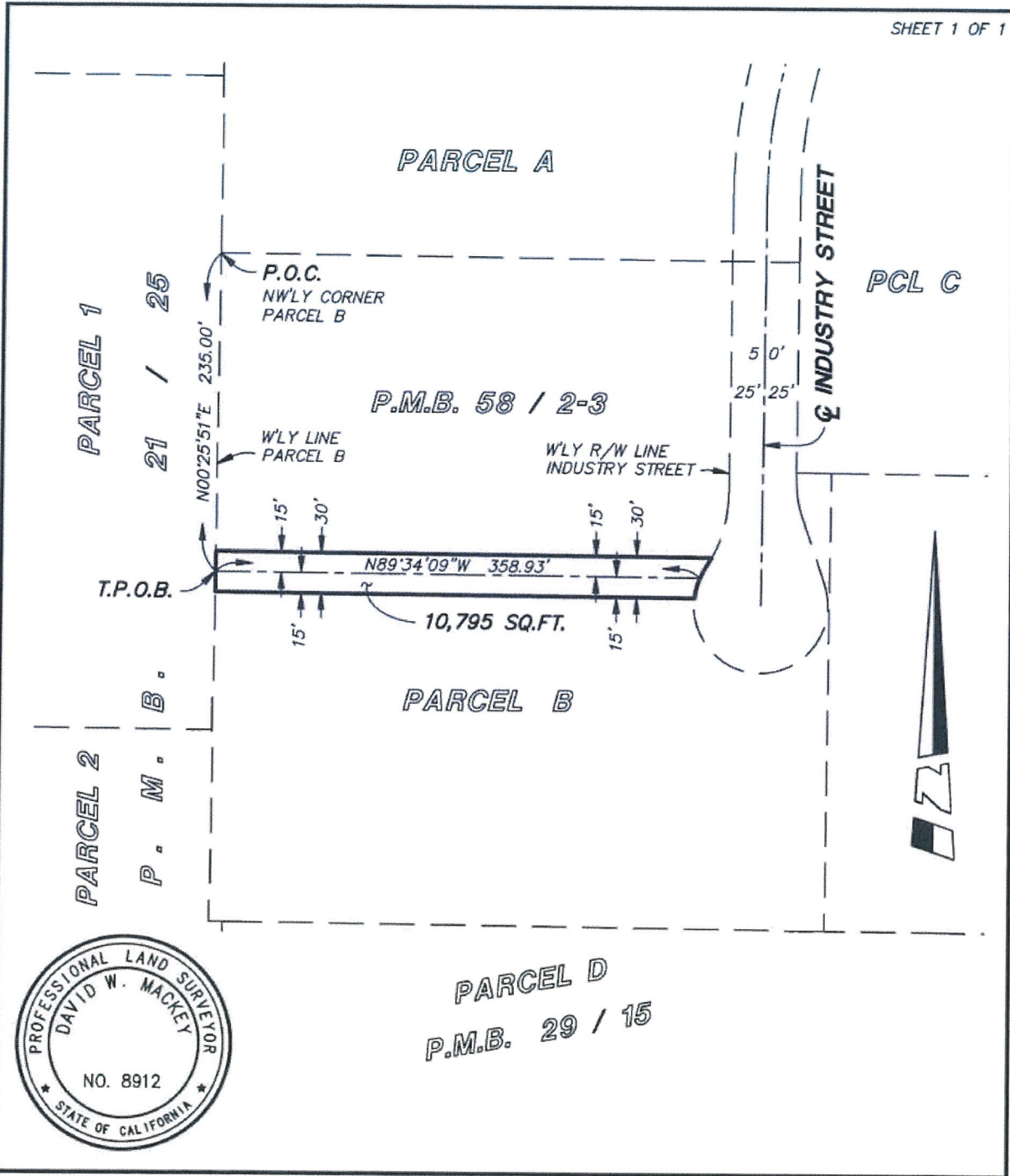
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- CONTRACTOR NOTES:**
- 1. VERIFY ALL DIMENSIONS AND LOCATIONS.
  - 2. VERIFY ALL MATERIALS AND FINISHES.
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- FINISH SCHEDULE:**
- 1. EXTERIOR WALLS: CONCRETE
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SHEET 1 OF 1



**HUITT-ZOLLARS**  
Huitt-Zollars, Inc. Irvine  
2603 Main Street, Suite 400, Irvine, CA 92614  
Phone (949) 988-5815 Fax (949) 988-5820

APPROVED BY  
*David W. Mackey* 3-8-22

SKETCH TO ACCOMPANY  
A LEGAL DESCRIPTION

**EXHIBIT 'B'**  
**RECIPROCAL ACCESS  
AND UTILITY EASEMENT**

SCALE	1"=100'
DRAWN BY	MB
CHECKED BY	DWM
DATE	3/8/2022
JOB NO.	R314098.01

RESOLUTION NO. 6040-22

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF GARDEN GROVE APPROVING SITE PLAN NO. SP-110-2022, VARIANCE NO. V-036-2022, AND LOT LINE ADJUSTMENT NO. LLA-029-2022 FOR PROPERTIES LOCATED ON THE WEST SIDE OF PALA DRIVE, NORTH OF ACACIA AVENUE, AND THE WEST SIDE OF INDUSTRY STREET, SOUTH OF LAMPSON AVENUE, AT 12691 PALA DRIVE AND 12641 INDUSTRY STREET, ASSESSOR'S PARCEL NOS. 215-011-11 AND 215-011-16.

BE IT RESOLVED that the Planning Commission of the City of Garden Grove, in regular session assembled on May 19, 2022, does hereby approve Site Plan No. SP-110-2022, Variance No. V-036-2022, and Lot Line Adjustment No. LLA-029-2022, for land located on the west side of Pala Drive, north of Acacia Avenue, and the west side of Industry street, south of Lampson avenue, at 12691 Pala Drive and 12641 Industry Street, Assessor's Parcel No. 215-011-11 and 215-011-16, subject to the Conditions of Approval attached hereto as "Exhibit A."

BE IT FURTHER RESOLVED in the matter of Site Plan No. SP-110-2022, Variance No. V-036-2022, and Lot Line Adjustment No. LLA-029-2022, the Planning Commission of the City of Garden Grove does hereby report as follows:

1. The subject case was initiated by Eddie Pang of Prologis, L.P. (the "Applicant").
2. The applicant is requesting Site Plan approval to build a new 149,335 gross square-foot industrial building, necessitating the demolition of the 65,880 square-foot building at 12641 Industry Street, and the 83,100 square-foot building at 12691 Pala Drive. In conjunction with this request, the applicant is also requesting a Variance request to deviate from the thirty-five foot (35'-0") maximum building height requirement of the M-P (Industrial Park) zone by two feet and two inches (2'-2"), and a Lot Line Adjustment request to relocate the property line between 12641 Industry Street and 12691 Pala Drive (collectively, the "Project").
3. The Planning Commission hereby determines that this project is categorically exempt from review under the California Environmental Quality Act ("CEQA") pursuant to Section 15302, Replacement or Reconstruction Projects of the CEQA Guidelines (14 Cal. Code Regs., 15302).
4. The property has a General Plan Land Use designation of Industrial (I), and is currently zoned Industrial Park (M-P). The subject site is currently improved with three industrial buildings across two parcels, comprising 9.02 acres.
5. Existing land use, zoning, and General Plan designation of property within the vicinity of the subject property have been reviewed.
6. Report submitted by City Staff was reviewed.

7. Pursuant to a legal notice, a public hearing was held on May 19, 2022, and interested persons were given an opportunity to be heard.
8. The Planning Commission gave due and careful consideration to the matter during its meeting of May 19, 2022, and considered all oral and written testimony presented regarding the project.

BE IT FURTHER RESOLVED, FOUND AND DETERMINED that the facts and reasons supporting the conclusion of the Planning Commission, as required under Municipal Code Section 9.32.30, are as follows:

FACTS:

The subject site is comprised of two (2) properties, across approximately 9.02 acres, located on the west side of Pala Drive, north of Acacia Avenue, and the west side of Industry Street, south of Lampson Avenue, at 12691 Pala Drive and 12641 Industry Street, respectively. The site has a General Plan Land Use designation of Industrial (I), and is zoned Industrial Park (M-P). The property at 12691 Pala Drive (Assessor's Parcel No. 215-011-11), is currently improved with a 65,800 square-foot industrial building. The property at 12641 Industry Street (Assessor's Parcel No. 215-011-16), is currently improved with an 83,100 square-foot industrial building, and a 36,338 square-foot building (12601 Industry Street). The buildings at 12691 Pala Drive, 12601 Industry Street, and 12641 Industry Street are currently unoccupied. The subject site abuts M-P zoned properties on all sides.

Now, the applicant is requesting to demolish the buildings at 12691 Pala Drive and 12641 Industry Street. The building at 12601 Industry Street is to remain. A new 149,335 gross square-foot industrial building will be constructed in virtually the same location as the existing buildings. The building features two entrances: one fronting Industry Street, and one fronting Pala Drive. At the Pala Drive entrance will be a 3,000 square-foot office area, and a 3,000 square-foot mezzanine office above. At the Industry Street entrance is another 2,000 square-foot office area. The remaining 141,335 square feet of the building will consist of open floor area.

Vehicle traffic can access the site via two (2) new driveways on Pala Drive, or two (2) new driveways on Industry Street. A drive aisle provides the vehicular circulation on-site, wrapping around all sides of the building, connecting the four (4) driveways, and the truck docking area. The drive aisle also provides reciprocal access to the property directly to the north, at 12601 Industry Street. Vehicular parking spaces are provided along all sides of the proposed building. Pedestrian access will also be provided from Pala Drive and Industry Street, with bicycle parking at both entrances.

The Municipal Code requires one (1) parking space per 1,000 square feet of gross floor area for industrial uses in excess of 100,000 square feet. Incidental offices

associated with the industrial use that do not exceed 30% of the gross floor area do not require additional parking. The proposed building is approximately 149,335 square feet in size. The total office space does not exceed 30% of the gross floor area, and therefore does not require additional parking. In total, 150 parking spaces are required for the use. The subject site provides 154 parking spaces, a surplus of four (4) parking spaces. Based on the parking requirements of the Municipal Code, the building at 12601 Industry Street requires 73 parking spaces, and provides 89, a sixteen (16) space surplus.

The M-P zone requires a minimum of 10% of the total site area to be dedicated to landscaping. The proposed site design will provide a total of approximately 36,534 square feet of landscaping (11.97% of the overall site). Furthermore, the M-P zone requires a minimum ten-foot (10'-0") landscaped setback along all non-arterial street frontages, excluding the driveway access points. The entirety of the street frontages along Pala Drive and Industry Street are landscaped, with a minimum dimension of twenty feet (20'-0") wide. Additionally, one (1) tree must be planted along every thirty feet (30'-0") of street frontage. In the parking lot, a minimum of one (1) tree is required for every ten (10) parking spaces. The proposed project complies with all landscaping requirements.

Characterized by a rectangular footprint, flat roof, and large, vertical windows accentuating the corners of the building, the building takes on a contemporary design. The building will be constructed in a tilt-up concrete style. Various scores in the concrete walls, and various paint colors add visual intrigue. The main entrances in the corners of the buildings feature vertical windows and storefronts glazed in green hues. Additional windows will be added above the loading bays, helping illuminate the interior of the building, and helping to add contrast against the concrete walls. The neutral color scheme consists of shades of white, grey, and green colors, which contributes to the building's contemporary design. Window and door trim are all constructed of metal, trimmed black to add contrast.

A Variance is also requested to allow for the new building to exceed the maximum height permitted in the M-P zone by two feet and two inches (2'-2"). The applicant is also requesting to move the lot line that is currently between the two properties approximately 262'-0" to the north, such that the existing building at 12601 Industry Street would reside on its own parcel. A reciprocal access agreement will be recorded along the new property line to maintain circulation access for both properties.

#### FINDINGS AND REASONS:

##### Site Plan:

1. The Site Plan complies with the spirit and intent of the provisions, conditions and requirements of Title 9 and is consistent with the General Plan.



The General Plan Land Use Designation of the subject site is Industrial (I), which is intended to encourage general industrial uses, such as warehousing and distribution or business parks, and more intensive industrial uses, such as manufacturing, fabrication, assembly, processing, trucking, warehousing and distribution, and servicing. The Industrial Park (M-P) zoning implements the General Plan, and is intended to provide for modern industrial, research and administrative facilities by requiring comprehensive planning of large parcels of land and the coordination of building design and locations. Goals, policies, and implementation programs of the General Plan strive, in part, to:

Goal LU-1: The City of Garden Grove is a well-planned community with sufficient land uses and intensities to meet the needs of anticipated growth and achieve the community's vision. The existing buildings were constructed in 1973. In the years since, the industry standards for industrial-type uses have changed. The new building would accommodate these new industry standards with more truck bays, higher interior ceilings, and a large, open floor area. By accommodating current market demands, it helps the City to be a more economically viable destination for industrial uses into the foreseeable future.

Goal LU-4: The City seeks to develop uses that are compatible with one another. The proposed industrial building is located in an existing industrial area. Various industrial uses, including manufacturing, distribution, and storage/warehousing facilities, abut the subject property on all sides. The siting of the proposed industrial use will continue the development patterns of the immediate surroundings.

Policy LU-4.5: Require that the commercial and industrial developments adjoining residential uses be adequately screened and buffered from residential areas. The subject property is not directly adjacent to residential uses. The design of the proposed building, however, has taken into account any impacts on the vicinity, and has therefore proposed landscaping, and other visual screening methods to limit any impacts of the building.

Policy LU-5.1: Work with property owners of vacant property to develop their sites into appropriate, economically viable projects. The existing buildings on the subject site have both been vacant since 2021, according to Business License records. By constructing a new building, and introducing a new use, the proposed project could redevelop an unoccupied property into a more economically viable use.

Policy LU-7.3: Monitor the appearance of industrial properties to prevent areas of decline by requiring improved maintenance or rehabilitation, as necessary. The proposed project will redevelop the entirety of the subject site. As a brand new construction, the project would rid the site of potential

property maintenance issues involved with the existing buildings. Furthermore, the Conditions of Approval associated with the project will require the proper maintenance of the development, including, but not limited to, maintenance of landscaping, trash disposal, and graffiti abatement.

2. The project will not adversely affect essential on-site facilities such as off-street parking, loading and unloading areas, traffic circulation, and points of vehicular and pedestrian access.

Vehicle traffic can access the site from either Pala Drive via two (2) new driveways, or Industry Street via two (2) new driveways. A drive aisle provides the vehicular circulation on-site, wrapping around all sides of the building and connecting the four (4) driveways, and the truck docking area. The drive aisle also provides reciprocal access to the property directly to the north, at 12601 Industry Street. Standard parking spaces are provided along the north, south, and east sides of the proposed building. To the west, in the rear of the building, parallel parking spaces will be adjacent to the drive aisle. The City's Engineering Division has reviewed the on- and off-site vehicle circulation, and has not raised any concerns with the project design.

A row of eighteen (18) truck bays will flank the eastern side of the building, between the two entrances. The docking bays connect directly into the open floor area, toward the center of the building. Outside, a truck turn-around and parking area is provided adjacent to the loading bays. The main entrance to the trucking area will be provided from Pala Drive, to the south, but secondary access is also provided from Industry Street.

The design of the building will also provide new pedestrian access from both Pala Drive and Industry Street. The accessible path of travel from Pala Drive will cross the drive aisle before reaching the southern entrance of the storage building. Access from Industry Street will pass through a landscaped area before reaching the north entrance of the building. This pedestrian access also connects to the accessible parking spaces in the parking lot. Bicycle parking will also be provided near the entrances of the building.

Parking requirements from Municipal Code Section 9.16.040.150.D. for "Industrial Uses" stipulate one (1) parking space per 1,000 square feet of gross floor area for buildings in excess of 100,000 square feet. Incidental offices associated with the industrial use that do not exceed 30% of the gross floor area do not require additional parking.

The proposed building is approximately 149,335 square feet in size, requiring 150 parking spaces. Of that, the office space totals approximately 8,000 square feet, or approximately 5% of the gross floor area. This does not

exceed 30% of the gross floor area, and therefore does not require additional parking. Therefore, 150 parking spaces are required for the use. The subject site provides 154 striped parking spaces, a surplus of four (4) spaces.

The building at 12601 Industry Street is approximately 36,338 square feet. Parking standards for an industrial use of that size requires two (2) parking spaces per 1,000 square feet of gross floor area. Only 4,791 square feet of the gross floor area is dedicated to office use, or 13.2%. This does not exceed 30% of the gross floor area, and therefore does not require additional parking. In total, 73 parking spaces are required for the use. The subject site provides 89 striped parking spaces, a surplus of sixteen (16) spaces.

The Community and Economic Development Department, and the Engineering Division, have reviewed the plans and all appropriate conditions of approval and mitigation measures have been incorporated to minimize any adverse impacts on surrounding streets. Accordingly, the design of the project will not adversely affect essential on-site facilities such as off-street parking, loading and unloading areas, traffic circulation, and vehicular and pedestrian access.

3. The project will not adversely affect essential public facilities such as streets and alleys, utilities and drainage channels.

The streets in the area are adequate to accommodate the development. Existing utilities and drainage channels in the area are adequate to accommodate the development. The proposed development will install and maintain landscaping, allowing adequate drainage of stormwater. Landscaping will also be added along the street frontages of Pala Drive and Industry Street. A preliminary water quality management plan (WQMP) has been reviewed and approved by the Engineering Division. The Public Works Department has reviewed the project, and has incorporated all of the appropriate conditions of approval to minimize any adverse impacts.

4. The project will not adversely impact the Public Works Department's ability to perform its required function.

The Public Works Department has reviewed the project, and has incorporated all of the appropriate conditions of approval to minimize any adverse impacts to ensure the project will not adversely impact the Public Works Department's ability to perform its required function(s).

5. The project is compatible with the physical, functional, and visual quality of the neighboring uses and desirable neighborhood characteristics.

The subject properties are located in an area that is adjacent to Industrial Park (M-P) zoned properties in all directions.

The proposed development would improve a currently unoccupied property, making it more compatible with the surrounding neighborhood. Architecturally, the facility has been designed with facades to be aesthetically complimentary with the surrounding industrial buildings. A variety of colors, materials, and massing help create visual intrigue. Contemporary architectural styles are compatible with the nearby industrial uses.

The proposed building will provide adequate parking, vehicular and pedestrian circulation for access to and from the site, and new landscaping. The architecture and design of the project will be of sufficiently high quality, consistent with developments elsewhere in the surrounding industrial area.

The project has been designed in accordance with the development standards applicable to the M-P zone. With exception to the requested Variance, the project meets all other Municipal Code development standards, such as, but not limited to: building setbacks, parking, and landscaping. The City's Community and Economic Development Department has reviewed the proposed project, and all appropriate conditions of approval have been incorporated to ensure physical, functional, and visual compatibility with the project's surroundings.

6. Through the planning and design of buildings and building placement, the provision of open space landscaping and other site amenities will attain an attractive environment for the occupants of the property.

The proposed building will provide adequate parking, vehicular and pedestrian circulation for access to and from the site, and new landscaping. The architecture and design of the building will be of sufficiently high quality, consistent with the industrial buildings nearby.

The new building will be situated toward the center of the lot, with setbacks of forty feet (40'-0") to the northerly property line, forty-four feet (44'-0") to the westerly property line, approximately seventy-six feet (76'-3") to the southerly property line, and a minimum of twenty feet (20'-0") to the easterly property line along Pala Drive and Industry Street. Landscape planters will be provided along the perimeter of the site to ensure adequate buffering of any potential noise and light/glare impacts. A total of 36,534 square feet of landscaping will be provided on-site.

The City's Community and Economic Development Department has reviewed the proposed project, and all appropriate conditions of approval have been

incorporated to ensure the attractiveness of the on-site landscaping and other amenities.

Variance:

1. That there are exceptional or extraordinary circumstances or conditions applicable to the property involved or to the intended use or development of the property that do not apply generally to other property in the same zone or neighborhood.

Approval of this Variance will allow the project to deviate from the maximum building height of thirty-five feet (35'-0") in the M-P (Industrial Park) zone by two feet and two inches (2'-2"), in order to facilitate the construction of the new building. With exception to the requested Variance, the project meets all other Municipal Code development standards such as, but not limited to, building setbacks, parking, and landscaping. There are exceptional or extraordinary circumstances or conditions applicable to the property involved that do not apply generally to other similar properties in the immediate vicinity, within the same zone, or other similarly zoned properties throughout the City.

The subject site is surrounded and constrained by previously developed properties to the north, west, and south. To accommodate the edge conditions created by the presence of these existing developments, the proposed elevations adjacent to the building in the northwest and southeast corners are lower to accommodate the positive flow of stormwater to the existing and proposed drainage inlets. Building height is measured from the average level of the building at grade. By lowering the adjacent grade, the average level of the building was also lowered, creating a taller measurement to the top of the building roof. This drainage condition coupled with the calculation process for determining building height, results in a building height that is measured slightly higher than the existing buildings.

Furthermore, increased building heights lead to more vertical racking, enhanced fire sprinkler systems, and more expansive commodity storage types, which may attract high quality tenants in the community. The current market demand for industrial buildings is calling for minimum interior ceiling height clearances of thirty-two feet (32'-0"), taller than buildings built in the past. Vertical expansion is favored over horizontal, resulting in the need for taller buildings to meet vertical racking space requirements. This provides sufficient space for the minimum standard vertical racking systems, as well as meeting storage capacity needs. The additional building height requested of two feet and two inches (2'-2") provides for five feet and two inches (5'-2") of space to accommodate fire sprinkler suppression system requirements, as well as all necessary structural building requirements.

Lastly, the property is irregularly shaped, with frontages along two culs-de-sac at Pala Drive and Industry Street. Some of the lot area are triangular slivers, and generally unusable space adjacent to the cul-de-sacs. This effectively reduces the usable area of the lot, preventing a larger building footprint. Horizontal expansion, with a larger building footprint is unfeasible. The most feasible option is to add vertical capacity, resulting in a taller building.

2. That such Variance is necessary for the preservation and enjoyment of a substantial property right possessed by other property in the same vicinity and zone, but which is denied to the subject property.

Approval of this Variance will allow the project to deviate from the maximum building height of thirty-five feet (35'-0") in the M-P (Industrial Park) zone by two feet and two inches (2'-2"), in order to facilitate the construction of the new building. With exception to the requested Variance, the project meets all other Municipal Code development standards such as, but not limited to, building setbacks, parking, and landscaping. Other properties within immediate vicinity of the subject lot, and within other similarly zoned properties throughout the City, have existing similar type buildings that exceed the maximum thirty-five foot (35'-0") building height allowed M-P zone.

Within the vicinity of the subject lot, an industrial building located at 12821 Knott Street, which is a similarly zoned property (Industrial Planned Unit Development), provides a building height of thirty-nine feet (39'-0") to the top of the roof, and forty feet (40'-0") to the top of the parapet, exceeding the thirty-five foot (35'-0") building height requirement found in the M-P zone. Another building at 12752 Monarch Street was granted a Variance (V-033-2021) in May 2021 to allow for a building height of approximately thirty-seven feet (36'-9"). This property is also located in the same neighborhood and zoning district as the subject property.

Accordingly, approval of the proposed Variance will not set a precedent, and will allow the applicant to enjoy a substantial property right possessed by other property owners located in the M-P zone, and other similar zoned properties in the City. With exception to the requested Variance to deviate from the maximum building height limitation, the proposed project complies with all other development standards of the M-P zone.

3. That the granting of a Variance will not be materially detrimental to the public welfare or injurious to the property or improvements in such zone or neighborhood in which the property is located.

Approval of this Variance will allow the project to deviate from the maximum building height of thirty-five feet (35'-0") in the M-P (Industrial Park) zone, in order to facilitate the construction of the new building. The approval of this Variance will allow a 37'-2" building height (to the topmost portion of the roof), which is slightly more than two feet (2'-2") above the maximum building height of thirty-five feet (35'-0") allowed in the M-P zone.

The Variance will not affect surrounding properties, which are developed with existing industrial uses. The subject lot is not within close proximity to sensitive uses (e.g., residential, schools, parks). Other properties within the vicinity of the subject lot, and within other similar zoned properties throughout the City, have existing similar type buildings that exceed the maximum building height of the M-P zone. Nearby, an industrial building located at 12821 Knott Street, which is a similar zoned property (Industrial Planned Unit Development), provides a building height of thirty-nine feet (39'-0") to the top of the roof, and forty feet (40'-0") to the top of the parapet, exceeding the thirty-five foot (35'-0") building height requirement found in the M-P zone. Another building at 12752 Monarch Street was granted a Variance (V-033-2021) in May 2021 to allow for a building height of approximately thirty-seven feet (36'-9").

The project will be required to comply with all applicable building, and life-safety codes and regulations to ensure that there are no adverse impacts on public health, safety, or welfare. Furthermore, the proposal has been reviewed by all City departments in order to ensure compliance with all applicable Municipal Code provisions. Provided the project complies with the Conditions of Approval, the approval of the Variance will not be materially detrimental to the public welfare or injurious to the property or improvements in such zone or neighborhood in which the property is located.

4. That the granting of such Variance will not adversely affect the City's General Plan.

The Industrial (I) General Plan Land Use Designation is intended to create, maintain, and enhance industrial areas characterized by general industrial uses, such as warehousing and distribution or business parks, and more intensive industrial uses, such manufacturing, fabrication, assembly, processing, trucking, warehousing and distribution, and servicing. The proposed building is consistent with the intent of the Industrial Land Use Designation. Specifically, the project meets the intent of the general plan by furthering the following General Plan goals, policies, and implementation programs:

Goal LU-7: The City values its industrial areas as an important contributor to a well-planned community and for the jobs and economic impacts they

provide. The proposed project will replace the existing buildings with a contemporary building that meets the needs of the current industrial market by providing increased vertical racking space for additional interior storage capacity. The proposed project will ensure that the City maintains healthy and competitive industrial areas contributing to a well-planned and well-maintained community.

Implementation Program LU-IMP-7B: Require improved maintenance and rehabilitation of industrial buildings and sites, as necessary. The project Conditions of Approval will remain in perpetuity for the life of the development. The Conditions of Approval will help ensure that the property is well-maintained, and will not be detrimental to the surrounding neighborhood in which the property is located. Additionally, the project proposes enhanced exterior elevations which are aesthetically pleasing, which can foster further investments in high quality, contemporary industrial areas around the community.

The proposed project will be consistent with the spirit and intent of the General Plan, furthering its goals, policies, and implementation programs. The project would also help meet the community's need for competitive industrial facilities in its industrial zoned areas. Therefore, the granting of the requested Variance will not adversely affect the City's General Plan.

5. That approval of the Variance is subject to such conditions as will assure that it does not constitute a grant of special privileges inconsistent with the limitations upon other properties in the vicinity and zone in which the subject property is situated.

Provided the Conditions of Approval are adhered to for the life of the project, approval of the subject Variance will not grant a special privilege that is inconsistent with the limitations upon other properties located within the vicinity or zone in which the subject property is situated.

To construct the proposed building, the Variance to deviate from the maximum building height of thirty-five feet (35'-0") allowed in the M-P (Industrial Park) zone by two feet and two inches (2'-2") is necessary to facilitate the development and improvement of the property, which otherwise meets all other Municipal Code development standards. This includes, but is not limited to, building setbacks, parking, and landscaping. Other properties within the immediate area of the subject lot, and within other similarly zoned properties throughout the City, have existing similar type buildings that exceed the maximum thirty-five foot (35'-0") building height allowed M-P zone.



Within the immediate vicinity of the subject site, an industrial building located at 12821 Knott Street, which is a similarly zoned property (Industrial Planned Unit Development) provides a building height of thirty-nine feet (39'-0") to the top of the roof, and forty feet (40'-0") to the top of the parapet, which exceeds the building height requirement of the M-P zone. A Variance (V-033-2021) was more recently granted for the redevelopment of an industrial building located 12752 Monarch Street, providing for a maximum building height of approximately thirty-seven feet (36'-9").

Accordingly, approval of the proposed Variance will not grant a special privilege that is inconsistent with the limitations upon other property owners located in the Industrial Park and other similar zoned properties in the City. With exception to the requested Variance to deviate from the maximum building height limitation, the proposed project complies with all other development standards of the M-P zone.

The industrial building market has evolved over the past years to necessitate a greater interior ceiling height. Current market demands are calling for a minimum thirty-two foot (32'-0") interior ceiling height to provide increased operations and storage capacity. Increased building heights lead to more vertical racking, enhanced fire sprinkler systems, and more expansive commodity storage types, which attract high quality tenants. Property acquisition in a built-out community to accommodate horizontal expansion is impractical, and the proposed project is otherwise consistent with the standards and intent of the M-P (Industrial Park) zone. Conditions applicable to the subject property and its intended development do not readily apply to other property in similar zones, the same zone, and neighborhood. Accordingly, approval of the proposed Variance will not set a precedent, and will allow the applicant to enjoy a substantial property right possessed by other property owners located in similar properties in the City.

In addition, Condition of Approval No. 84 states, "The rights granted the applicant pursuant to Variance No. V-036-2022 shall continue in effect for only so long as the improvements authorized and contemplated by Site Plan No. SP-110-2022, and these Conditions of Approval (as they may be amended from time to time) continue to exist on the Site. In the event that that Site Plan No. SP-110-2022 is not exercised within one year of approval (or the length of any extension approved by the City), or the improvements authorized and contemplated by Site Plan No. SP-110-2022 are demolished and not re-established within one year of demolition, Variance No. V-036-2022 shall cease to be effective or grant the applicant any rights to construct other improvements inconsistent with the then-currently applicable development standards."

Lot Line Adjustment:

1. The parcels, as a result of the Lot Line Adjustment, will conform to the zoning and building codes.

The subject parcels comprising the project are both zoned M-P (Industrial Park). The M-P zone requires a minimum lot area of 15,000 square feet, or 0.34 acres. The property at 12691 Pala Drive (APN: 215-011-11) is currently approximately 4.4 acres. The northern property at 12641 Industry Street (APN: 215-011-16) is currently approximately 4.7 acres. The requested Lot Line Adjustment would move the property line between the two properties approximately 262'-0" to the north. As a result of the Lot Line Adjustment, the southern parcel will be expanded to approximately 6.98 acres, and the property to the north will be reduced to approximately 2.04 acres. Both properties will comply with the minimum lot size required by the Code, even after the lot line adjustment.

INCORPORATION OF FACTS AND REASONS SET FORTH IN STAFF REPORT

In addition to the foregoing, the Planning Commission incorporates herein by this reference, the facts and reasons set forth in the staff report.

BE IT FURTHER RESOLVED that the Planning Commission does conclude:

1. The Site Plan No. SP-110-2022, Variance No. V-036-2022, and Lot Line Adjustment No. LLA-029-2022 possess characteristics that would indicate justification of the requests in accordance with Municipal Code Section 9.32.030.
2. In order to fulfill the purpose and intent of the Municipal Code, and thereby promote the health, safety, and general welfare, the following conditions of approval, attached as Exhibit "A", shall apply to Site Plan No. Site Plan No. SP-110-2022, Variance No. V-036-2022, and Lot Line Adjustment No. LLA-029-2022.

Adopted this 19th day of May 2022

## **EXHIBIT "A"**

**Site Plan No. SP-110-2022  
Variance No. V-036-2022  
Lot Line Adjustment No. LLA-029-2022**

12691 Pala Drive & 12641 Industry Street

### **CONDITIONS OF APPROVAL**

#### **General Conditions**

1. The applicant and each owner of the property shall execute, and the applicant shall record against the property a "Notice of Agreement with Conditions of Approval and Discretionary Permit of Approval," as prepared by the City Attorney's Office. Proof of such recordation is required prior to issuance of building permits.
2. All Conditions of Approval set forth herein shall be binding on and enforceable against each of the following, and whenever used herein, the term "applicant" shall mean and refer to each of the following: the project applicant, Prologis L.P., the developer of the project, the current owner of the Property, the future owner(s) and tenants(s) of the Property, and each of their respective successors and assigns. All Conditions of Approval are required to be adhered to for the life of the project, regardless of property ownership. Any changes of the Conditions of Approval require approval by the Planning Commission. All Conditions of Approval herein shall apply to Site Plan No. SP-110-2022, Variance No. V-036-2022, and Lot Line Adjustment No. LLA-029-2022.
3. Approval of this Site Plan, Variance, and Lot Line Adjustment shall not be construed to mean any waiver of applicable and appropriate zoning and other regulations; and wherein not otherwise specified, all requirements of the City of Garden Grove Municipal Code shall apply.
4. Minor modifications to the Site Plan, Variance, Lot Line Adjustment, and/or these Conditions of Approval may be approved by the Community and Economic Development Director, in his or her discretion. Proposed modifications, to the project and/or these Conditions of Approval, determined by the Community and Economic Development Director not to be minor in nature shall be subject to approval of new and/or amended land use entitlements by the applicable City hearing body.
5. The approved site plan, floor plan, and use of the subject property, as represented by the Applicant, are an integral part of the decision approving this Site Plan. If major modifications are made to the approved floor plan,

site plan, or other related changes that result in the intensification of the project or create impacts that have not been previously addressed, the proper entitlements shall be obtained reflecting such changes.

6. All conditions of approval shall be implemented at the applicant's expense, except where specified in the individual condition.

### **Engineering Division**

7. The applicant shall be subject to Traffic Mitigation Fees, Drainage Facilities Fees, Water Assessment Fees, and other applicable mitigation fees identified in Chapter 9.44 of the Garden Grove Municipal Code, along with all other applicable fees duly adopted by the City. The amount of said fees shall be calculated based on the City's current fee schedule at the time of permit issuance.
8. Prior to issuance of a grading permit, the applicant shall design overhead street lighting within the development in a manner meeting the approval of the City Engineer. Location of lighting poles shall be shown on the precise grading and street improvement plans.
9. A geotechnical study prepared by a registered geotechnical engineer is required. The report shall analyze the liquefaction potential of the site and make recommendations. The report shall analyze sub-surface issues related to the past uses of the site, including sub-surface tanks, infiltration and stormwater treatment structures, and basement and septic facilities. Any soil or groundwater contamination shall be remediated prior to the issuance of a building permit in a manner meeting the approval of the City Engineer in concert with the Orange County Health Department. The report shall make recommendations for pavement design of the interior streets and parking spaces. The report shall also test and analyze soil conditions for LID (Low Impact Development) principles and implementations, including potential infiltration alternatives, soil compaction, saturation, permeability and groundwater levels. Requirements for any "dewatering" will also need to be addressed in the report.
10. A separate street permit shall be required for all work performed within the public right-of-way.
11. Grading and street improvement plans prepared by a registered Civil Engineer are required. The grading plan shall be based on a current survey

of the site, including a boundary survey, topography on adjacent properties up to thirty feet (30'-0") outside the boundary, and designed to preclude cross-lot drainage. Minimum grades shall be 0.50% for concrete flow lines, and 1.25% for asphalt. The grading plan shall also include water and sewer improvements. The grading plan shall include a coordinated utility plan. All improvements within the public right-of-way shall conform to all format and design requirements of the City Standard Drawings & Specifications. Special features, such as decorative pavers or other improvements, may be required to have an agreement prepared between the owner and the city to cover any encroachment limitations, responsibilities and maintenance requirements.

12. The applicant shall complete the following for the Lot Line Adjustment (LLA) application:
  - a. Prior to issuance of a grading permit, the applicant shall submit to the Planning Services Division an updated title report along with copies of the recorded instruments listed in the title report, reference maps used to prepare legal description, and the plat per County of Orange Lot Line Adjustment Manual.
  - b. Preparation, formatting, and packaging of the LLA application and exhibits must follow the requirements of the County of Orange Lot Line Adjustment Manual, and shall be reviewed and approved by the City Engineer.
  - c. The order of recording documentation of the LLA application with the County of Orange Recorder's Office shall be as prescribed by the City Engineer. The instrument numbers assigned by the County Recorder for the LLA shall be written into the new property description of the succeeding Grant Deed or Quit Claim conveyances as prescribed by the City Engineer.
  - d. If there is a lien against either parcel affected by the LLA resulting from a loan or deed of trust, whether or not indicated in the Title Report, each lender and/or beneficiary of the deed of trust, as applicable, must consent to the LLA by execution of a recordable Modification of Deed of Trust, or similar recordable document acceptable to the City. The Modification of Deed of Trust, or similar document, must be signed by a person authorized to represent the lender or beneficiary, as applicable, and notarized. Exhibits for the Modification of Deed of Trust shall be the exhibits as prepared for the

LLA prior to recordation. A blank line must be left in the acknowledgement description to write the LLA instrument number as recorded by the County Clerk.

13. The grading plan shall depict an accessibility route for the ADA pathway in conformance with the requirements of the Department of Justice standards, latest edition, and Section 1110A of the California Building Code.
14. Grading fees shall be calculated based the current fee schedule at the time of permit issuance.
15. All parking spaces that abut sidewalks that are not elevated with a curb face to the stall, shall have wheel stops in order to prevent vehicle overhang into sidewalk. A minimum six-foot (6'-0") wide sidewalk is required for parking spaces that are utilizing elevated sidewalk curb face as a wheel stop, and must maintain a minimum four-foot (4'-0") from the overhang of the vehicle bumper for the ADA pathway.
16. In accordance with the Orange County Storm Water Program manual, the applicant and/or their contractors shall provide dumpsters onsite during construction unless an Encroachment Permit is obtained for placement in street.
17. Prior to the issuance of any grading or building permits, or prior to recordation upon subdivision of land if determined applicable by the City Building Official, the applicant shall submit to the City for review and approval a Water Quality Management Plan that:
  - a. Addresses required mitigation Site Design Best Management Practices (BMPs) based upon the latest Santa Ana Regional Water Quality Control Board (SARWQCB) approved Drainage Area Management Plan (DAMP), as identified in the geotechnical report recommendations and findings, including, but not limited to, infiltration minimizing impervious areas, maximizing permeability, minimizing directly connected impervious areas, creating reduced or "zero discharge" areas, and conserving natural areas as required by the latest adopted County of Orange Technical Guidance Document (TGD).
  - b. BMP's shall be sized per the requirements of the latest Technical Guidance Documents.

- c. Incorporates the applicable Routine Source Control BMPs as defined in the DAMP.
  - d. Incorporates structural and Treatment Control BMPs as defined in the DAMP.
  - e. Generally describes the long-term operation and maintenance requirements for the Treatment Control BMPs.
  - f. Identifies the entity that will be responsible for long-term operation and maintenance of the Treatment Control BMPs.
  - g. Describes the mechanism for funding the long-term operation and maintenance of the Treatment Control BMPs.
  - h. Provides a hydrological analysis with scaled map, as well as hydrologic and hydraulic calculations to size storm drains, per the Orange County RDMD standards.
18. All trash container areas shall meet the following requirements per City of Garden Grove Standard B-502, and state-mandated commercial organic recycling laws, including AB 1826 and its implementing regulations, and any other applicable State recycling laws related to refuse, recyclables, and/or organics:
- a. Paved with an impervious surface, designed not to allow run-on mixing of drainage from adjoining areas, designed to divert drainage from adjoining roofs and pavements to be directed around the area for trash roll out, and screened or walled to prevent off-site transport of trash by water or wind.
  - b. Provide solid roof or awning to prevent direct precipitation into the enclosure.
  - c. Connection of trash area drains to the municipal storm drain system is prohibited. Drainage from the enclosure may be directed to a conforming grease or contaminant interceptor.
  - d. Potential conflicts with fire code access requirements and garbage pickup routing for access activities shall be considered in implementation of design and source control. See CASQA Storm

Water Handbook Section 3.2.9 and BMP Fact Sheet SD-32 for additional information.

- e. The trash enclosure and containers shall be located to allow pick-up and maneuvering, including turnarounds, in the area of enclosures, and concrete aprons for roll-out areas.
  - f. Pursuant to commercial organic recycling state law (AB 1826), the applicant shall coordinate storage and removal of the organics waste with the local recycling/trash company.
  - g. Pursuant to applicable state laws, the applicant shall contact and coordinate with the operations manager of the local recycling/trash company (Republic Services, 800-700-8610) to ensure the trash enclosure includes the appropriate size and number of containers for the disposal of items, such as, but not limited to, municipal solid waste (MSW), recyclables, and organic green waste.
  - h. Based on the amount of waste disposed per week, the applicant shall coordinate with the local recycling/trash company to ensure the adequate frequency of trash pick-up is serviced to the site for municipal solid waste (MSW), recyclables, and organic green waste, and any other type of waste.
  - i. The applicant shall ensure large, bulky items, intended for coordinated and scheduled pick-up by the local recycling/trash company, are not placed in areas that encroach into drive aisles, parking spaces, pedestrian pathways, or areas in the front of the property, including the public right-of-way (e.g., street, sidewalk), during and after construction. Any large bulky items shall be out of public vantage points.
  - j. The requirements for the trash enclosure and design criteria are bound and coordinated with the Water Quality Management Plan (WQMP), when required, as depicted on the project grading plan, which shall be incorporated into the WQMP by narrative description, exhibits, and an Operation and Maintenance Plan (O&M).
19. The applicant and its contractor shall be responsible for protecting all existing horizontal and vertical survey controls, monuments, ties (centerline and corner), and benchmarks located within the limits of the project. If any of



the above require removal, relocation, or resetting, the contractor shall, prior to any construction work, and under the supervision of a California licensed Land Surveyor, establish sufficient temporary ties and benchmarks to enable the points to be reset after completion of construction. Any ties, monuments, and benchmarks disturbed during construction shall be reset per Orange County Surveyor Standards after construction. The applicant and its contractor shall also reset the tie monuments where curb or curb ramps are removed and replaced, or new ramps are installed. The Applicant and its contractor shall be liable for, at their expense, any resurvey required due to their negligence in protecting existing ties, monuments, benchmarks, or any such horizontal and vertical controls. Temporary Benchmarks shall not be used for vertical control. Benchmarks shall be to the National Geodetic Vertical Datum (NGVD).

20. Prior to the issuance of any grading or building permits for projects that will result in soil disturbance of one acre or more of land, the applicant shall demonstrate that coverage has been obtained under California's General Permit for Stormwater Discharges Associated with Construction Activity by providing a copy of the Notice of Intent (NOI) submitted to the State Water Resources Control Board, and a copy of the subsequent notification of the issuance of a Waste Discharge Identification (WDID) Number. Projects subject to this requirement shall prepare and implement a Stormwater Pollution Prevention Plan (SWPPP). A copy of the current SWPPP shall be kept at the project site, and be available for City review on request.
21. Any new or required block walls and/or retaining walls shall be shown on the grading plans, both in plan-view and cross-sections. Cross-sections shall show vertical and horizontal relations of improvements (existing and proposed) on both sides of property lines. Required wall heights shall be measured vertically from the highest adjacent finished grade. Block walls shall be designed in accordance to City of Garden Grove Standard B-504, B-505, B-506 & B-508, or designed by a professional registered engineer. In addition, the following shall apply:
  - a. The color and material of all proposed block walls, columns, and wrought iron fencing shall be approved by the Planning Services Division Prior to installation.
  - b. Openings for drainage through walls shall be shown in section details, and approved by the City Engineer. Cross-lot drainage is not allowed.

22. The applicant shall identify a temporary parking site(s) for construction crew(s) prior to issuance of a grading permit. No construction parking is allowed on local streets.
23. Prior to issuance of a grading permit, the applicant submit and obtain approval of a worksite traffic control plan, satisfactory to the City Traffic Engineer.
24. Heavy construction truck traffic and hauling trips shall occur outside of peak travel periods. Peak travel periods are considered to be from 7:00 a.m. to 9:00 a.m., and 4:00 p.m. to 6:00 p.m.
25. Any required lane closures shall occur outside of peak travel periods.
26. Construction vehicles shall be parked off traveled roadways, in a designated parking lot.
27. Prior to issuance of a grading permit, the applicant shall provide a hydrological analysis with scaled map, calculations, and hydraulic calculations to size storm drains per the Orange County RDMD standards. Parkway culverts shall be designed per Orange County Standard Plan 1309, Type B. BMP's shall be sized per the requirements of the latest Technical Guidance Documents.
28. Prior to the issuance of a building permit, the applicant shall design and construct street frontage improvements as identified below. All landscaping installed within public rights-of-way shall be maintained by the applicant in a manner meeting the approval of the City Engineer, and the Planning Services Division. A separate street improvement plan shall be prepared for Industry Street and Pala Drive, and submitted to the Engineering Division for improvements within the City rights-of-way.

#### Industry Street

- a. The applicant shall cold mill (grind)the existing asphalt pavement three-inch (0'-03") uniform depth, and replace with three inches (0'-3") of fiber-reinforced asphalt surface course, from the edge of the westerly gutter to the edge of easterly gutter on Industry Street, along the property frontage, per City specifications, and the direction of the City Engineer.

- b. Any proposed landscaping in the public right-of-way shall be approved by the Planning Services Division, and maintained by the applicant.
- c. The applicant shall coordinate the location of all new water meters, backflow preventers, and backflow devices to be placed in the sidewalk area on Industry Street with the Planning Services Division, and the Water Services Division.

Pala Drive

- a. Remove and replace the existing northerly substandard driveway approach to the site on Pala Drive, and construct a new commercial driveway approach in accordance with City of Garden Grove Standard Plan B-121.
- b. Any proposed landscaping in the public right-of-way shall be approved by the Planning Services Division, and maintained by the applicant.
- c. The applicant shall coordinate the location of all new water meters, backflow preventers, and backflow devices to be placed in the sidewalk area on Pala Drive with the Planning Services Division, and the Water Services Division.

**Public Works Environmental**

- 29. The applicant shall comply with all applicable Garden Grove Sanitary District and California State recycling requirements for commercial developments.

**Orange County Fire Authority**

- 30. The applicant shall comply with all applicable Orange County Fire Authority requirements, including, but not limited to the Fire Master Plan.

**Building and Safety Division**

- 31. All work shall comply with the current California Building Standards Code at the time of permit application.
- 32. The application shall provide "clean air" and "future electric vehicle charging" parking stalls, in compliance with California Green Building Code Sections 5.106.5.2 and 5.106.5.3. Additionally, all electric vehicle charging stations

and parking stalls shall comply with CBC Section 11B-228.3.

33. The applicant shall provide an accessible path-of-travel to the trash enclosures.
34. The applicant shall provide a building height/area analysis to show compliance with CBC Chapter 5
35. The applicant shall specify the type of fire sprinkler system to be used.
36. The applicant shall provide an exiting plan, showing the path-of-travel, and the maximum travel distance, including a common path of egress.
37. The applicant shall provide an accessible means of egress complying with CBC Section 1009.
38. The applicant shall provide a copy of a soils investigation report at the time of permit application.
39. All rooms/spaces/elements shall meet the accessibility requirements of CBC Chapter 11B.

#### **Water Services Division**

40. New water service installations two inches (0'-2") and smaller, shall be installed by the City of Garden Grove at owner's/developer's expense. Installation shall be scheduled upon payment of applicable fees, unless otherwise noted. Fire services and larger water services three inches (0'-3") and larger, shall be installed by developer/owner's contractor per City Standards.
41. Water meters shall be located within the City right-of-way, or within dedicated waterline easement. Fire services and large water services three inches (0'-3") and larger, shall be installed by contractor with a Class A or C-34 license, per City water standards, and inspected by approved Public Works inspection.
42. A Reduced Pressure Principle Device (RPPD) backflow prevention device shall be installed for meter protection. The landscape system shall also have RPPD device. Any carbonation dispensing equipment shall have a RPPD device. Installation shall be per City standards, and shall be tested by a certified

- backflow device tester immediately after installation. The cross-connection inspector shall be notified for inspection after the installation is completed. The owner shall have the RPPD device tested once a year thereafter by a certified backflow device tester, and the test results submitted to the Public Works Department, Water Services Division. The property owner must open a water account upon installation of a RPPD device.
43. It shall be the responsibility of the owner/developer to abandon any existing private water well(s) per Orange County Health Department requirements. Abandonment(s) shall be inspected by an Orange County Health Department inspector after permits have been obtained.
  44. A composite utility site plan shall be part of the water plan approval.
  45. There is an existing water main and a water easement running along a portion of the westerly property line. There shall be no structures or utilities built on or crossing water or sewer main easements.
  46. There shall be a minimum fifteen-foot (15'-0") clearance of building footings from the water main. Clearances less than fifteen feet (15'-0") shall be reviewed and approved by the Water Services Division.
  47. New utilities shall have a minimum five-foot (5'-0") horizontal, and a minimum one-foot (1'-0") vertical clearance from the water main and appurtenances.
  48. There shall be a minimum clearance from the sewer main or laterals, and the water main of ten feet (10'-0") from outside of pipe to outside of pipe.
  49. No permanent structures, trees or deep-rooted plants shall be placed over sewer main or water main.
  50. Any new or existing water valve located within a new concrete driveway or sidewalk shall be constructed per City Standard B-753.
  51. The City shall determine if existing water services(s) is/are usable and meets current City standards. Any existing meter and service located within new driveway(s) shall be relocated at the owner's expense.
  52. Existing fire services on both Pala Drive and Industry Street have single-check detector assembly in a vault. These are required to be upgraded to

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- above-ground Double-Check Detector Assembly (DCDA), as per condition 53, below. The Applicant shall prepare quitclaim deeds, at the Applicant's sole cost and expense, satisfactory to the City Engineer and City Attorney, for quitclaim by the City to the property owner(s) the existing water easements for the two (2) fire service vaults, following completion of the upgrade to the above-ground DCDA.
53. Fire service shall have an above-ground backflow device with a double-check valve assembly. The device shall be tested immediately after installation and once a year thereafter by a certified backflow device tester, and the results to be submitted to the Public Works Department, Water Services Division. The device shall be on private property, and is the responsibility of the property owner. The above-ground assembly shall be screened from public view as required by the Planning Services Division.
  54. The location and number of fire hydrants shall be as required by the Water Services Division and Orange County Fire Authority (OCFA).
  55. Commercial food use of any type shall require the installation of an approved grease interceptor prior to obtaining a business license. The plumbing plan for any grease interceptor shall be routed to Environmental Services for review.
  56. Food grinders (garbage disposal devices) are prohibited per Ordinance 6 of the Garden Grove Sanitary District Code of Regulations. Existing units are to be removed.
  57. If needed, the owner shall install a new sewer lateral with clean out at the street right-of-way line. Laterals in the public right-of-way shall be a minimum six-inch (0'-6") diameter, extra strength VCP with wedgelock joints.
  58. The contractor shall abandon any existing unused sewer lateral(s) at the street right-of-way on the property owner's side. The sewer pipe shall be capped with an expansion sewer plug, and encased in concrete. Only one sewer connection per lot is allowed.
  59. All perpendicular crossings of the sewer, including laterals, shall maintain a minimum vertical separation of twelve inches (1'-0") below the water main, outer diameter to outer diameter. All exceptions to the above require a variance from the State Water Resources Control Board.

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60. If the water main is exposed during the installation of a sewer lateral, a twenty-foot (20'-0") section of the water main shall be replaced with twenty feet (20'-0") of PVC C-900 DR-14 Class 305 water pipe, size in kind and centered at the crossing.

### **Planning Services Division**

61. A prominent, permanent sign, stating "NO LOITERING IS ALLOWED ON OR IN FRONT OF THE PREMISES," shall be posted in a place that is clearly visible to patrons of the applicant. The sign lettering shall be four to six inches tall (0'-4" – 0'-6"), with black letters on a white background. The sign shall be displayed near or at the entrance, and shall also be visible to the public.
62. Litter shall be removed daily from the premises, including adjacent public sidewalks, and from all parking areas under the control of the applicant. These areas shall be swept or cleaned, either mechanically or manually, on a weekly basis, to control debris.
63. The applicant/property owner shall abate all graffiti vandalism within the premises. The applicant/property owner shall implement best management practices to prevent and abate graffiti vandalism within the premises throughout the life of the project, including, but not limited to, timely removal of all graffiti, the use of graffiti resistant coatings and surfaces, the installation of vegetation screening of frequent graffiti sites, and the installation of signage, lighting, and/or security cameras, as necessary. Graffiti shall be removed/eliminated by the applicant/property owner as soon as reasonably possible after it is discovered, but not later than 72 hours after discovery.
64. The applicant is advised that the establishment is subject to the provisions of State Labor Code Section 6404.5 (ref: State Law AB 13), which prohibits smoking inside the establishment as of January 1, 1995.
65. Permits from the City of Garden Grove shall be obtained prior to displaying any temporary advertising (i.e., banners).
66. Signs shall comply with the City of Garden Grove sign requirements. No more than 15% of the total window area and clear doors shall bear advertising or signs of any sort. No signs advertising alcoholic beverages shall be placed on the windows. Any opaque material applied to the store

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- front, such as window tint, shall count toward the maximum window coverage area.
67. Exterior advertisements displays or exterior wall advertisements shall not be allowed.
  68. Any modifications to existing signs or the installation of new signs shall require approval by the Community and Economic Development Department, Planning Services Division prior to issuance of a building permit.
  69. Hours and days of construction and grading shall be as follows as set forth in the City of Garden Grove's Municipal Code Chapter 8.47 as adopted, except that:
    - a. Monday through Saturday - not before 7 a.m. and not after 8 p.m. (of the same day).
    - b. Sunday and Federal Holidays may work same hours, but subject to noise restrictions as stipulated in Chapter 8.47 of the Municipal Code.
  70. Construction activities shall adhere to SCAQMD Rule 403 (Fugitive Dust) that includes dust minimization measures, the use of electricity from power poles rather than diesel or gasoline powered generators, and the use methanol, natural gas, propane or butane vehicles instead of gasoline or diesel powered equipment, where feasible. Also, the use of solar or low-emission water heaters, and use of low-sodium parking lot lights, and to ensure compliance with Title 24.
  71. No exterior piping, plumbing, roof top access ladders, or mechanical ductwork shall be permitted on any exterior facade and/or be visible from any public right-of-way or adjoining property.
  72. Any and all correction notice(s) generated through the plan check and/or inspection process is/are hereby incorporated by reference as conditions of approval and shall be fully complied with by the owner, applicant, and all agents thereof.
  73. No roof-mounted mechanical equipment shall be permitted unless a method of screening complementary to the architecture of the building is approved by the Community and Economic Development Department, Planning Services



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- Division. Said screening shall block visibility of any roof-mounted mechanical equipment from view of public streets and surrounding properties.
74. Building color and material samples shall be submitted to the Planning Services Division for review and approval prior to issuance of building permits.
  75. All lighting structures shall be placed so as to confine direct rays to the subject property. All exterior lights shall be reviewed and approved by the Planning Division. Lighting adjacent to residential properties shall be restricted to low decorative type wall-mounted lights, or a ground lighting system. Lighting shall be provided throughout all private drive aisles and entrances to the development per City standards for street lighting.
  76. The site improvements and subsequent operation of the site/business(es) shall adhere to the following:
    - a. There shall be no business activities, or storage permitted outside of the building. All business related equipment and material shall be kept inside the building except for loading or unloading purposes.
    - b. Property owners, employees, and business operators shall not store vehicles anywhere on the site.
    - c. All drive aisles on the site are considered to be fire lanes and shall remain clear and free of any materials, and/or vehicles.
    - d. The property owner shall comply with the adopted City Noise Ordinance.
  77. All landscaping shall be consistent with the landscape requirements of the Landscape Water Efficiency Guidelines (Appendix A), per Title 9 of the Municipal Code. The applicant shall submit a separate and complete Water Efficient Landscape Plan. The water efficient landscape submittal shall include landscape plans, irrigation plans, soils report, grading plans, and all other applicable documentation. The landscape plans shall include type, size, location, and quantity of all plant material. The landscape plans are also subject to the following:
    - a. A complete, permanent, automatic remote control irrigation system shall be provided for all landscaping areas shown on the plans. The

- sprinklers shall be of low flow/precipitation sprinkler heads for water conservation.
- b. The plans shall provide a mixture of a minimum of ten percent (10%) of the trees at 48-inch box, ten percent (10%) of the trees at 36-inch box, fifteen percent (15%) of the trees at 24-inch box and sixty percent (60%) of the trees at 15-gallon, the remaining five percent (5%) may be of any size. These trees shall be incorporated into the landscaped frontages of all streets. Where clinging vines are considered for covering walls, drought tolerant vines shall be used.
  - c. Landscape treatments and irrigation shall be installed within the front, side, and rear setback areas of the property. The landscaping shall incorporate a mixture of ground cover, flowerbeds, shrubs, and trees. The Community and Economic Development Department shall review the type and location of all proposed trees.
  - d. Clinging vines shall be planted within landscape planters along any proposed block walls, and/or trash enclosure walls to deter graffiti.
  - e. The applicant shall be responsible for all installation and permanent maintenance of all landscaping on the property. Said responsibility shall extend to the parkway landscaping, sidewalk, curb, and pavement of the site. All planting areas are to be kept free of weeds, debris, and graffiti.
  - f. All above-ground utilities (e.g., water backflow devices, electrical transformers, irrigation equipment, etc.) shall be shown on the landscaping plans in order to ensure proper screening.
  - g. The landscape plans shall incorporate and maintain, for the life of the project, means and methods to address water run-off, including Low Impact Development (LID) provisions which address water run-off. This includes, without limitation, all applicable requirements of the Water Quality Management Plan (WQMP), Drainage Area Management Plan (DAMP), or Local Implementation Plan (LIP), and any other water conservation measures applicable to this type of development required by applicable ordinance or regulation.
78. During construction, if paleontological or archaeological resources are found, all attempts will be made to preserve in place or leave in an undisturbed

state. In the event that fossil specimens or cultural resources are encountered on the site during construction, and cannot be preserved in place, the applicant shall contact and retain, at applicant's expense, a qualified paleontologist or archaeologist, as applicable, acceptable to the City, to evaluate and determine appropriate treatment for the specimen or resource, and work in the vicinity of the discovery shall halt until appropriate assessment and treatment of the specimen or resource is determined by the paleontologist or archeologist (work can continue elsewhere on the project site). Any mitigation, monitoring, collection, and specimen/resource treatment measures recommended by the paleontologist/archaeologist shall be implemented by the applicant at its own cost.

79. The applicant shall comply with the Migratory Bird Treaty Act (MBTA), and Sections 3503, 3503.5, and 3513 of the California Fish and Game Code, which require the protection of active nests of all bird species, prior to the removal of any on-site landscaping, including the removal of existing trees.
80. A copy of the resolution, including the conditions approving Site Plan No. SP-110-2022, Variance No. V-036-2022, and Lot Line Adjustment No. LLA-029-2022, shall be kept on the premises at all times.
81. The applicant/property owner shall submit signed letters acknowledging receipt of the decision approving Site Plan No. SP-110-2022, Variance No. V-036-2022, and Lot Line Adjustment No. LLA-029-2022, and their agreement with all conditions of approval.
82. The applicant shall, as a condition of project approval, at its sole expense, defend, indemnify and hold harmless the City, its officers, employees, agents and consultants from any claim, action, or proceeding against the City, its officers, agents, employees and/or consultants, which action seeks to set aside, void, annul or otherwise challenge any approval by the City Council, Planning Commission, or other City decision-making body, or City staff action concerning Site Plan No. SP-110-2022, Variance No. V-036-2022, and Lot Line Adjustment No. LLA-029-2022. The applicant shall pay the City's defense costs, including attorney fees and all other litigation related expenses, and shall reimburse the City for court costs, which the City may be required to pay as a result of such defense. The applicant shall further pay any adverse financial award, which may issue against the City including, but not limited, to any award of attorney fees to a party challenging such project approval. The City shall retain the right to select its counsel of choice in any action referred to herein.

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83. In accordance with Garden Grove Municipal Code Sections 9.32.160, the rights granted pursuant to Site Plan No. SP-110-2022, Variance No. V-036-2022, and Lot Line Adjustment No. LLA-029-2022 shall be valid for a period of one (1) year from the effective date of this approval. Unless a time extension is granted pursuant to Section 9.32.030.D.9 of the Municipal Code, the rights conferred by Site Plan No. SP-110-2022, Variance No. V-036-2022, and Lot Line Adjustment No. LLA-029-2022 shall become null and void if the subject development and construction necessary and incidental thereto is not commenced within one (1) year of the expiration of the appeal period, and thereafter diligently advanced until completion of the project. In the event construction of the project is commenced but not diligently advanced until completion, the rights granted pursuant to Site Plan No. SP-110-2022, Variance No. V-036-2022, and Lot Line Adjustment No. LLA-029-2022 shall expire if the building permits for the project expire.
84. The rights granted the applicant pursuant to Variance No. V-036-2022 shall continue in effect for only so long as the improvements authorized and contemplated by Site Plan No. SP-110-2022, and these Conditions of Approval (as they may be amended from time to time) continue to exist on the Site. In the event that that Site Plan No. SP-110-2022 is not exercised within one year of approval (or the length of any extension approved by the City), or the improvements authorized and contemplated by Site Plan No. SP-110-2022 are demolished and not re-established within one year of demolition, Variance No. V-036-2022 shall cease to be effective or grant the applicant any rights to construct other improvements inconsistent with the then-currently applicable development standards.

# COMMUNITY AND ECONOMIC DEVELOPMENT DEPARTMENT PLANNING STAFF REPORT

<b>AGENDA ITEM NO.:</b> C.4.	<b>SITE LOCATION:</b> Northeast corner of Garden Grove Boulevard and Brookhurst Street at 10201 and 10231 Garden Grove Boulevard
<b>HEARING DATE:</b> May 19, 2022	<b>GENERAL PLAN:</b> Residential/Commercial Mixed Use 2
<b>CASE NOS.:</b> Site Plan No. SP-111-2022 and Tentative Parcel Map No. PM-2021-190	<b>ZONE:</b> GGMU-2 (Garden Grove Boulevard Mixed Use 2)
<b>APPLICANT:</b> Dr. Michael Dao	<b>APN:</b> 089-072-53, 089-072-66
<b>PROPERTY OWNER:</b> Same as applicant	<b>CEQA DETERMINATION:</b> Exempt- Section 15332 "In-Fill Development Projects"

## **REQUEST:**

The applicant is requesting Tentative Parcel Map approval to consolidate two (2) existing parcels into a single 1.86 acre parcel and Site Plan approval to construct a five-story mixed-use development on the 1.86-acre site with 9,786 square feet of retail space, 9,270 square feet of medical space, and 52 apartment units. Pursuant to the State Density Bonus law, in exchange for reserving three (3) of the apartment units for very low-income households, the applicant is requesting a density bonus of seven residential units (7) and the following concession/incentive and waivers from the GGMU-2 (Garden Grove Boulevard Mixed Use 2) zone development standards: (1) a concession to allow each unit to deviate from the minimum private balcony area and dimensions for each unit, (2) a waiver to deviate from the minimum active recreation dimensions and to allow the indoor and roof deck recreation areas to contribute to more than 50% of the required open space; (3) a waiver to allow the building to exceed the maximum building height of 50 feet or 4-stories; and (4) a waiver to deviate from the minimum 300 cubic feet of private storage per unit.

## **BACKGROUND:**

The project site is comprised of two (2) parcels with a combined land area of 1.86-acres located on the northeast corner of Garden Grove Boulevard and Brookhurst Street. The subject site has a General Plan Land Use designation of Residential/Commercial Mixed Use 2, and is zoned GGMU-2 (Garden Grove Boulevard Mixed Use 2).

The project site is located in an area developed with commercial and residential developments. The project site abuts a GGMU-2 zoned property to the north developed with a four-story office professional building and a one-story restaurant building; a Planned Unit Development (PUD-101-95) zoned residential development to the east

developed with 104 detached, three-story, residential homes; GGMU-3 zoned properties to the south, across Garden Grove Boulevard, developed with a Wienerschnitzel restaurant and an auto body shop; and PUD-123-09 zoned parcels to the west, across Brookhurst Street, entitled for the Brookhurst Triangle Project.

The project site is located in the vicinity of two (2) new mixed-use projects: the Brookhurst Triangle Project and the Garden Brook Senior Village Project. The Brookhurst Triangle Project is scheduled to be developed in phases and allows up to 600 residential units and up to 200,000 square feet of commercial/retail uses. Phase 1 of the Brookhurst Triangle Project, 180 apartment units, was completed in 2018. The project is also located in close proximity to a second mixed-use project, the Garden Brook Senior Village (10032 Garden Grove Boulevard), which is an eight-story mixed-use project, with 11,041 square feet of commercial space and 394 senior units, which is currently under construction.

The Residential/Commercial Mixed Use 2 land use designation is intended to allow a mix of residential and commercial uses around older and underutilized commercial developments, while the GGMU-2 zone allows for mixed-use developments.

The applicant purchased the project site in 2019. The project site is currently vacant and secured with a perimeter chain-link fence. The project site was previously developed and occupied by a furniture store (10201 Garden Grove Boulevard) and a used car dealership (10231 Garden Grove Boulevard) since the late 1950s. In 2019, building permits were issued to demolish all on-site building structures.

The applicant proposes to develop the project site with a five-story, mixed-use development consisting of both commercial lease space and residential units. The project will consist of 19,056 square feet of commercial spaces (9,786 square feet for retail use and 9,270 square feet for medical use), and 52 apartment units, which includes a density bonus of approximately 15.55%. The Project will reserve three (3) units for "very low-income" households. A Density Bonus Housing Agreement will be recorded to restrict the affordability of the units for 55-years.

The applicant has requested State Density Bonus allowances for density, parking, and concession and waivers to deviate from the GGMU-2 development standards. The applicant is requesting one (1) concession and three (3) waivers from the GGMU-2 zone development standards: (1) a concession to deviate from Section 9.18.110.030.F.1 of the Municipal Code that requires each unit to provide a private balcony with minimum dimensions of 9 feet and a minimum area of 90 square feet; (2) a waiver to deviate from Section 9.18.110.030.F.2 of the Municipal Code, which requires active recreation areas to maintain minimum dimensions of 20 feet, and limits indoor or roof deck recreation areas from contributing no more than 50% to the required recreation area; (3) a waiver to deviate from Section 9.18.090.020 of the Municipal Code that requires a maximum building height of 50 feet or 4 stories; and (4) a waiver to deviate from Section 9.18.110.030.H.2 of the Municipal Code, which requires each unit to provide a minimum of 300 cubic feet of storage per unit.

**PROJECT STATISTICS:**

	<b>Provided</b>	<b>Code Requirement</b>	<b>Meets Code</b>	<b>Requires a Concession or Waiver</b>
<b>Total Lot Size</b>	1.86-acres (Net)	15,000 S.F.	Yes	
<b>Maximum Commercial (FAR)<sup>1</sup></b>	0.235	0.50	Yes	
<b>Commercial Component</b>	19,056 S.F. Retail: 9,786 S.F. Medical: 9,270 S.F.	40,550 S.F. (Max)	Yes	
<b>Residential Density</b>	27.95 units per acre <sup>2</sup>	24 units per acre	State Density Bonus Law	
<b>Residential Units</b>	52 units <sup>2</sup>	45 units	State Density Bonus Law	
<b>Total Parking</b>				
Residential Parking	65	65 <sup>3</sup>	State Density Bonus Law	
Commercial Parking	117	104	Yes	
Total	182	169	Yes	
<b>Recreation Area Total</b>	16,961 S.F.	15,600 S.F.	Yes	
Recreation Area	14,249 S.F.		State Density Bonus Law	Yes
Private Balcony	2,712 S.F.			
<b>Building Setbacks</b>				
North (Rear)	10'-3"	None	Yes	
South (Front)	15'-0"	15'-0"	Yes	
East (Side)	60'-3"	10'-0" plus encroachment plan	Yes	
West (Corner Street)	11'-5"	10'-0"	Yes	
<b>Building Height</b>	54'-8" (overall to elevator shaft: 67'-8")	50'-0 or 4-stories	State Density Bonus Law	Yes
<b>Building Height Stepbacks<sup>5</sup></b>				
Garden Grove Boulevard	34'-8" high	50'-0" height within 45 feet		
Brookhurst Street	34'-8" high	50'-0" height within 25 feet		

1. Floor Area Ratio (F.A.R.): means the ratio of total net building area to the total lot area.
2. The applicant requests a State Density Bonus to increase the project's density by 22.5%, which entitles the project to eleven (11) additional units above the base density. However, the project will only provide seven (7) additional units above the base density.
3. The State Density Bonus law requires parking at 1 space per 1 bedroom unit, and 1.5 spaces per 2-3 bedroom unit.
4. While the project complies with the total required recreation area, a concession and waivers are requested to facilitate the development of the project, as discussed in the concession and waiver section of the report
5. The code requires building heights within 45 feet of the Garden Grove Boulevard property line not to exceed 50 feet in height, and building heights within 25 feet of the Brookhurst Street property line not to exceed 50 feet in height.

**Residential Units**

<b>Number of Bedrooms/Baths</b>	<b>Total Living Area</b>	<b>Total Number of Units</b>
1 Bedroom, 1 Bath	756 S.F.	26
2 Bedrooms, 2 Bath	1,006 S.F.	26
	<b>Total</b>	<b>52</b>

**DISCUSSION:**

State Density Bonus Law:

The State Density Bonus Law (Section 65915 *et seq.* of the California Government Code) entitles applicants to a density bonus, concessions, waivers, and reduced parking to encourage the construction of affordable housing units. The applicant has developed its proposal based on the State Density Bonus law allowances. In exchange for providing three (3) "very low-income" units, the applicant has requested a density bonus, one (1) concession and three (3) waivers to deviate from the development standards of the GGMU-2 zone, and to apply parking ratios established by the State Density Bonus law.

Under the GGMU-2 zone, the base density for the site, which is the maximum number of units allowed by the zone, is 45 dwelling units. The applicant has offered to reserve three (3) of the apartment units, or slightly more than 6% of the base density, for very low-income households. Under the State Density Bonus Law, this entitles the applicant to a density bonus of up to 22.5%. A density bonus of 22.5% allows eleven (11) additional units above the base density, for a total of 56 units. However, the applicant only proposes to construct seven (7) additional units above the base density, for a total of 52 units.

The Project has been designed to comply with the parking requirements of the State Density Bonus, which requires one (1) parking space for each one (1) bedroom unit, and one and one-half (1.5) parking spaces for each two (2) to three (3) bedroom unit. The project provides 26 one-bedroom units, and 26 two-bedroom units, which requires a total of 65 parking spaces.

Concession and Waivers

The applicant has requested one (1) concession and three (3) waivers pursuant to the State Density Bonus Law to facilitate the development of the Project.

The State Density Bonus Law provides that, in addition to a density bonus, an applicant is entitled to one (1) "concession or incentive" if it offers to restrict 5% of the housing units (excluding the density bonus units) for very low-income households. A "concession or incentive" includes a reduction in site development standards or a modification of zoning code requirements or architectural design requirements. The State Density Bonus Law provides that a city must grant a requested concession or incentive unless it makes a written finding, based upon substantial evidence, that the



concession or incentive (1) does not result in identifiable and actual cost reductions to provide for affordable housing costs or rents for the targeted units; (2) would have a specific, adverse impact upon public health and safety or on any real property that is listed in the California Register of Historical Resources and for which there is no feasible method to satisfactorily mitigate or avoid the specific, adverse impact without rendering the development unaffordable to low-income and moderate-income households; or (3) would be contrary to state or federal law.

The following concession is requested for the Project:

Concession 1: The applicant requests a concession to deviate from Section 9.18.110.030.F.1 of the Municipal Code that requires each unit to provide a private balcony with minimum dimensions of 9 feet and a minimum area of 90 square feet. The project is designed to provide a private balcony for each unit that is less than the minimum dimension and minimum area required by the Municipal Code. Each 1-bedroom unit (total of 26 units) is designed with a private balcony with a dimension of 5'-8" in one direction and an area of 59.97 square feet. Each 2-bedroom unit (total of 26 units) is designed with a private balcony with a dimension of 5'-10" in one direction and an area of 44.71 square feet. While the private balconies deviate from the minimum dimension and area required by the code, the balconies are of adequate size to render the space usable by tenants. Furthermore, the total recreation area proposed by the development of 16,961 square feet exceeds the minimum code requirement of 15,600 square feet.

The State Density Bonus Law provides that, in addition to a density bonus and concessions or incentives required to be provided, an applicant may also request the waiver or reduction of development standards that will have the effect of physically precluding the construction of the proposed development, with the required density bonus and concessions or incentives. Similar to a concession or incentive, a city must waive or reduce a development standard that would physically preclude construction of the proposed development unless the waiver or reduction (1) would have a specific, adverse impact upon health or safety, and for which there is no feasible method to satisfactorily mitigate or avoid the specific adverse impact; (2) would have an adverse impact on any real property that is listed in the California Register of Historical Resources; or (3) would be contrary to state or federal law.

The following waivers are requested for the Project:

Waiver 1: The applicant requests a waiver to deviate from Section 9.18.110.030.F.2 of the Municipal Code, which requires active recreation areas to maintain minimum dimensions of 20 feet, and limits indoor or roof deck recreation areas from contributing no more than 50% to the required recreation area.

The project will provide recreation areas to serve the residential component designed as indoor space and roof decks. The recreation areas are located on the fourth and fifth floors of the project. The active recreation areas are designed as two (2) large roof gardens (Roof Garden No. 1 and No. 2) located on the fourth floor roof deck, and an indoor gym and indoor lounge, each located on the fourth and fifth floors. Roof Garden

No. 1 and No. 2 and the two (2) indoor lounges have dimensions of less than 20 feet in one direction. The two (2) roof gardens have walkways within the recreation area, including several with adjoining landscape planters, that are less than 20-feet wide. The width vary in size from approximately 5 feet to 15 feet. These walkways and landscape planters are an integral part of the recreation area. Nevertheless, while the walkways that serve the roof garden are less than 20-feet wide, each roof garden provides a usable recreation space to accommodate appropriate outdoor activities. Roof Garden No. 1 provides a total area of 5,365 square feet, while Roof Garden No. 2 provides a total area of 3,975 square feet.

Furthermore, the two (2) indoor lounges are smaller recreation areas with an area of 336 square feet and 428 square feet, and with dimensions of less than 20 feet, with approximately 6 feet at the narrowest portion and approximately 14 feet at the widest portion of the room. The indoor lounges are intended to be used as community rooms that will be appropriately furnished to encourage residents to socialize or to serve as a quiet area for reading.

Also, the applicant proposes to count 100% of the total area of the indoor gym, indoor lounge, and roof gardens toward the total recreation area requirement. Currently, the Municipal Code allows up to 50% of the area of an indoor recreation space and roof decks to count toward the required recreation area. The purpose of this provision is to require projects to design the majority of the recreation area on the ground-level. The proposed project is designed as a five-story, urban, mixed-use project. The ground-level is designed to accommodate the project's vehicular circulation, drive-aisles, parking, and turn-around areas. Based on the design of the project, locating a major portion of the recreation area on the ground-level is not feasible. The recreation area for the project is proposed on the fourth and the fifth floors, which includes a combination of usable roof decks and indoor recreation areas. The project includes four (4) indoor recreation areas (two (2) gyms and two (2) lounge areas), and roof gardens that are secured and conveniently located to the residential units. Allowing 100% of the indoor recreation areas and the roof gardens to count toward the required recreation area allows the project to comply with its recreation requirement in a configuration that is more conducive to an urban, mixed-use project.

Waiver 2: The applicant requests a waiver to deviate from Section 9.18.090.020 of the Municipal Code that requires a maximum building height of 50 feet or 4 stories. The project is designed as a five-story building with an overall building height of 54'-8" to the roof line, which exceeds the maximum stories and building height of the GGMU-2 zone. The project also includes roof structures that will house the elevator shaft and stairwells, and parapet walls to screen roof equipment that extends 13'-0" and 5'-6", respectively, above the roof line. Section 9.18.100.020.F of the Municipal Code allows roof structures for elevator shafts and stairways, and parapet walls to extend 15'-0" above the building roof height. The proposed building height of the roof extension complies with the code once the waiver for the building height deviation is granted.

The waiver will allow the building to exceed the 50'-0" building height and the 4-story requirement in order to accommodate a 3-level parking structure. The commercial component will be located on the first and third floors of the building, the parking

structure will be located on the first, second, and third floors, while the residential units will be located on the fourth and fifth floors. The building height increase from 50'-0" to 54'-8" to the roof line will not create impacts to the adjacent residential use located to the east since the project is designed to comply with the encroachment plane requirement of the mixed-use zone. The encroachment plane requires the building to maintain a 45-degree angle that commences 15 feet above the grade of the property line located adjacent to the residential use to buffer the residential use from taller and denser developments. The project maintains the required encroachment plane distance as measured from the easterly property line. The proposed building will maintain a 60'-3" setback from the eastern property line to screen, buffer, and to minimize impacts to the adjacent residential use from the first three floors, and a 68'-3" setback from the fourth and fifth floors. Furthermore, while the building height to the roof line will be 54'-8," the overall building height as measured to the top of the parapet wall and to the top of the elevator shaft and stairwell will be 60'-2" and 67'-8," respectively.

Waiver 3: The applicant requests a waiver to deviate from Section 9.18.110.030.H.2 of the Municipal Code, which requires each unit to provide a minimum of 300 cubic feet of storage per unit. This storage area is not part of the normal closet space that is provided in bedrooms or in cupboards, but is additional storage space that is required by the code for each unit. The applicant proposes to provide 70.83 cubic feet of storage for each 2-bedroom unit, and 58.5 cubic feet for each 1-bedroom unit. The storage is designed as a storage closet located within each unit's private balcony. The reduction to the storage area allows the developer to earmark building area to each unit for purposes of meeting the minimum unit living area. The code requires a living area of 750 square feet for 1-bedroom units, and a living area of 900 square feet for 2-bedroom units. Each 1-bedroom unit is designed with approximately 756 square feet of living area, and each 2-bedroom unit is designed with approximately 1,006 square feet of living area.

A copy of the density bonus application for the project is attached as Exhibit A, and includes the applicant's justification for granting the concession and waivers to facilitate the development of the project.

#### Site Design and Circulation:

The applicant proposes to construct a five-story, mixed-use development consisting of 19,056 square feet of commercial space and 52 residential apartment units with a State Density Bonus. The project incorporates a three (3) level parking structure that will serve the commercial and residential uses.

The building footprint occupies the westerly two-thirds portion of the site. The building's commercial storefront is oriented toward Garden Grove Boulevard and Brookhurst Street with a garden plaza located at the southeast corner of the lot. The site design includes an uncovered drive-aisle located along the easterly portion of the site, and a secondary drive-aisle located on the northwest corner of the site.

The site design incorporates several pedestrian walkways that connect the site from the Garden Grove Boulevard and Brookhurst Street sidewalks to promote pedestrian

activity. These walkways connect directly to each storefront and to the garden plaza. On-site walkways are also provided that run parallel to the building's storefront to connect the ground-level tenants and the garden plaza. Additional walkways are provided to access the parking structure and the interior sides of the project.

The vehicular access to the site will be provided from a new drive approach on Garden Grove Boulevard, and a new drive approach from Brookhurst Street. The drive approach located on Garden Grove Boulevard leads directly to a drive-aisle that circulates along the east side of the project site. The drive-aisle incorporates seven (7) parking spaces located along the easterly property line, a designated vehicle lane with a residential call box, and an emergency vehicle turn-around area. The drive-aisle also connects to the parking structure.

The parking structure is designed with three (3) vehicular entry points that are served by the easterly drive-aisle. Two (2) entry points are located in the center of the drive-aisle, and the third entry point is located at the most northerly portion of the drive-aisle, adjacent to the emergency turn-around area. The parking structure also includes one (1) entry point on the west side of the structure that is accessed directly from Brookhurst Street.

The parking structure will include roll-up gates to secure the structure during non-business hours. A sliding gate is also proposed at the center most portion of the easterly drive-aisle that will be closed during non-business hours. The parking structure's gates, and the drive-aisle sliding gate, will remain open during the day when the commercial tenants are in operation, typically from 8:00 a.m. to 8:00 p.m. During non-business hours, the gates will be closed, and residents will use a gate remote to access the parking structure from the northerly entrance from the easterly drive-aisle. The project is designed to provide adequate turn-around area in front of the parking structure gates to allow guests to safely maneuver vehicles off of the site if access to the parking structure is unavailable.

The project will also provide the appropriate red curbs and "No Parking" signs to allow maneuvering of trash trucks and emergency vehicles within the turn-around area.

The project is required to maintain a six-foot high, decorative masonry block wall along the north and east property lines.

### Building Floor Plan Design

The proposed Project is a mixed-used development with approximately 19,056 square feet of commercial space, and 52 residential apartment units.

The GGMU-2 zone allows a maximum commercial floor area ratio (FAR) of 0.5, and a maximum residential density of 24 units per acre. The proposed Project will provide a commercial FAR of 0.235, and through a State Density Bonus, the project will provide a residential density of 27.5 units per acre. The base density of the project is 45 units, and the project will provide seven (7) additional "density bonus" units above the base density, for 52 units. The project will reserve three (3) units for "very low income"

households as required by the State Density Bonus. It should be noted that a density bonus of 22.5% entitles the project to eleven (11) additional units above the base density for total of 56 units; however, the applicant only proposes to construct seven (7) units above the base density.

The proposed building is five-stories with an integrated three-level parking structure. The distribution of uses within the building include commercial tenants and parking on the first floor, parking on the second floor, commercial tenants and parking on the third floor, and residential units on the fourth and fifth floors.

The first floor includes two (2) commercial tenant areas that are intended for retail use, while the third floor includes two (2) commercial tenant areas that are intended for medical use. The plan calls for a commercial shell that will be divided as tenants are identified. The building includes separate lobby areas with elevators to serve the commercial and residential uses. The residential use includes three (3) separate lobby areas with elevator access, while the commercial use is designed with one centralized lobby area. The commercial elevators can access the first, second, and third floors, while the residential elevators can access all five floors. The residential lobbies will be equipped with a key code pad to restrict access to residents only.

The project includes separate trash areas to serve the commercial and residential uses. A trash enclosure is provided along the east side of the property to serve the commercial uses, while a trash room with trash shoots is located within the building that can be accessed by residents on the fourth and fifth floors.

### *Residential Units*

The project will provide 52 apartment units configured as 26 one (1) bedroom units and 26 two (2) bedroom units, each with a private balcony. The residential units are located on the fourth and fifth floors of the building.

The 1-bedroom units are designed with the same floor plan layout. The units will consist of a 756 square foot living area, and include a living room, a kitchen, a washer and dryer closet, a bedroom with a walk-in closet, and one (1) bathroom. The unit includes a private balcony with a storage closet.

The 2-bedroom units are also designed with the same floor plan layout. The units will consist of a 1,006 square feet living area, and include a living room, a kitchen, a washer and dryer closet, a master bedroom with a walk-in closet and a private bathroom, a standard bedroom, and one (1) bathroom. Each of the unit includes a private balcony with a storage closet.

### Parking

The project provides 182 parking spaces to serve the commercial and residential uses. The parking distribution includes 175 parking spaces located within the three (3) level parking structure, and seven (7) parking spaces located outside of the parking structure, on the ground-level, along the easterly property line. The parking spaces will

include the required number of accessible, electric vehicle (EV), and charging stations for both the commercial and residential uses as required by the California Building Code and the Green Building Code.

The commercial parking spaces will be located on the ground-level, and on the first and second levels of the parking structure, while the parking for the residential units will be located on the second and third levels of the parking structure. Table 1 below identifies the distribution of the commercial and residential parking spaces. The residential parking spaces will be clearly labeled, and residents will be issued a parking permit. During non-business hours, the residents will be allowed to use the commercial parking spaces for overflow parking.

Table 1: On-Site Parking Distribution

Parking	Commercial Parking	Residential Parking	Total
Ground-Level	7	0	7
1 <sup>st</sup> Level Structure	52	0	52
2 <sup>nd</sup> Level Structure	58	10	68
3 <sup>rd</sup> Level Structure	0	55	55
Total	117	65	182

Based on the proposed uses, a total of 169 parking spaces are required for the project, including 104 parking spaces for the commercial component and 65 parking spaces for the residential component. The project provides 182 parking spaces, which includes 13 additional parking spaces to serve the commercial use.

The commercial component will include retail and medical uses that are parked with different parking ratios. Retail uses are parked at 1 space per 200 square feet of Gross Floor Area (GFA), while medical uses are parked at 1 space per 170 square feet of GFA. The commercial tenant square footages include approximately 9,786 square feet of retail space, which requires 49 parking spaces, and approximately 9,270 square feet of medical space, which requires 55 parking spaces. The commercial component is required to provide 104 parking spaces, and the project provides 117 parking spaces to serve the commercial use.

The parking for the residential use complies with the State Density Bonus parking requirement. The State Density Bonus law requires 1 space for each 1-bedroom unit, and 1.5 spaces for each 2 and 3 bedroom unit. The project provides 26 one-bedroom units and 26, two-bedroom units. As such, based on the unit design, the residential use is required to provide 65 parking spaces.

Residential Recreation Area:

The project is designed to provide approximately 16,961 square feet of recreation area for the residential use designed as indoor recreation areas, roof deck gardens, private balconies, and passive recreation areas. The Municipal Code requires the project to provide a minimum of 15,600 square feet of recreation area, and the project exceeds

the minimum recreation area requirement. The Municipal Code allows 50% of passive recreation areas and 25% of the garden plaza to count toward the project’s required total recreation area.

The recreation areas are located on the fourth and fifth floors of the building, as well as on the ground-level within the garden plaza area. Table 1 below identifies the location, type, use, and area (square footage) of each recreation area.

Table 2: Residential Recreation Areas

Location	Type	Use	Area <sup>1</sup>
Ground-level	Garden Plaza	Garden Plaza (3,367 S.F. at 25%)	842 S.F.
4 <sup>th</sup> Floor	Active	Roof Garden No. 1	5,365 S.F.
	Active	Roof Garden No. 2	3,976 S.F.
	Passive	Roof Garden No. 3 (528.82 S.F. @ 50%)	195 S.F.
	Passive	Roof Garden No. 4 (528.82 S.F. @ 50%)	264 S.F.
	Indoor	Gym	1,345 S.F.
	Indoor	Lounge	336 S.F.
5 <sup>th</sup> Floor	Indoor	Gym	1,497 S.F.
	Indoor	Lounge	429 S.F.
Units	Private Balconies	Balconies	2,712 S.F.
Total			16,961 S.F.

1. Fractions have been rounded to nearest whole number.

The indoor recreation areas are located on the fourth and fifth floors of the building, and each floor will provide a gym and a lounge. The lounge area will function as a community room to encourage residents to socialize, such as a children’s play area or quiet reading room, and will be furnished based on the needs of the residents, such as with chairs, sofas, and coffee tables.

The roof gardens are located on the fourth floor of the building. The roof gardens include two (2) roof gardens, and two (2) passive roof gardens. Roof Garden No. 1 will feature a raised wood deck with a fire pit, synthetic grass, benches with connecting walkways and landscaping. Roof Garden No. 2 will include outdoor tables and lounge furniture, barbeque grills, benches, synthetic turf, fiberglass planters, and landscaping. Roof Garden No. 3 and No. 4 are smaller gardens that will serve as passive recreation space enhanced with landscaping and amenities required by the code.

As previously mentioned, 25% of the garden plaza area will be counted toward the residential recreation area requirement.

Garden Plaza and Landscaping:

The mixed-use zone requires mixed-use projects to incorporate a garden plaza adjacent to the public right-of-way to liven the streetscape, and to promote a multi-purpose space that can be used for pedestrian activity, public gatherings, art, and passive recreation.

The garden plaza is located at the southeast corner of the lot, and is oriented toward the Garden Grove Boulevard and Brookhurst Street intersection. The garden plaza will feature a central lawn area, a seating area designed as a single, continuous step that meanders through the garden plaza, landscape planters, decorative concrete paving, and several freestanding, circular, LED light fixtures. The LED light fixtures will be a unique art feature that will enhance the garden plaza environment.

Landscaping is also an important component to visually enhance, activate, and promote pedestrian friendly streets. The project will feature canopy trees along the Garden Grove Boulevard and Brookhurst Street rights-of-ways, as well as on-site landscape planters within the street setback areas to create a lively streetscape that is urban in design; that creates an attractive environment for pedestrians; and that is visually appealing to passing motorists.

Furthermore, the applicant is required to submit a detailed landscape and irrigation plan to the City that complies with the landscaping requirements of Title 9 of the Municipal Code, including the City's Landscape Water Efficiency Guidelines. Planning staff will review the type and location of all proposed plant materials.

#### Building Design/Architecture:

The proposed architectural design of the building will feature an attractive contemporary style that includes enhanced articulation, detailing, and varied building massing appropriate for an urban, mixed-use development.

The building design features commercial storefronts that are oriented toward Garden Grove Boulevard and Brookhurst Street. The façade incorporates a curved building element with a glass storefront system at the center of the building that extends vertically along each floor of the elevation to create a focal point. The commercial façade also incorporates the use of columns that are evenly placed along the elevation to provide articulation and to frame the storefront of each ground level commercial tenant.

The commercial ground floor features a double-height storefront of 20'-6." The upper level commercial tenants incorporate storefront systems that are visible, and oriented, to the street. The upper level commercial tenants are served by an open walkway with a railing.

The commercial component street façade incorporates the use of complimentary architecture detailing that is well-designed and integrated into the overall building design. The architecture detailing includes the use of terracotta color, chiseled limestone stone veneer along the columns, mounted horizontal metal louvers, and steel canopies over the entrance to several of the ground-level tenants.

The building design includes a building step-back of 45'-0" from Garden Grove Boulevard, and a building step-back of 25'-0" from Brookhurst Street to the residential units to further vary the building massing. The building area within the step-backs will be activated with the residential roof gardens.



The building façade for the residential component is located on the upper two (2) levels of the building elevation and include the residential unit windows and balconies. The architectural detailing compliments the commercial component and includes a beige color, chiseled limestone veneer, horizontal metal louvers on portions of the parapet wall, and a white smooth stucco finish.

The interior side (east) and rear (north) building elevations include the parking structure openings with railing, and window systems and balconies for the residential units. These elevations will feature a white smooth stucco finish.

#### TENTATIVE PARCEL MAP:

In accordance with the State Subdivision Map Act, the applicant is requesting approval of Tentative Parcel Map No. PM-2021-190 to consolidate two (2) existing parcels into one (1) parcel to accommodate the proposed development. The parcel at 10201 Garden Grove Boulevard has a lot size of 31,560 square feet, while the parcel at 10231 Garden Grove Boulevard has a lot size of 49,272 square feet. The consolidated project site will have a combined lot area of 1.86-acres. The Tentative Parcel Map is the most appropriate instrument to consolidate the project site due to existing utilities easements, such as power poles, that have to be relocated and realigned to facilitate the development. The proposed Tentative Parcel Map is in conformance with the City's General Plan, the City's Subdivision Ordinance, the GGMU-2 zone requirements, and the State's Subdivision Map Act.

#### California Environmental Quality Act:

The proposed development has been determined to be exempt from the California Environmental Quality Act ("CEQA"), pursuant to Section 15332 (In-Fill Development Projects) of the CEQA Guidelines (14 Cal. Code Regs., Section 15303). As set forth in the Class 32 exemption, the proposed project is: (1) consistent with the applicable general plan designation and all applicable general plan policies as well as with applicable zoning designation and regulations; (2) the proposed development occurs within City limits on a project site of no more than five acres substantially surrounded by urban uses; (3) the project site has no value as habitat for endangered, rare or threatened species; (4) approval of the project would not result in any significant effects relating to traffic, noise, air quality or water quality; and (5) the site can be adequately served by all required utilities and public services.

#### No Net Loss

Government Code Section 65863 requires jurisdictions to maintain adequate sites to accommodate their remaining unmet Regional Housing Needs Allocation (RHNA) by each income category at all times throughout the Housing Element planning period. A jurisdiction may not take any action to reduce a parcel's residential density unless it makes findings that the reduction is consistent with the General Plan, including the Housing Element, and that the remaining sites identified in its Housing Element sites inventory can accommodate its remaining unmet RHNA by each income category or it identifies additional sites so that there is no net loss of residential unit capacity.

In addition, if a jurisdiction approves a development on a parcel identified in its Housing Element sites inventory with fewer units than shown in the Housing Element, the jurisdiction must either make findings that the Housing Element's remaining sites have sufficient capacity to accommodate the remaining unmet RHNA by each income level, or identify and make available additional adequate sites to accommodate the remaining unmet RHNA for each income category. However, a jurisdiction may not disapprove a housing development project on the basis that approval of the development would trigger the identification or zoning of additional adequate sites to accommodate the remaining RHNA.

The City's 6<sup>th</sup> Cycle RHNA requires the City to plan for 19,168 housing units for all income levels. A component of preparing the City's Housing Element is the identification of vacant and underutilized sites suitable for residential development, and an evaluation of the housing development potential of these sites in fulfilling the City's RHNA. The combined project site is identified in the City's Housing Element sites inventory as having a realistic capacity to accommodate a total of 54 "above moderate income" units, consisting of 25 units on 10201 Garden Grove Boulevard and 29 units on 10231 Garden Grove Boulevard. The project proposes a total of 52 units, consisting of 49 "above moderate income" units and three (3) "very low-income" units. Because the proposed project includes fewer units than shown in the Housing Element sites inventory, the City must determine whether the remaining Housing Element inventory sites have sufficient capacity to accommodate the City's remaining unmet RHNA by income level if the project is approved as proposed. Staff has evaluated the housing projects that have been permitted or approved during the current planning period, or which are otherwise not accounted for in the Housing Element site analysis, and has determined that the remaining sites identified in the sites inventory have sufficient capacity to accommodate the City's remaining unmet RHNA need for each income level.

At the time the Housing Element was prepared and adopted, the City's *unmet* RHNA was calculated to be 18,208 units, broken down as follows: 6,567 low and very low-income units, 3,087 moderate-income units, and 8,554 above moderate-income units. The sites identified in the adopted Housing Element were determined adequate to accommodate a total of 18,291 units, including 401 more units than the City's unmet RHNA in the low and very low-income categories and 240 more units than the City's unmet RHNA for the moderate-income category. Sites deemed adequate to accommodate the lower and moderate-income categories are also adequate to accommodate above-moderate income units. Overall, the sites inventory in the adopted Housing Element reflected a total capacity surplus of 83 units. Due to this surplus, the proposed project will not create a reduction to the City's total required RHNA allocation.

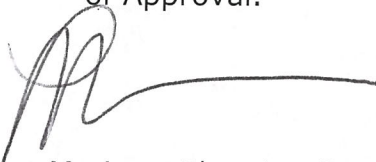
Excluding ADUs, Staff has identified 17 recently permitted or approved new housing units not accounted for in the Housing Element site capacity analysis. These include two (2) deed restricted units in the lower income categories (one very low-income unit and one low-income unit). The other 15 units have been tentatively classified as "above moderate-income" units pending further evaluation. Some of these units were developed on sites not included in the Housing Element sites inventory. After accounting for these permitted and entitled units, and the 52 units in the proposed project, the City's total remaining unmet RHNA would be 18,139 units, broken down as

follows: 6,562 low and very low-income units, 3,087 moderate-income units, and 8,490 above-moderate income units. The remaining capacity of the sites identified in the Housing Element would be 18,226 units, resulting in a total capacity surplus of 87 units. The remaining sites are also adequate to accommodate a surplus of low and very low-income units (403) and moderate-income units (240), and such sites are also adequate to accommodate the City's remaining unmet above moderate-income RHNA. Due to this surplus, the overall residential capacity on sites identified in the Housing Element will still be sufficient to accommodate the City's total remaining unmet RHNA if the proposed project is approved. The required No Net Loss findings can be made and are included in Resolution No. 6041-22.

**RECOMMENDATION:**

Staff recommends that the Planning Commission take the following action:

1. Adopt Resolution No. 6041-22 approving Site Plan No. SP-111-2022 and Tentative Parcel Map No. PM-2021-190, subject to the recommended Conditions of Approval.

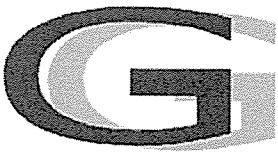


Lee Marino - Planning Services Manager



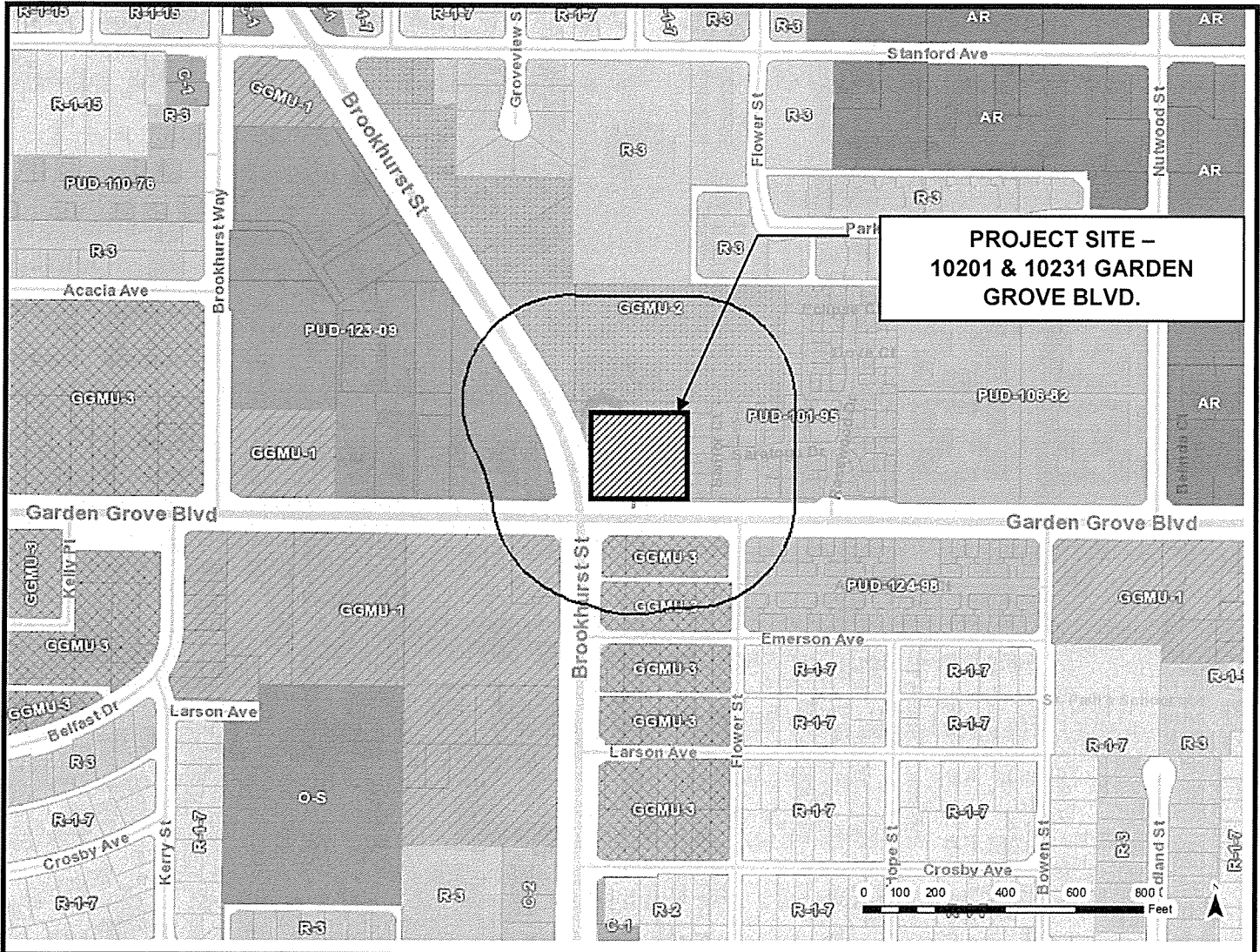
By: Maria Parra - Senior Planner

Attachment: Exhibit "A" – Density Bonus Application  
Exhibit "B" – Technical Studies



**GARDEN GROVE**

**SITE PLAN NO. SP-111-022  
TENTATIVE PARCEL MAP NO. PM-2021-190**



**PROJECT SITE –  
10201 & 10231 GARDEN  
GROVE BLVD.**

**LEGEND**



SUBJECT SITE(S) – 10201 AND 10231 GARDEN GROVE BOULEVARD



300 FOOT RADIUS

**NOTES**

1. SITE ADDRESSES – 10201 AND 10231 GARDEN GROVE BOULEVARD
2. ZONING: GGMU-2 (GARDEN GROVE MIXED USE 2)
3. GENERAL PLAN: RESIDENTIAL/COMMERCIAL 2

**CITY OF GARDEN GROVE  
 COMMUNITY & ECONOMIC DEVELOPMENT DEPARTMENT  
 PLANNING DIVISION  
 MAY 2022**





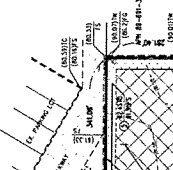
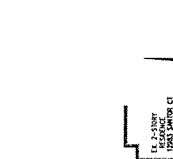


# CITY OF GARDEN GROVE PRELIMINARY GRADING PLAN

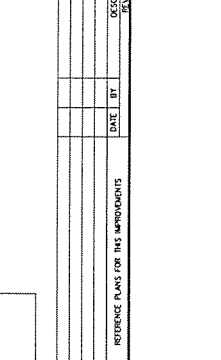
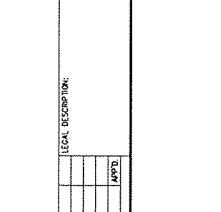
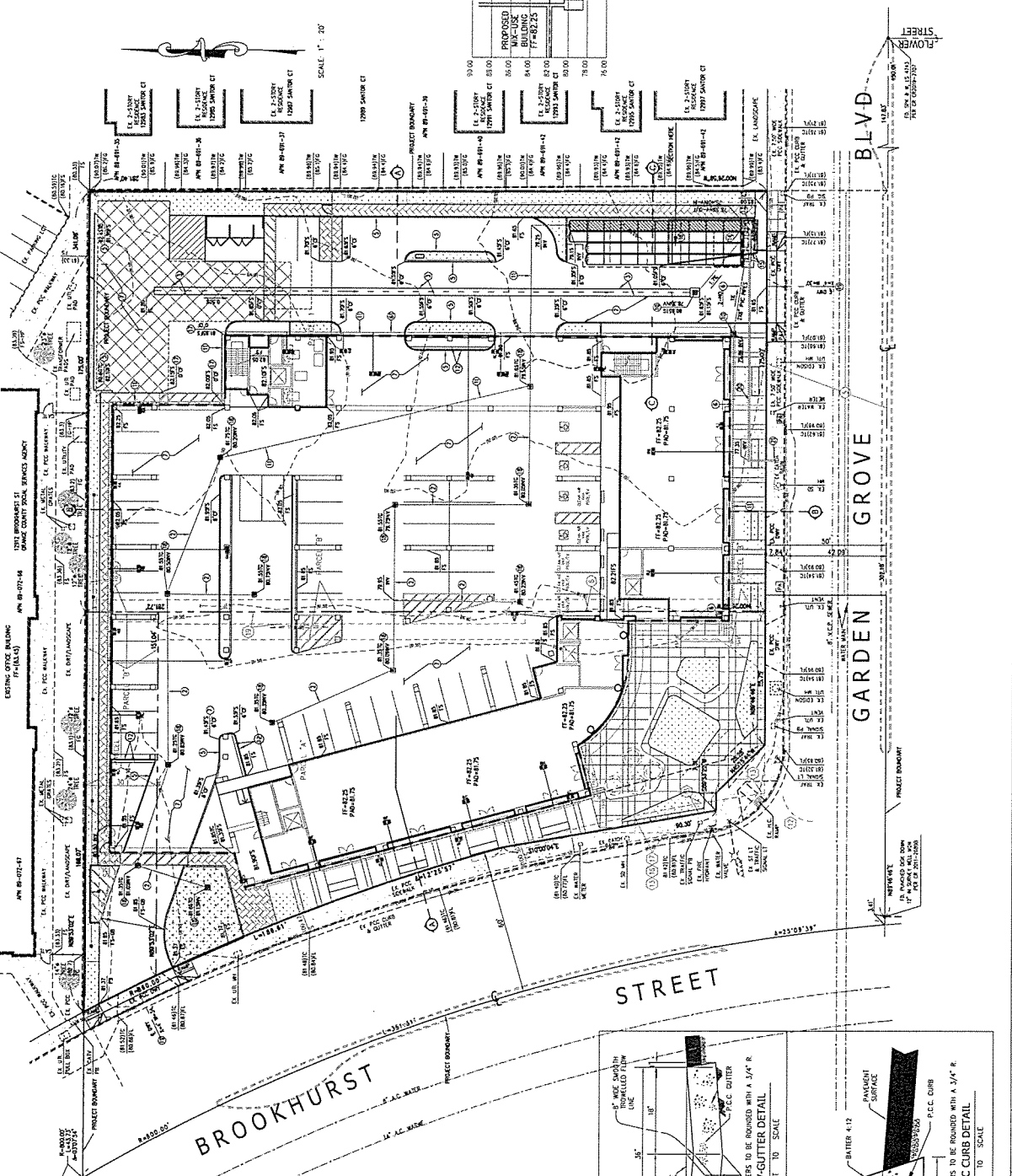
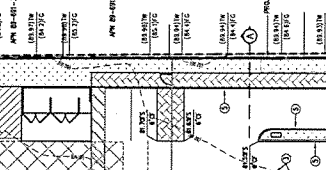
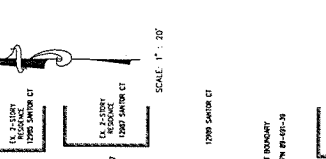
10201 & 10231 GARDEN GROVE BLVD.,  
GARDEN GROVE, CA 92605

DATE: 02/02/2017

NO.	DESCRIPTION
1	PROTECT EX. PLACE
2	CONSTRUCT 4" PVC PIPE SCHEDULE 40
3	CONSTRUCT 36" WIDE PCC V-GUTTER FOR DETAIL SECTION
4	ADJUST EXIST. UTILITY BUNKER & GRATES TO NEW GRADE
5	CONSTRUCT PCC WALKWAY, TRODDERS AND ROCK BASE FOR SOLE REPORT
6	REMOVE (FILL DEPTH) AND RECONSTRUCT EXISTING 24" WIDE AC PARADEWAY
7	CONSTRUCT 10" x 14" GROUT BENCH, BUNKER PRODUCTS INTO OR ON EXIST. W/PAVEMENT INLET FLOOR AND 36" PAVEMENT STORM DRAIN SYSTEM STRENGTH AND DRAINAGE.
8	CONSTRUCT 6" PVC PIPE SCHEDULE 40
9	CONSTRUCT 1/2" WIDE, 2" THICK CONCRETE STOP-AND-STAMP
10	CONSTRUCT 30" DIA. AND WORKLAP ROUND CORNER CURB WITH FRAME, GRATE AND DRAIN SYSTEM STRENGTH AND DRAINAGE AND WITH 24" DEEP SAND, PAINT STAIN UNDERNEATH
11	CONSTRUCT 24" HOPE ACCESS PIPE TO STORMDRAIN UNDERGROUND CHAMBERS (SEALATOR ROW)
12	CONSTRUCT UIC, CHAMBERED HOPE PIPE WITH SANDWICH INTERIOR CONNECTION TO STORMDRAIN
13	CONSTRUCT 3/4" DIA. AND WORKLAP ROUND CORNER CURB WITH 8" END CHAIRS AND AN INSPECTOR PORT PER MANUFACTURER'S DETAILS, RED W/PAVEMENT, BELOW CHAMBER. AT NEED ADVANCE CHECK FOR CHAMBERS AT 10' INTERVALS MANUFACTURER DETAIL ON SHEET 3.
14	CONSTRUCT PCC CURB (6" C.C.) ONLY
15	CONSTRUCT 17" x 17" GROUT BENCH, BUNKER PRODUCTS INTO OR ON EXIST. W/PAVEMENT INLET FLOOR AND 36" PAVEMENT STORM DRAIN SYSTEM STRENGTH AND DRAINAGE.
16	CONSTRUCT 6" PVC PIPE SCHEDULE 40
17	CONSTRUCT 1/2" THICK CONCRETE STOP-AND-STAMP
18	CONSTRUCT UIC, CHAMBERED HOPE PIPE WITH SANDWICH INTERIOR CONNECTION TO STORMDRAIN



NO.	DESCRIPTION
1	PROTECT EX. PLACE
2	CONSTRUCT 4" PVC PIPE SCHEDULE 40
3	CONSTRUCT 36" WIDE PCC V-GUTTER FOR DETAIL SECTION
4	ADJUST EXIST. UTILITY BUNKER & GRATES TO NEW GRADE
5	CONSTRUCT PCC WALKWAY, TRODDERS AND ROCK BASE FOR SOLE REPORT
6	REMOVE (FILL DEPTH) AND RECONSTRUCT EXISTING 24" WIDE AC PARADEWAY
7	CONSTRUCT 10" x 14" GROUT BENCH, BUNKER PRODUCTS INTO OR ON EXIST. W/PAVEMENT INLET FLOOR AND 36" PAVEMENT STORM DRAIN SYSTEM STRENGTH AND DRAINAGE.
8	CONSTRUCT 6" PVC PIPE SCHEDULE 40
9	CONSTRUCT 1/2" WIDE, 2" THICK CONCRETE STOP-AND-STAMP
10	CONSTRUCT 30" DIA. AND WORKLAP ROUND CORNER CURB WITH FRAME, GRATE AND DRAIN SYSTEM STRENGTH AND DRAINAGE AND WITH 24" DEEP SAND, PAINT STAIN UNDERNEATH
11	CONSTRUCT 24" HOPE ACCESS PIPE TO STORMDRAIN UNDERGROUND CHAMBERS (SEALATOR ROW)
12	CONSTRUCT UIC, CHAMBERED HOPE PIPE WITH SANDWICH INTERIOR CONNECTION TO STORMDRAIN
13	CONSTRUCT 3/4" DIA. AND WORKLAP ROUND CORNER CURB WITH 8" END CHAIRS AND AN INSPECTOR PORT PER MANUFACTURER'S DETAILS, RED W/PAVEMENT, BELOW CHAMBER. AT NEED ADVANCE CHECK FOR CHAMBERS AT 10' INTERVALS MANUFACTURER DETAIL ON SHEET 3.
14	CONSTRUCT PCC CURB (6" C.C.) ONLY
15	CONSTRUCT 17" x 17" GROUT BENCH, BUNKER PRODUCTS INTO OR ON EXIST. W/PAVEMENT INLET FLOOR AND 36" PAVEMENT STORM DRAIN SYSTEM STRENGTH AND DRAINAGE.
16	CONSTRUCT 6" PVC PIPE SCHEDULE 40
17	CONSTRUCT 1/2" THICK CONCRETE STOP-AND-STAMP
18	CONSTRUCT UIC, CHAMBERED HOPE PIPE WITH SANDWICH INTERIOR CONNECTION TO STORMDRAIN



DATE: 02/02/2017	PROJECT: 10201 & 10231 GARDEN GROVE BLVD.
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DESIGNED BY: MICHAEL DAVIS	CHECKED BY: JASON WAINMAN
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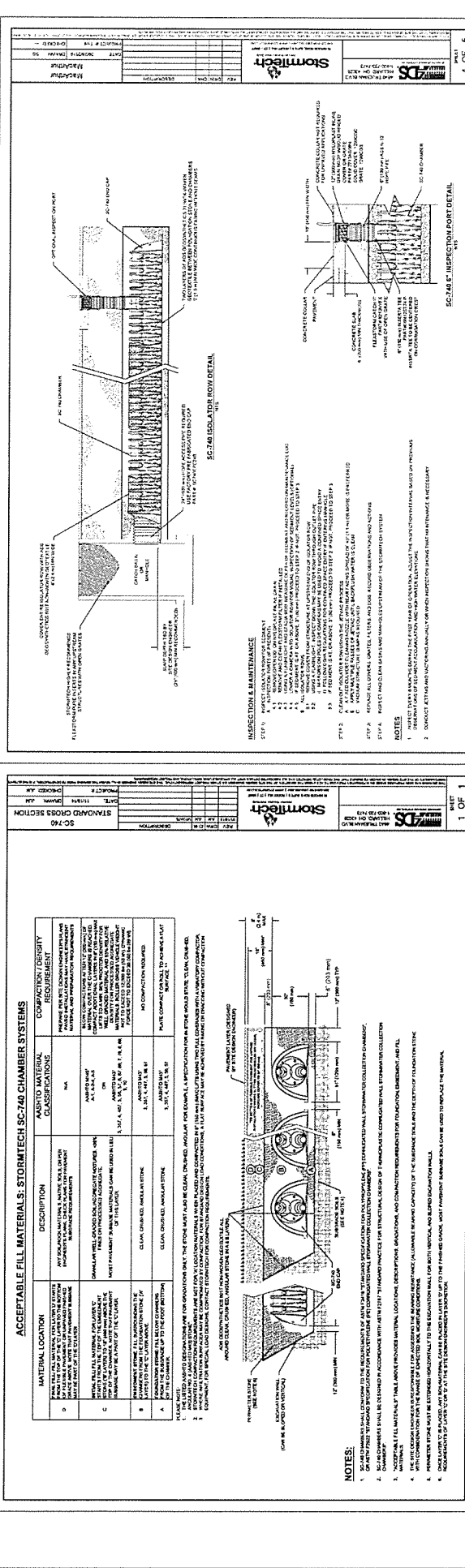
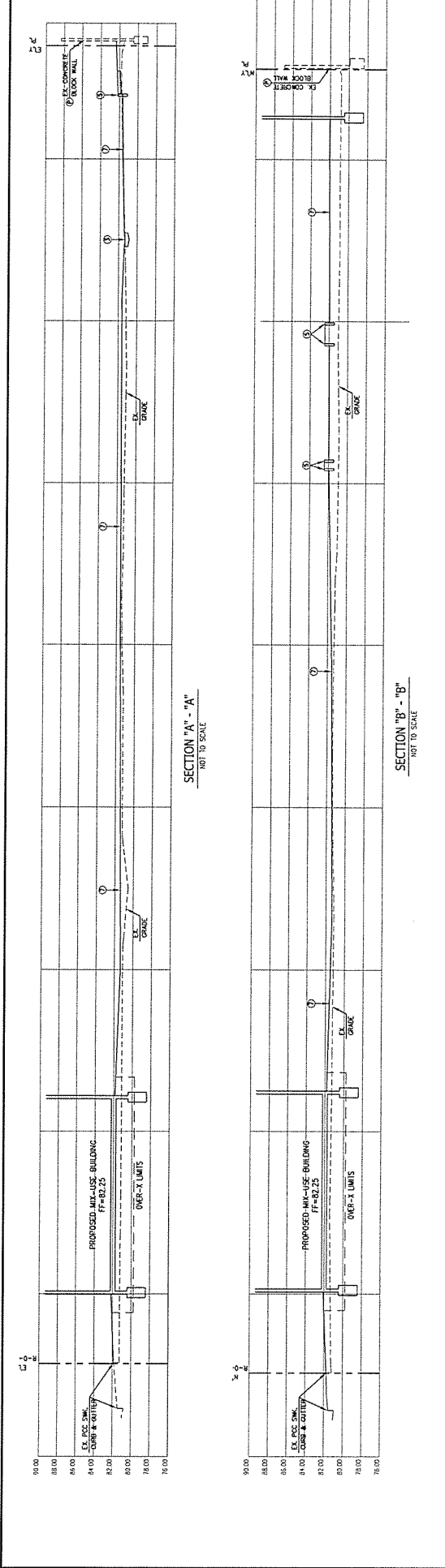
DRAWN BY: JASON WAINMAN	PROJECT NO: 10201 & 10231 GARDEN GROVE BLVD.
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DATE: 02/02/2017	DESCRIPTION: PRELIMINARY GRADING PLAN
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DATE: 02/02/2017	DESCRIPTION: PRELIMINARY GRADING PLAN
------------------	---------------------------------------

DATE: 02/02/2017	DESCRIPTION: PRELIMINARY GRADING PLAN
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**STORMTECH MANUFACTURER'S DETAILS**  
NOT TO SCALE

**16**

**STORMTECH MANUFACTURER'S DETAILS**  
NOT TO SCALE

**OWNER:** CITY OF GARDEN GROVE  
10001 & 102TH GARDEN GROVE BLVD., GARDEN GROVE, CA 92645  
TEL (714) 761-2300  
DATE: 05/21/2020

**PROJECT:** 16083  
NO. 16083  
SHEET NO. 25  
DATE: 05/21/2020

**DRAWN BY:** L. GORDON  
**CHECKED BY:** L. GORDON

**PROJECT LOCATION:** 10001 & 102TH GARDEN GROVE BLVD., GARDEN GROVE, CA 92645

**DATE:** 05/21/2020

**SCALE:** AS SHOWN

**PROJECT:** 16083  
NO. 16083  
SHEET NO. 25  
DATE: 05/21/2020

**OWNER:** CITY OF GARDEN GROVE  
10001 & 102TH GARDEN GROVE BLVD., GARDEN GROVE, CA 92645  
TEL (714) 761-2300  
DATE: 05/21/2020

**PROJECT:** 16083  
NO. 16083  
SHEET NO. 25  
DATE: 05/21/2020

**OWNER:** CITY OF GARDEN GROVE  
10001 & 102TH GARDEN GROVE BLVD., GARDEN GROVE, CA 92645  
TEL (714) 761-2300  
DATE: 05/21/2020

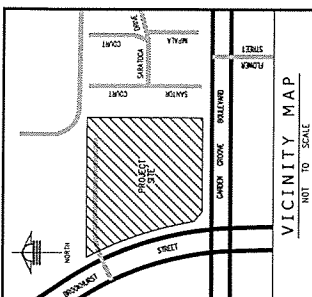
**REVISIONS:**

NO.	DESCRIPTION	DATE	BY
1	ISSUED FOR PERMIT	05/21/2020	L. GORDON

**DATE:** 05/21/2020  
**SCALE:** AS SHOWN

**PROJECT:** 16083  
NO. 16083  
SHEET NO. 25  
DATE: 05/21/2020

**OWNER:** CITY OF GARDEN GROVE  
10001 & 102TH GARDEN GROVE BLVD., GARDEN GROVE, CA 92645  
TEL (714) 761-2300  
DATE: 05/21/2020



- EASEMENT NOTES: (10231 GARDEN GROOVE BLVD.)**
1. EASEMENT FOR STREET AND SIDEWALK IMPROVEMENTS TO THE CITY OF GARDEN GROOVE RECORDED FEBRUARY 03, 1973 AS BOOK 1554 PAGE 211 OF OFFICIAL RECORDS.
  2. EASEMENT FOR STREET AND SIDEWALK IMPROVEMENTS TO THE CITY OF GARDEN GROOVE RECORDED FEBRUARY 03, 1973 AS BOOK 1554 PAGE 211 OF OFFICIAL RECORDS.
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  7. EASEMENT FOR STREET AND SIDEWALK IMPROVEMENTS TO THE CITY OF GARDEN GROOVE RECORDED FEBRUARY 03, 1973 AS BOOK 1554 PAGE 211 OF OFFICIAL RECORDS.
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PREPARED UNDER THE SUPERVISION OF  
 J. J. JONES, C.E.  
 JONES & ASSOCIATES, INC.  
 12000 S. BROADWAY, SUITE 200, GARDEN GROOVE, CA 92644  
 TEL: (714) 541-2900

GRAPHIC SCALE  
 1 inch = 20 feet

CITY OF GARDEN GROVE  
 TENTATIVE PARCEL MAP NO. 2021-190  
 10231 & 10233 GARDEN GROOVE BLVD.,  
 GARDEN GROOVE, CA 92605

DATE: 07/02/2021 (EXISTING CONDITION)

**LEGAL DESCRIPTION:**  
 PARCEL 4  
 THAT PORTION OF THE SOUTHWEST QUARTER OF THE SOUTHWEST QUARTER OF SECTION 15, TOWNSHIP 4 NORTH, RANGE 12 WEST, SAN JUAN BAUTISTA MERIDIAN, COUNTY OF ORANGE, STATE OF CALIFORNIA, BEING THE LAND DESCRIBED AS TO BE A 1/4 ACRES OF LAND, MORE OR LESS, AS SHOWN ON THE RECORDS OF COUNTY CLERK'S OFFICE, COUNTY OF ORANGE, STATE OF CALIFORNIA, AS RECORDED AS BOOK 28, PAGE 13 OF RECORDS OF OFFICIAL RECORDS.

**LEGAL DESCRIPTION:**  
 10231 GARDEN GROOVE BLVD.,  
 GARDEN GROOVE, CA 92605  
 (APRIL 1984) (APR 1984)

**LEGAL DESCRIPTION:**  
 10231 GARDEN GROOVE BLVD.,  
 GARDEN GROOVE, CA 92605  
 (APRIL 1984) (APR 1984)

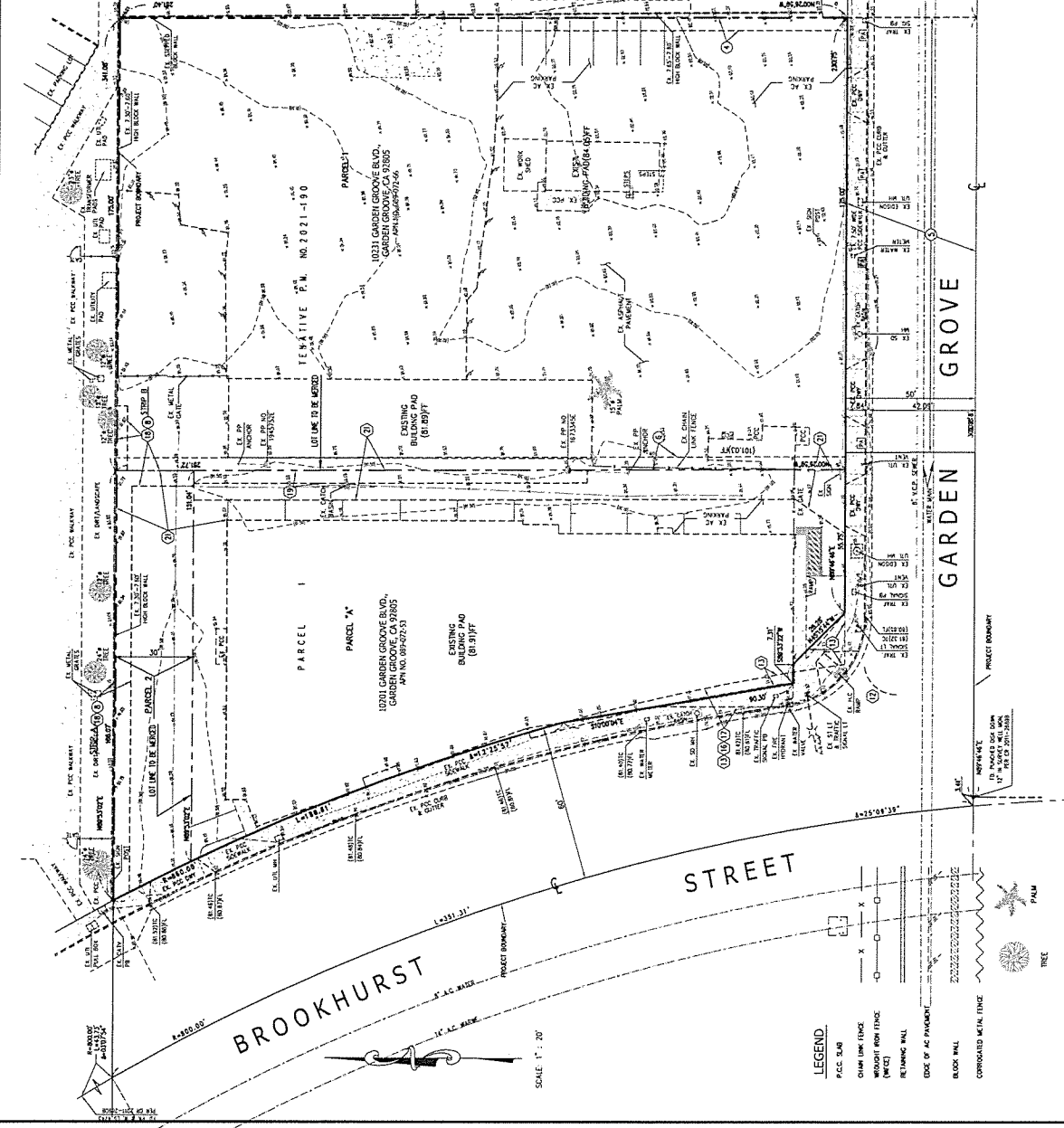
**BENCHMARK:**  
 1157 + 0.377  
 1158 + 0.377

**BASES OF BEARING:**

**EARTHWORK QUANTITIES**

CUT	120
FILL	100
SHRIMPAGE	274
SHRIMPAGE	274

**EARTHWORK NOTE:**  
 NOTE: \* INCLUDES SHRIMPAGE FACTOR OF 15%  
 EARTHWORK QUANTITIES ARE ESTIMATES ONLY. THEY DO NOT REFLECT THE QUANTITIES TO BE ORDERED FOR CONSTRUCTION. THE QUANTITIES SHOWN ABOVE ARE INTENDED FOR USE IN ESTABLISHING THE QUANTITIES FOR THE CONTRACTOR'S RESPONSIBILITY TO BALANCE THE SITE SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR.

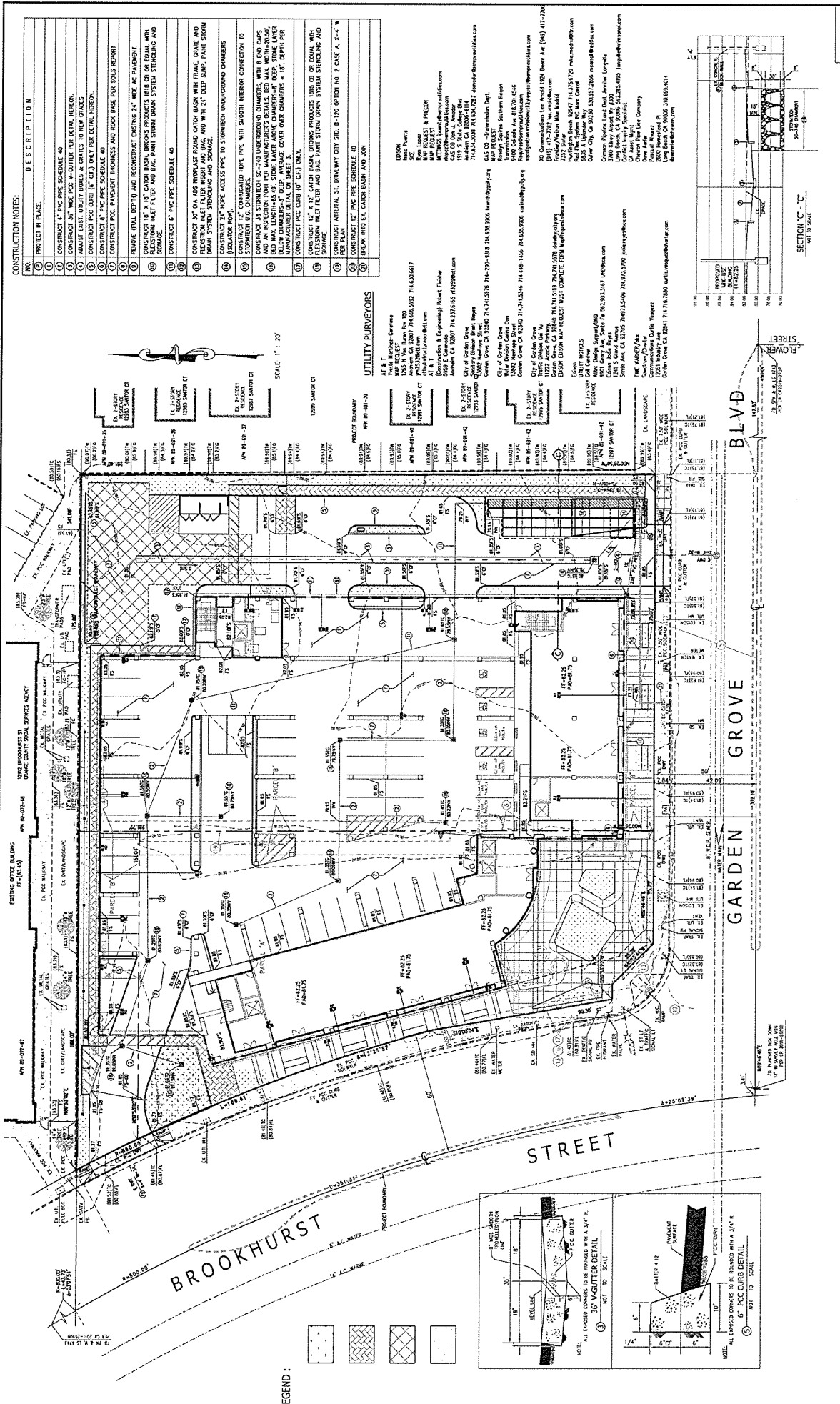


DATE	BY	DESCRIPTION	REVISIONS

DATE: 07/02/2021  
 CHECKED BY: JJJ  
 DRAWN BY: JJJ

PLANS PREPARED UNDER SUPERVISION OF:  
 J. J. JONES, C.E.  
 JONES & ASSOCIATES, INC.  
 12000 S. BROADWAY, SUITE 200, GARDEN GROOVE, CA 92644  
 TEL: (714) 541-2900

PROJECT NO. 025-107



**CONSTRUCTION NOTES:**

NO.	DESCRIPTION
1	PROTECT IN PLACE
2	CONTRACTOR TO PROTECT SCHEDULE 40
3	CONTRACTOR TO PROTECT 3" WOOD PIPE V-DIFFER PER DETAIL HEREON
4	ADJUST CURB UNITS TO MATCH EXISTING CURBS TO NEW GRABES
5	CONTRACTOR TO PROTECT SCHEDULE 40
6	CONTRACTOR TO PROTECT 3" WOOD PIPE DETAIL HEREON
7	CONTRACTOR TO PROTECT 2" WOOD PIPE DETAIL HEREON
8	REMOVE (DUAL DEPTH) AND RECONSTRUCT EXISTING 24" WOOD PIPE DETAIL HEREON
9	CONTRACTOR TO PROVIDE 24" WOOD PIPE DETAIL HEREON
10	CONTRACTOR TO PROVIDE 24" WOOD PIPE DETAIL HEREON
11	CONTRACTOR TO PROVIDE 24" WOOD PIPE DETAIL HEREON
12	CONTRACTOR TO PROVIDE 24" WOOD PIPE DETAIL HEREON
13	CONTRACTOR TO PROVIDE 24" WOOD PIPE DETAIL HEREON
14	CONTRACTOR TO PROVIDE 24" WOOD PIPE DETAIL HEREON
15	CONTRACTOR TO PROVIDE 24" WOOD PIPE DETAIL HEREON
16	CONTRACTOR TO PROVIDE 24" WOOD PIPE DETAIL HEREON
17	CONTRACTOR TO PROVIDE 24" WOOD PIPE DETAIL HEREON
18	CONTRACTOR TO PROVIDE 24" WOOD PIPE DETAIL HEREON
19	CONTRACTOR TO PROVIDE 24" WOOD PIPE DETAIL HEREON
20	CONTRACTOR TO PROVIDE 24" WOOD PIPE DETAIL HEREON

**UTILITY PURVEYORS**

AT & T  
 1500 N. Main Street, Suite 100  
 1502 N. Main Street, Suite 100  
 1502 N. Main Street, Suite 100  
 1502 N. Main Street, Suite 100

Southern California Gas Company  
 1000 N. Main Street, Suite 100  
 1000 N. Main Street, Suite 100  
 1000 N. Main Street, Suite 100

City of Garden Grove  
 1000 N. Main Street, Suite 100  
 1000 N. Main Street, Suite 100  
 1000 N. Main Street, Suite 100

City of Orange  
 1000 N. Main Street, Suite 100  
 1000 N. Main Street, Suite 100  
 1000 N. Main Street, Suite 100

City of Santa Ana  
 1000 N. Main Street, Suite 100  
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 1000 N. Main Street, Suite 100

City of Tustin  
 1000 N. Main Street, Suite 100  
 1000 N. Main Street, Suite 100  
 1000 N. Main Street, Suite 100

City of Westminster  
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City of Fullerton  
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 1000 N. Main Street, Suite 100

City of Anaheim  
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City of Brea  
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City of San Juan Capistrano  
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City of Newport Beach  
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City of Laguna Hills  
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City of San Jose  
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City of San Bernardino  
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City of Redlands  
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City of Fontana  
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City of Rialto  
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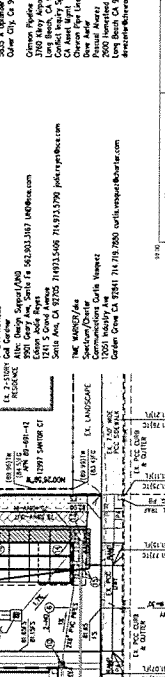
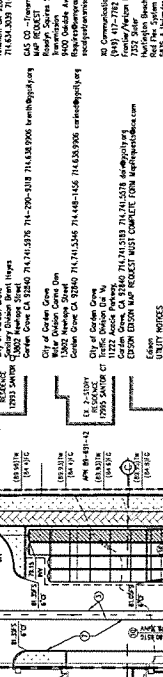
City of San Marcos  
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City of Escondido  
 1000 N. Main Street, Suite 100  
 1000 N. Main Street, Suite 100  
 1000 N. Main Street, Suite 100

City of San Diego  
 1000 N. Main Street, Suite 100  
 1000 N. Main Street, Suite 100  
 1000 N. Main Street, Suite 100

**LEGEND:**

- 1. 12" CONC. CURB
- 2. 12" CONC. CURB
- 3. 12" CONC. CURB
- 4. 12" CONC. CURB
- 5. 12" CONC. CURB
- 6. 12" CONC. CURB
- 7. 12" CONC. CURB
- 8. 12" CONC. CURB
- 9. 12" CONC. CURB
- 10. 12" CONC. CURB
- 11. 12" CONC. CURB
- 12. 12" CONC. CURB
- 13. 12" CONC. CURB
- 14. 12" CONC. CURB
- 15. 12" CONC. CURB
- 16. 12" CONC. CURB
- 17. 12" CONC. CURB
- 18. 12" CONC. CURB
- 19. 12" CONC. CURB
- 20. 12" CONC. CURB



**LEGAL DESCRIPTION:**

RECORD MAP NO. 100000  
 SHEET 100000  
 SHEET 100000  
 SHEET 100000

DATE: 07/20/2010

PROJECT NO. 475-102

**CITY OF GARDEN GROVE**  
 TENTATIVE PARCEL MAP NO. 2021-190  
 PRELIMINARY GRADING PLAN  
 10001 & 10231 GARDEN GROVE BLVD.,  
 GARDEN GROVE, CA 92645

DATE: 07/20/2010

FOR: FORESIGHT ENGINEERING, INC., 475  
 10001 & 10231 GARDEN GROVE BLVD.,  
 GARDEN GROVE, CA 92645  
 TEL: (714) 942-2111 FAX: (714) 942-2117

DRAWN BY: MJD  
 CHECKED BY: MJD  
 PLANS PREPARED UNDER SUPERVISION OF:  
 MANSUR J. GONZALEZ  
 DATE: 07/20/2010  
 E.C.E. NO. 46119

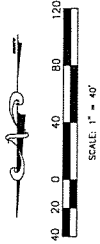
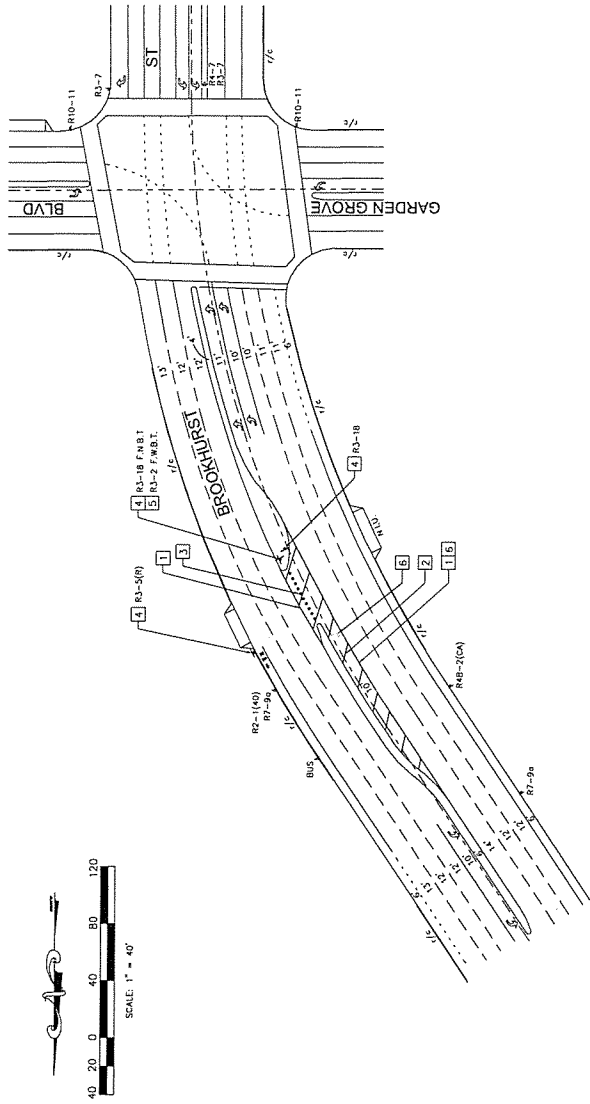
LEGAL DESCRIPTION:  
 REFERENCE PLANS FOR THIS IMPROVEMENTS  
 DATE: 07/20/2010  
 PREPARED BY:  
 DATE: 07/20/2010  
 PREPARED BY:

**CONSTRUCTION NOTES:**

1. INSTALL 6" YELLOW LEFT EDGE LINE AND MARKERS PER CALTRANS STANDARD PLAN AZ09B DETAIL Z5.
2. INSTALL 4" YELLOW DIAGONALS AT 20' EACH.
3. INSTALL GLUE-DOWN CHANNELIZER AT 4' EACH.
4. INSTALL NEW SIGN POST AND PANEL AS SHOWN.
5. INSTALL NEW SIGN PANEL AS SHOWN.
6. REMOVE CONFLICTING PAVEMENT MARKING AND MARKER.

**STRIPING GENERAL NOTES:**

1. ALL STRIPING AND PAVEMENT MARKINGS SHALL CONFORM TO THE 2014 EDITION OF THE CALIFORNIA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (CA-MUTCD).
2. ALL STRIPING ALIGNMENT AND LAYOUT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR AND IS SUBJECT TO THE CITY ENGINEER'S APPROVAL.
3. THE CONTRACTOR SHALL NOTIFY THE CITY ENGINEER UPON THE COMPLETION OF STRIPING AND SIGNING.
4. ALL CONFLICTING SIGNING AND STRIPING SHALL BE REMOVED BY SANDBLASTING. SALVAGED SIGNS AND POSTS SHALL BE RETURNED TO CITY-YARD, COORDINATE WITH THE CITY ENGINEER.
5. ALL STRIPING DETAILS, PAVEMENT MARKINGS SHALL BE TWO-COAT PAINT UNLESS OTHERWISE INDICATED.
6. ALL EXISTING STRIPING AND PAVEMENT MARKINGS SHOWN ON THE PLAN THAT ARE BEING REMOVED, SHALL BE REMOVED BY SANDBLASTING/GRINDING OR OTHER APPROVED METHODS. THE CONTRACTOR SHALL NOTIFY THE CITY ENGINEER UPON THE COMPLETION OF STRIPING. LINES IS PERMITTED IN THE CITY, ALL CONFLICTING BASED PAVEMENT MARKINGS SHALL BE REMOVED. PAVEMENT THAT IS DAMAGED DUE TO REMOVAL OF MARKERS SHALL BE REPAIRED TO THE SATISFACTION OF THE CITY REPRESENTATIVE.
7. STRIPING SHALL BE CAT-TRACKED AND APPROVED BY THE CITY REPRESENTATIVE PRIOR TO FINAL INSTALLATION.



**LEGEND:**

- EXISTING RED CURB MARKINGS
- EXISTING DRIVEWAY.
- EXISTING SIGN AND POST.
- PROPOSED SIGN AND POST.
- F.N.B.T. FACE NORTHBOUND TRAFFIC
- F.W.B.T. FACE WESTBOUND TRAFFIC
- N.L.U. NOT IN USE

**SIGN LEGEND:**

- PROPOSED: R3-2
- R3-5(R)
- R3-10
- R3-11
- R48-2(CA)
- R7-9a
- EXISTING: R2-1(40)
- R3-7
- R4-7

PREPARED BY:  
**KZ TRAFFIC ENGINEERING, INC.**  
 1442 AVANCE BLVD, SUITE 210  
 COSTA MESA, CA 92626  
 TEL: (714) 947-2116

NO.	DATE	REVISIONS	DESIGNER	CHECKER	DATE

PREPARED BY THE SURVEYOR OF PUBLIC WORKS  
 ASSESSING ENGINEER  
 DATE: 07/16/2021  
 APPROVED:  
 DATE:



SIGNING AND STRIPING PLAN  
**BROOKHURST STREET**  
 CITY OF GARDEN GROVE  
 DEPARTMENT OF PUBLIC WORKS

**SITE PLAN SYMBOL LEGEND**

[Pattern]	PROJECT BUILDING
[Pattern]	HAMMER HEAD PER ADA STANDARD
[Pattern]	LANDSCAPE AREA, SEE LANDSCAPE
[Pattern]	ROULETTING PLAZA, 25% OF PLAZA AREA SHALL BE LANDSCAPED
[Pattern]	PEDESTRIAN WALKWAY, PAVED & LANDSCAPED
[Pattern]	(F) BUILDING

DIFFERENTIAL MARKING (200PT MIN, TRIMITE WHITE PAINT)

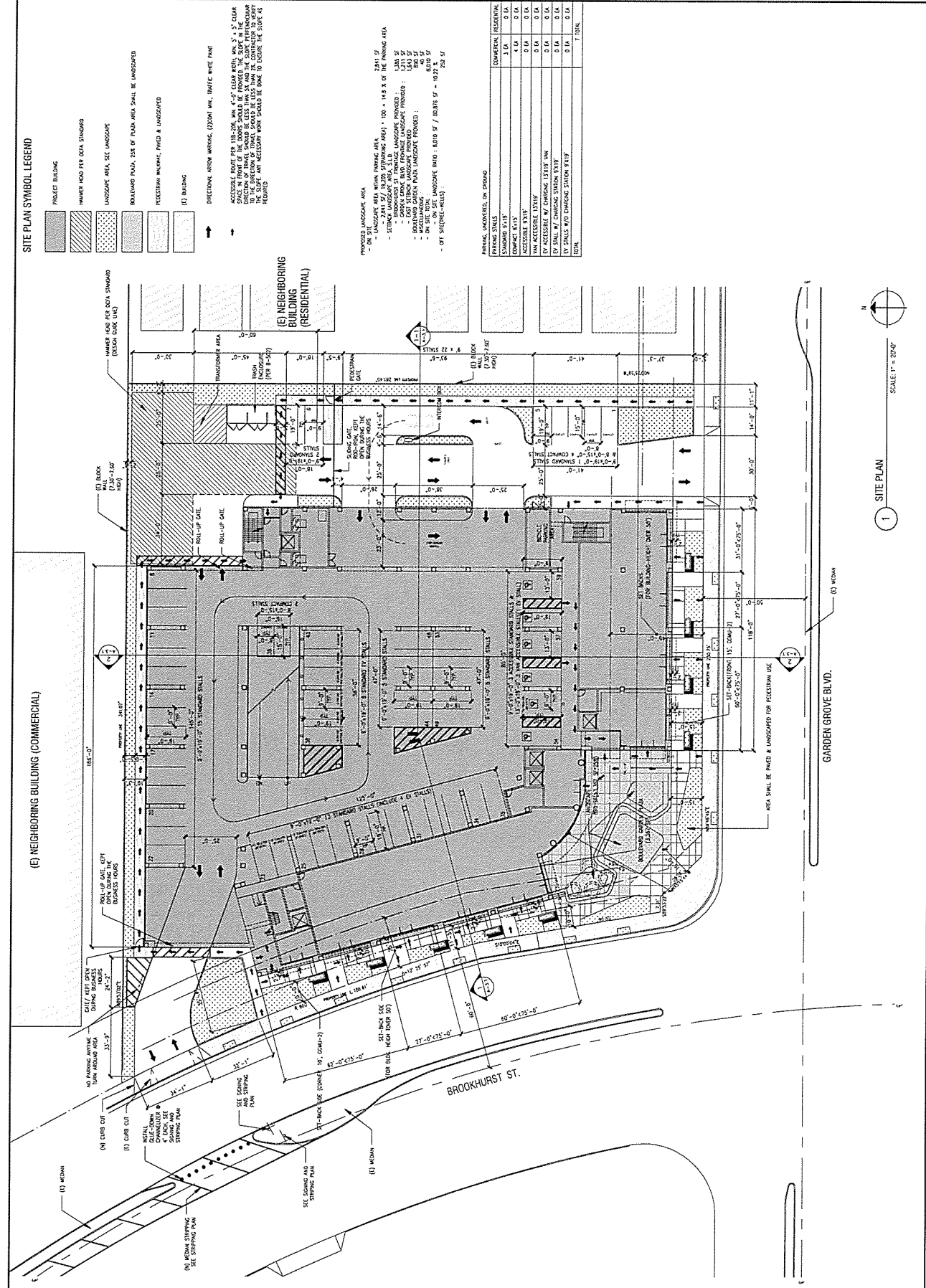
ACCESSIBLE ROUTE PER 119.206, MIN. 4'-0" CLEAR WIDTH, MIN. 5' x 5' CLEAR SECTION OF TRAVEL, SLOPED BELT LIPS, MAX. 1/4" AND THE SLOPE OF THE BELT LIPS TO BE 1:4. ANY ACCESSIBLE ROUTE SHALL BE MARKED TO ASSURE THE SLOPE IS REQUIRED.

**PROPOSED LANDSCAPE AREA**

- ON SITE LANDSCAPE AREA WITHIN PARKING AREA: 2,841 SF
- SITE - 2,841 SF / 19,200 PARKING AREA: 100 x 14.8 X OF THE PARKING AREA
- PROPOSED AT TERRAZZO LANDSCAPE PROVIDED: 1,335 SF
- OFF SITE LANDSCAPE PROVIDED: 1,506 SF
- TOTAL LANDSCAPE PROVIDED: 2,841 SF
- ON SITE TOTAL LANDSCAPE: 8,820 SF
- OFF SITE (TREE-WELLS) LANDSCAPE: 8,000 SF / 10,014 SF = 1077.25 SF

**PARKING, UNCOVERED, ON GRASS**

DESCRIPTION	COMMERICAL	RESIDENTIAL
CONCRETE DRIVE	2.0	0.0
ASPHALT DRIVE	4.0	0.0
ACCESSIBLE DRIVE	0.0	0.0
BY ACCESSIBLE W/ CHANGING STATION	0.0	0.0
BY STALLS W/O CHANGING STATION	0.0	0.0
BY STALLS W/O CHANGING STATION	0.0	0.0
<b>TOTAL</b>	<b>6.0</b>	<b>0.0</b>



1 SITE PLAN SCALE: 1" = 20'-0"

DATE	REVISION	BY

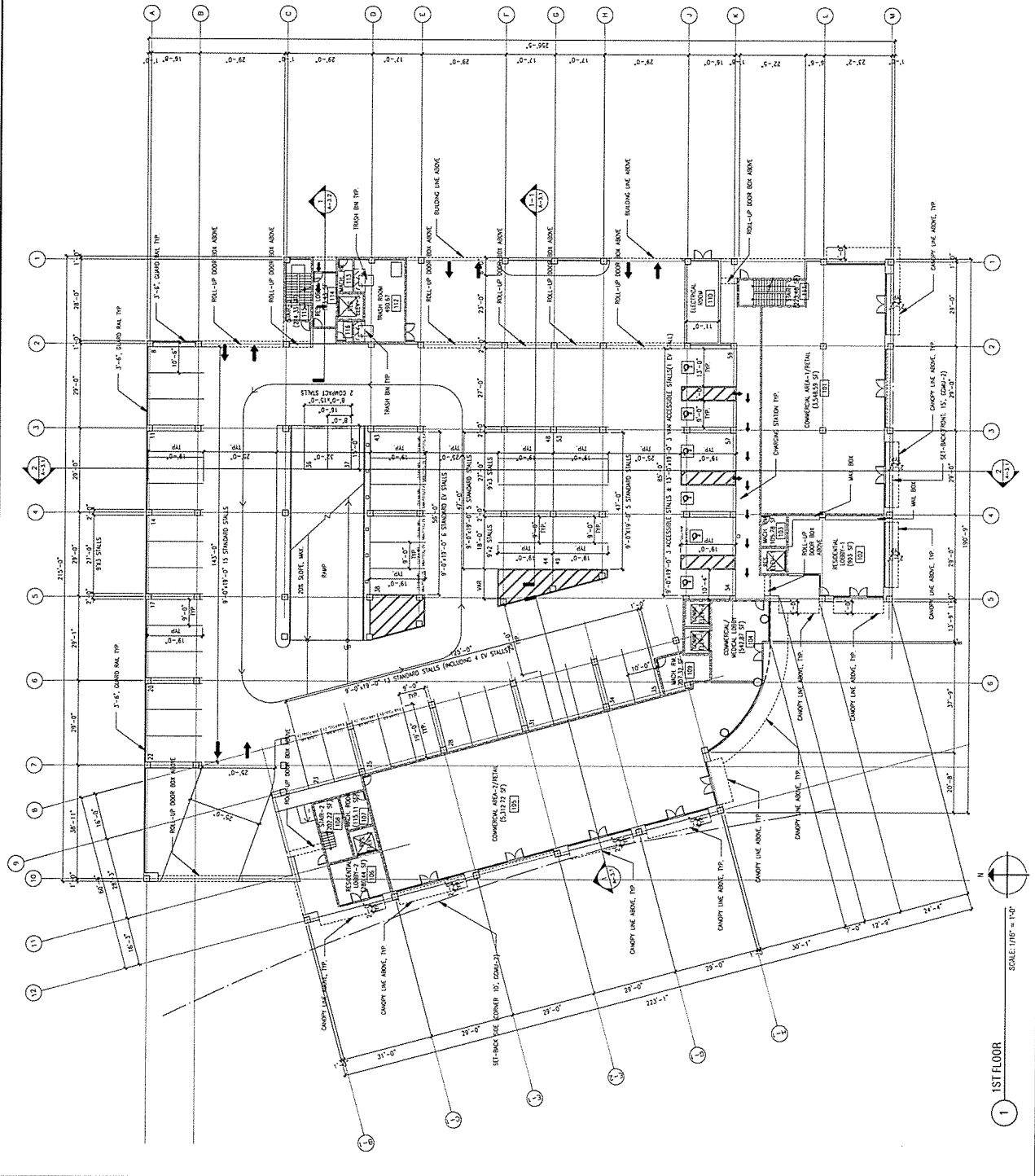
- NOTES**
1. SEE SCHEDULE 1-HR. SEPARATION BETWEEN RUMF-105-2
  2. STAIRS SHOWN & SHALL BE PROVIDED BY EDC RATED ENCLOSURES PER EDC
  3. ROLL-UP DOORS LEFT OPEN DURING THE BUSINESS HOURS
  4. ALL FINISHING SHALL MEET STANDARD (9-03) 14-07

**PLAN SYMBOL LEGEND**

BY STAIR CHARGING STATION

ALL CLEAR HEIGHTS SHALL BE 7'-0" CLEAR FROM FINISH FLOOR TO FINISH CEILING. ALL CLEAR HEIGHTS SHALL BE 7'-0" CLEAR FROM FINISH FLOOR TO FINISH CEILING. ALL CLEAR HEIGHTS SHALL BE 7'-0" CLEAR FROM FINISH FLOOR TO FINISH CEILING. ALL CLEAR HEIGHTS SHALL BE 7'-0" CLEAR FROM FINISH FLOOR TO FINISH CEILING.

1ST FLOOR CHARGING STATION	COMPOSITION	REVISION
STAIRS	3.0A	0.0A
COMPART STAIRS	2.0A	0.0A
ACCESSIBLE STAIRS	3.0A	0.0A
WH ACCESSIBLE STAIRS	2.0A	0.0A
EV ACCESSIBLE BY CHARGING STATION STAIRS	1.0A	0.0A
EV STAIRS W/O CHARGING STATION STAIRS	4.0A	0.0A
TOTAL		50 TOTAL



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DATE: 10/23/2024  
DRAWN BY: J. SMITH  
CHECKED BY: M. JONES  
APPROVED BY: S. ROBERTS

PROJECT: MIXED-USE BUILDING FOR DR. DAO  
10231 & 10231 GARDEN GROVE BLVD  
GARDEN GROVE, CA 92643-1041  
MR. MICHAEL DAO

DATE: 10/23/2024  
DRAWN BY: J. SMITH  
CHECKED BY: M. JONES  
APPROVED BY: S. ROBERTS

DATE: 10/23/2024  
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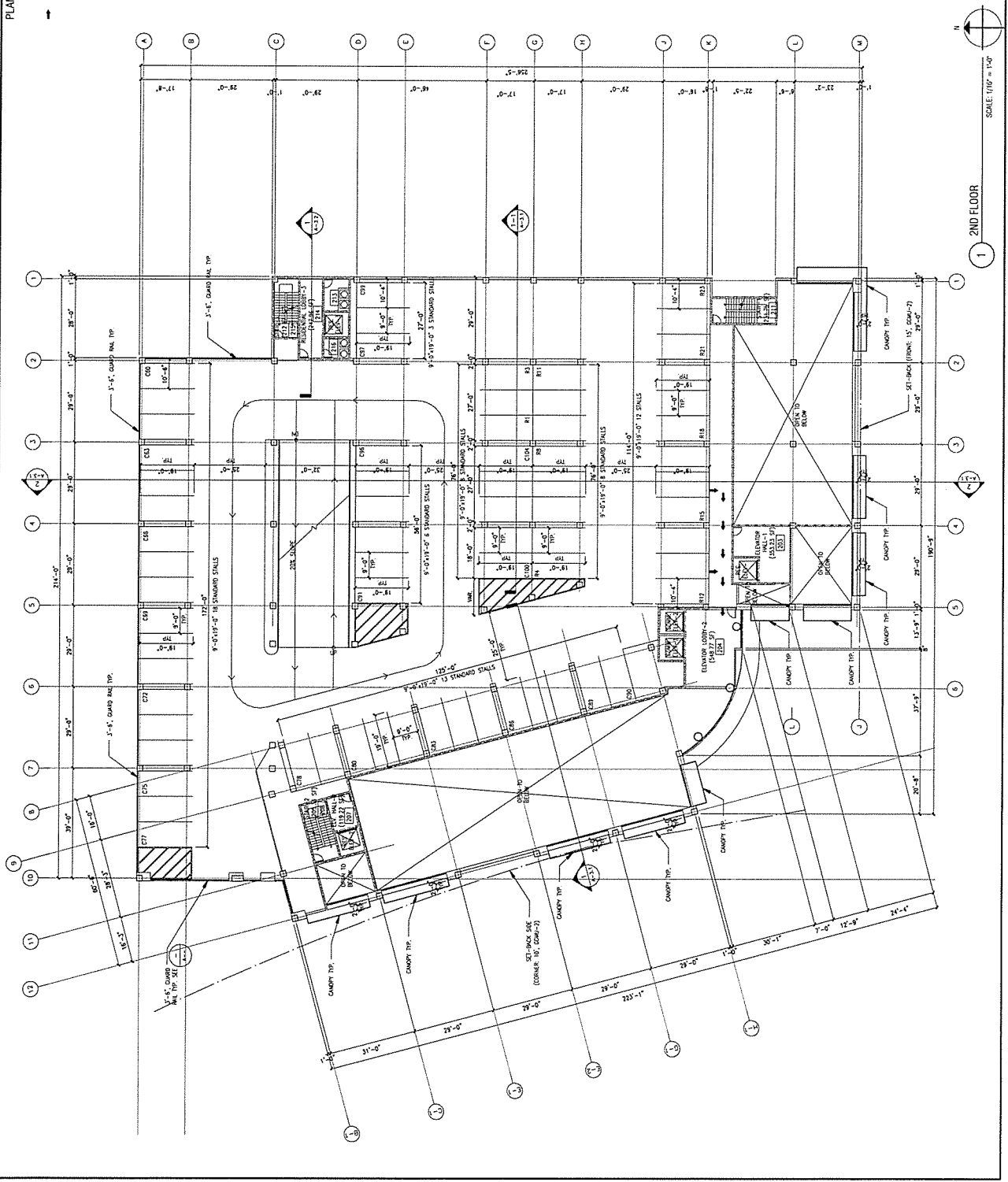
DATE: 10/23/2024  
DRAWN BY: J. SMITH  
CHECKED BY: M. JONES  
APPROVED BY: S. ROBERTS

DATE: 10/23/2024  
DRAWN BY: J. SMITH  
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APPROVED BY: S. ROBERTS

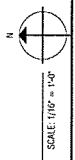
PLAN SYMBOL LEGEND

ACCESSIBLE ROUTE FOR 18"-24" MIN. 4'-0" CLEAR WIDTH, MIN. 4'-0" CLEAR SPACE IN FRONT OF THE ROUTE SHOULD BE PROVIDED. THE ROUTE IN THE ACCESSIBLE ROUTE SHALL BE 48" WIDE. THE ROUTE SHALL BE 48" WIDE TO THE CENTERLINE OF TRAVEL SHOULD BE LESS THAN 24" CONSIDERED TO BE A SIDEWALK. ANY NECESSARY WORK SHOULD BE DONE TO ENSURE THE SIGN AS INDICATED.

2ND FLOOR PARKING GARAGE	COMMERCIAL	RESIDENTIAL
STANDARD STALL	58 EA	10 EA
COMPACT STALL	0 EA	0 EA
ACCESSIBLE STALL	0 EA	0 EA
EV STALL	0 EA	0 EA
EV STALL WITH CHARGING STATION	0 EA	0 EA
EV STALL WITH CHARGING STATION	0 EA	0 EA
TOTAL	58	10



- NOTES:
1. 1-1/2" MIN. SEPARATION BETWEEN RAMP-TO-2
  2. STAIRS SHALL BE PROVIDED WITH 1/2" MIN. RISE AND 1/2" MIN. RUN FOR EACH STEP.
  3. ALL PARKING STALLS ARE STANDARD (9'-0" X 19'-0")



2ND FLOOR  
1

SCALE: 1/8" = 1'-0"

PLAN SYMBOL LEGEND

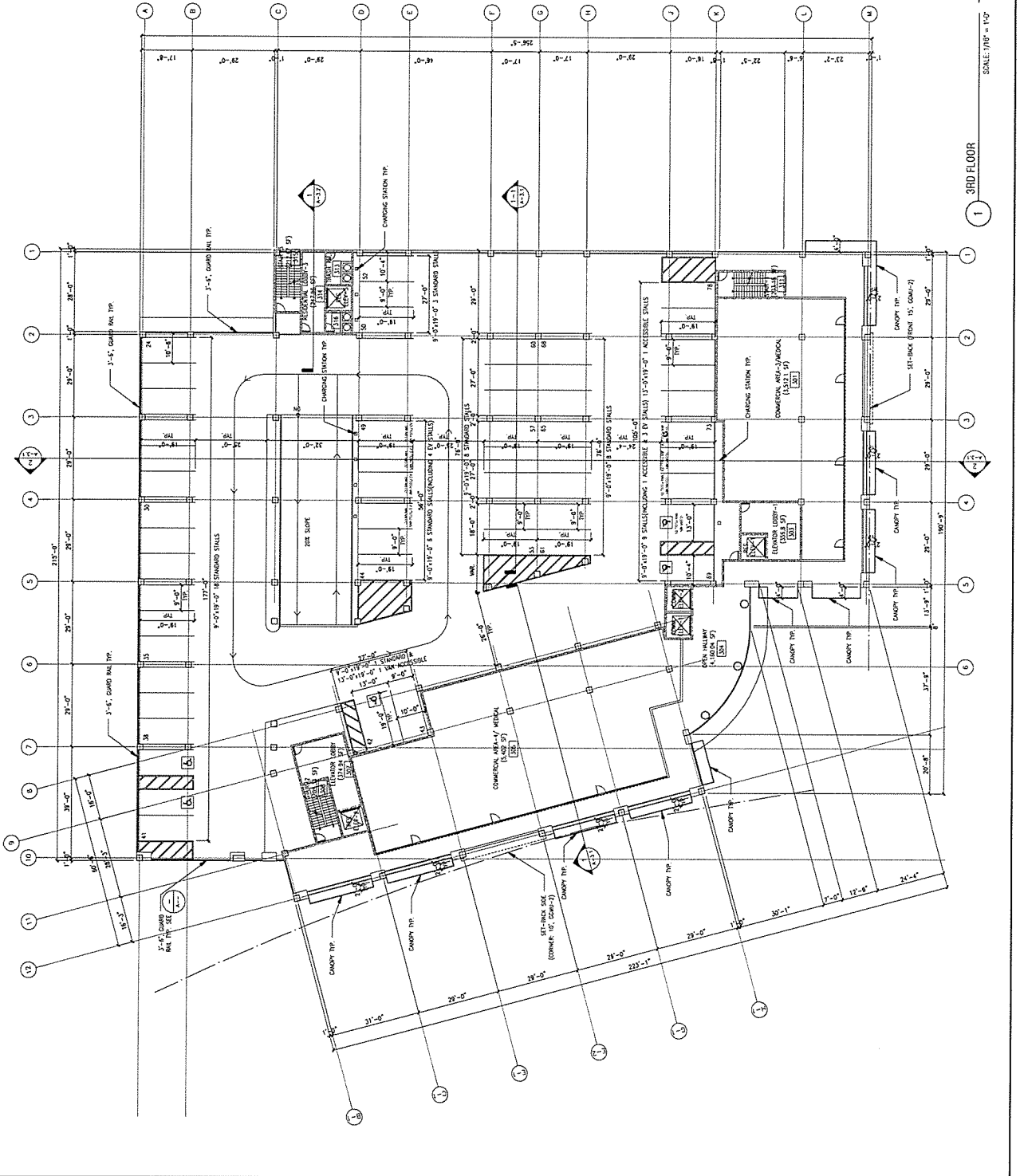
○ BY STALL CHANGING STATION

3RD FLOOR PARKING DANCE

COMPONENT	PERSONAL
STALLS	55 TOTAL
ACCESSIBLE 8'x12'	3 EA
ACCESSIBLE 6'x8'	1 EA
NON ACCESSIBLE 8'x12'	0 EA
NON ACCESSIBLE 6'x8'	0 EA
BY STALL W/ CHANGING STATION 8'x12'	0 EA
BY STALL W/ CHANGING STATION 6'x8'	0 EA
BY STALL W/O CHANGING STATION 8'x12'	0 EA
BY STALL W/O CHANGING STATION 6'x8'	3 EA
TOTAL	56 TOTAL

NOTES

- 1. ...
- 2. STALLS SHALL BE PROVIDED BY THE OWNER.
- 3. ALL PARKING STALLS ARE STANDARD 8'-0" X 12'-0"



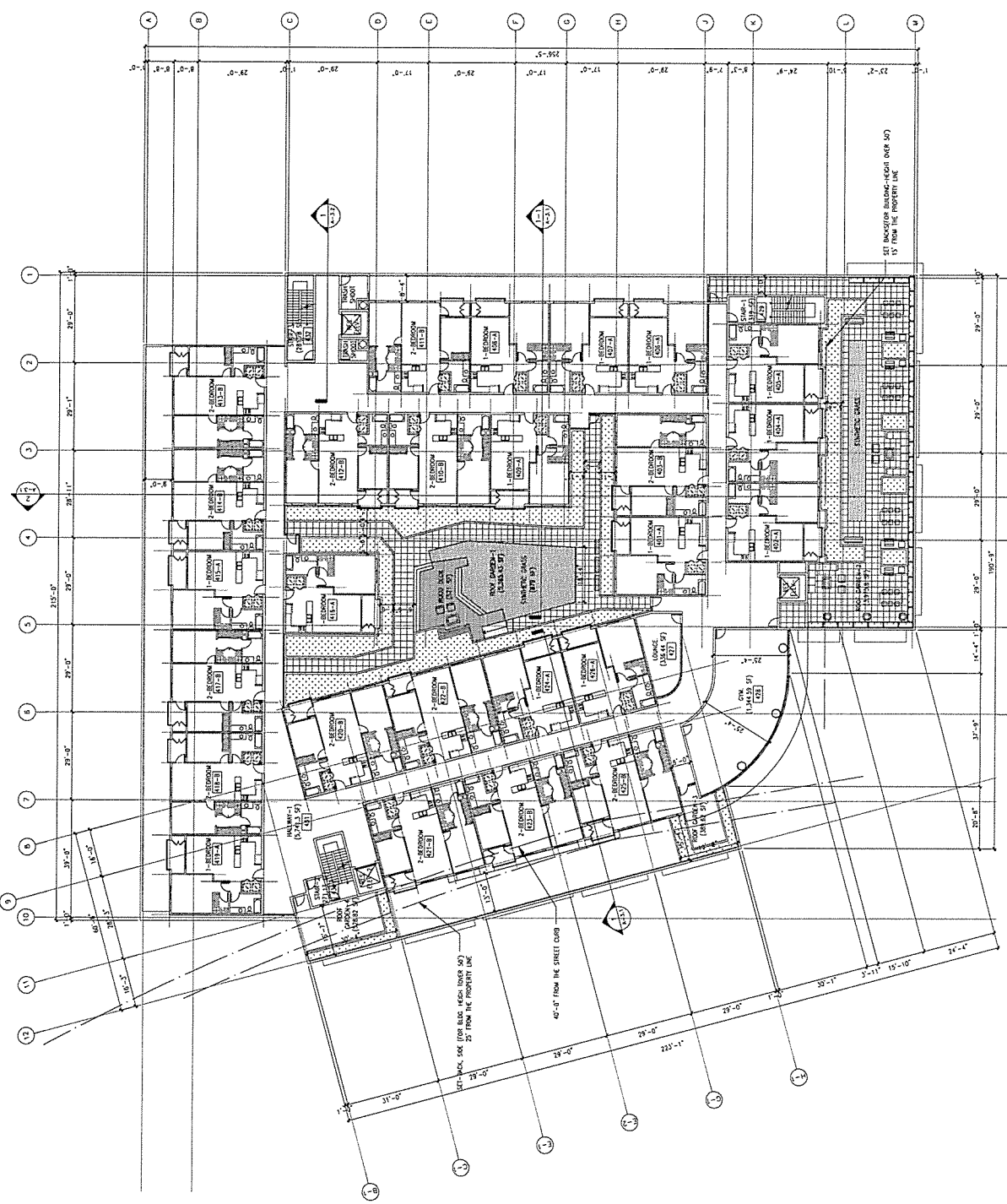


**PLAN SYMBOL LEGEND**

- LANDSCAPE AREA, S.L.O.
- WOOD DECK, S.L.O.
- WOOD BECK, S.L.O.
- RECREATION AREA, WOOD DECK, S.L.O.
- RECREATION AREA, SYNTHETIC GRASS, S.L.O.
- RECREATION AREA, SYNTHETIC GRASS, S.L.O.
- RECREATION AREA, SYNTHETIC GRASS, S.L.O.

**NOTES**

- LANDSCAPE AREA PROPOSED: 2,978 SF, S.L.O.
- ACTIVE RECREATION AREA: 1,143 SF
- ROOF GARDEN-1: 3,572.00 SF
- ROOF GARDEN-2: 3,572.00 SF
- ROOF GARDEN-3: 3,572.00 SF
- ROOF GARDEN-4: 3,572.00 SF
- ROOF GARDEN-5: 3,572.00 SF
- ROOF GARDEN-6: 3,572.00 SF
- ROOF GARDEN-7: 3,572.00 SF
- ROOF GARDEN-8: 3,572.00 SF
- ROOF GARDEN-9: 3,572.00 SF
- ROOF GARDEN-10: 3,572.00 SF
- ROOF GARDEN-11: 3,572.00 SF
- ROOF GARDEN-12: 3,572.00 SF
- ROOF GARDEN-13: 3,572.00 SF
- ROOF GARDEN-14: 3,572.00 SF
- ROOF GARDEN-15: 3,572.00 SF
- ROOF GARDEN-16: 3,572.00 SF
- ROOF GARDEN-17: 3,572.00 SF
- ROOF GARDEN-18: 3,572.00 SF
- ROOF GARDEN-19: 3,572.00 SF
- ROOF GARDEN-20: 3,572.00 SF
- ROOF GARDEN-21: 3,572.00 SF
- ROOF GARDEN-22: 3,572.00 SF
- ROOF GARDEN-23: 3,572.00 SF
- ROOF GARDEN-24: 3,572.00 SF
- ROOF GARDEN-25: 3,572.00 SF
- ROOF GARDEN-26: 3,572.00 SF
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- ROOF GARDEN-93: 3,572.00 SF
- ROOF GARDEN-94: 3,572.00 SF
- ROOF GARDEN-95: 3,572.00 SF
- ROOF GARDEN-96: 3,572.00 SF
- ROOF GARDEN-97: 3,572.00 SF
- ROOF GARDEN-98: 3,572.00 SF
- ROOF GARDEN-99: 3,572.00 SF
- ROOF GARDEN-100: 3,572.00 SF



SCALE: 1/8" = 1'-0"  
4TH FLOOR

10201 & 10231 GARDEN GROVE BLVD  
GARDEN GROVE, CA 92643  
MR. MICHAEL DAO  
PROJECT  
5TH FLOOR PLAN  
DATE: 12/15/11  
REVISION: 01  
DRAWN BY: AMANDA'S  
CHECKED BY: SAUL'S  
SCALE: A-1.6  
SHEET: 05  
DATE: 12/15/11

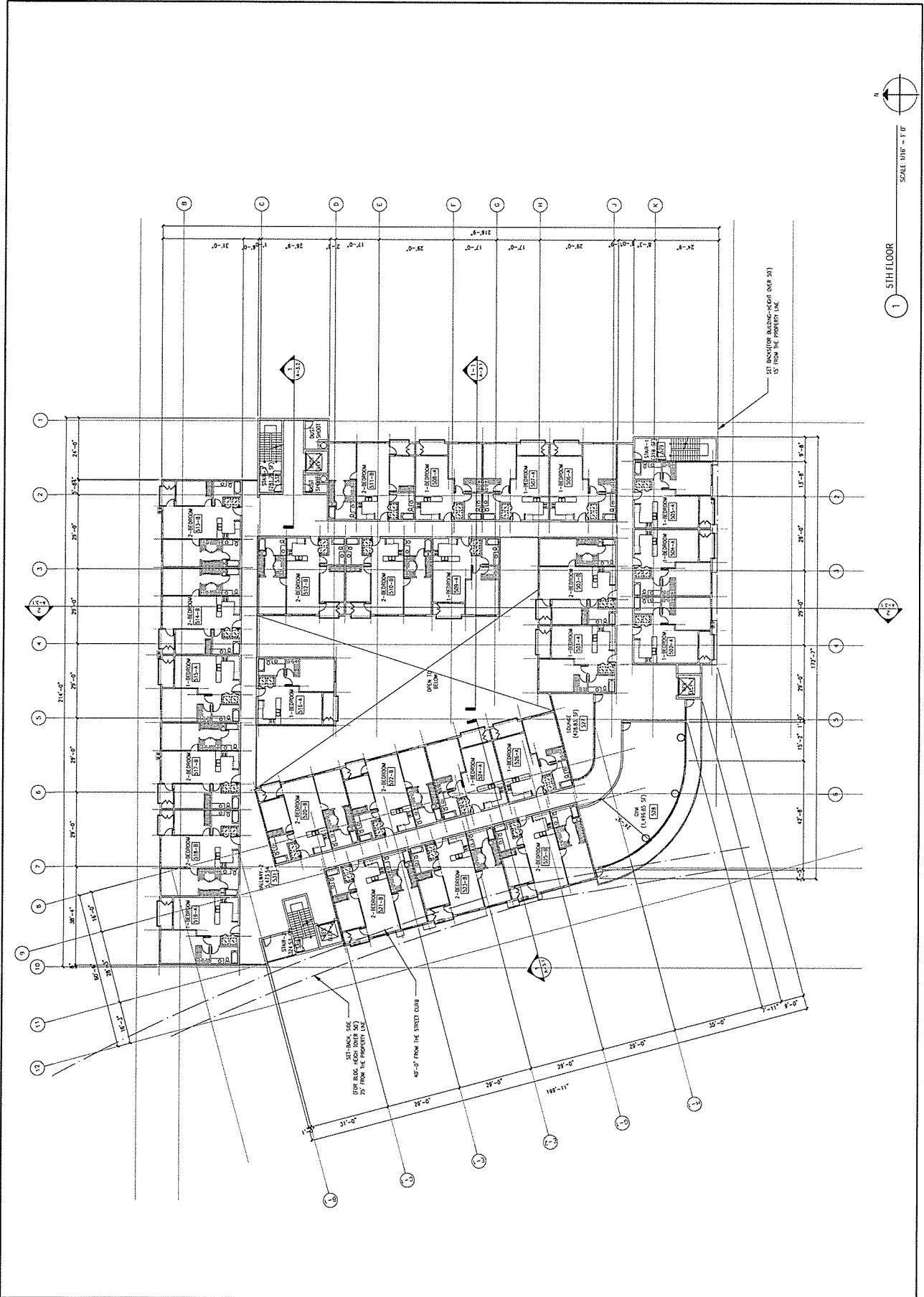
CONTRACT

CONTRACT

MIXED-USE BUILDING FOR DR. DAO  
10201 & 10231 GARDEN GROVE BLVD  
GARDEN GROVE, CA 92643  
MR. MICHAEL DAO

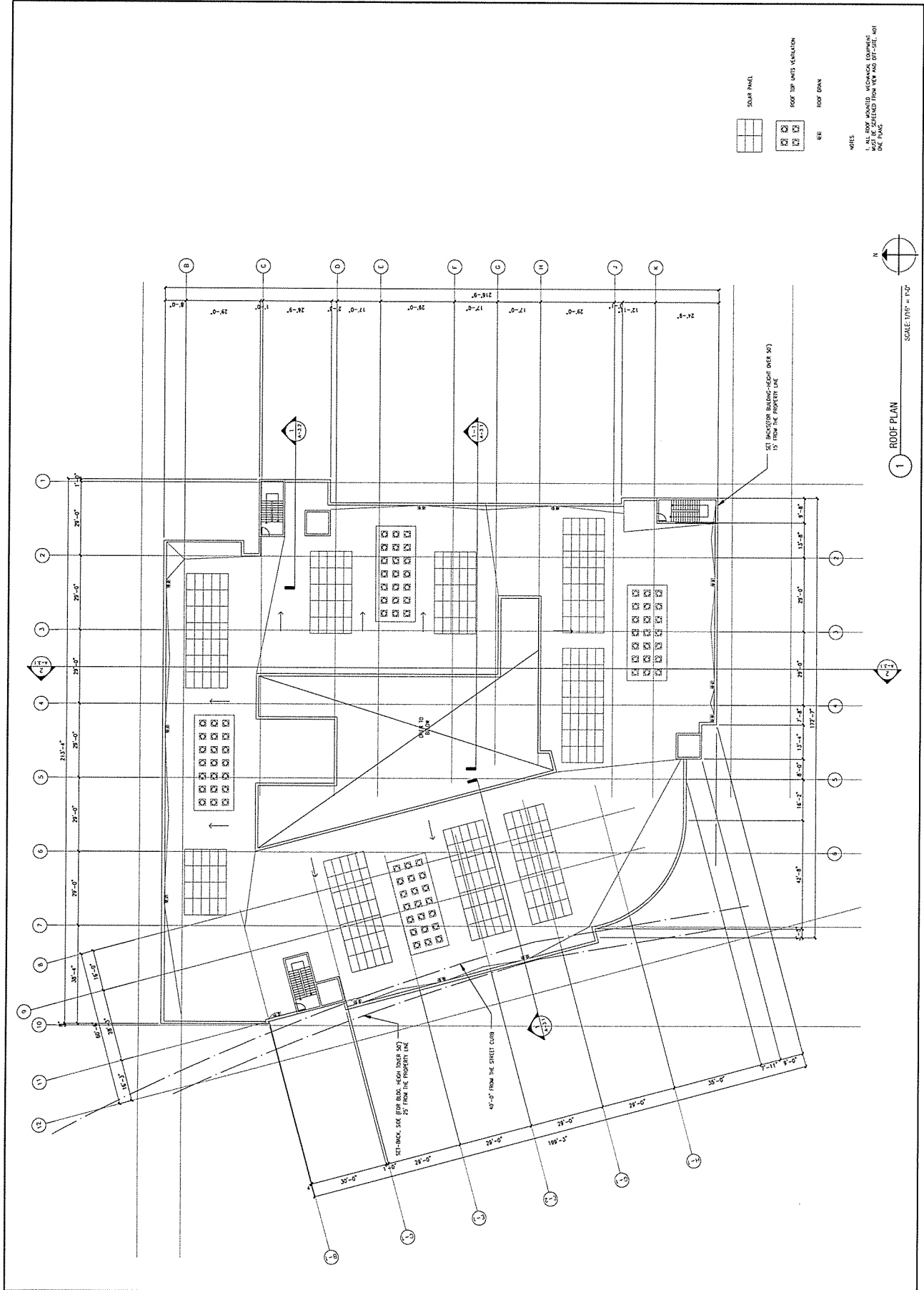
PROJECT  
5TH FLOOR PLAN

DATE:	12/15/11
REVISION:	01
DRAWN BY:	AMANDA'S
CHECKED BY:	SAUL'S
SCALE:	A-1.6
SHEET:	05
DATE:	12/15/11



SCALE: 1/8" = 1'-0"

1 5TH FLOOR

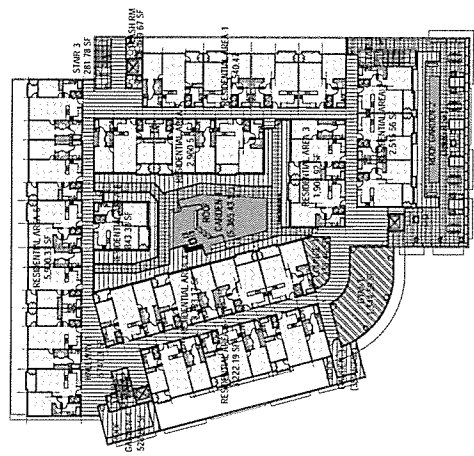
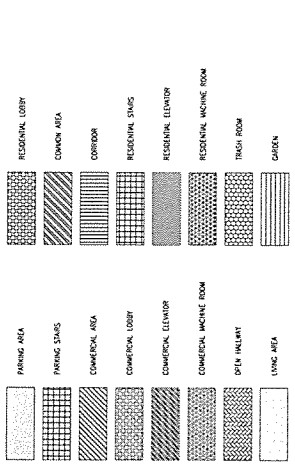




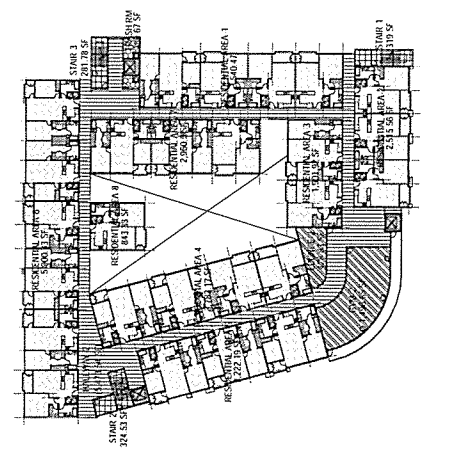
**AREA TABULATION**

	1ST FL.	2ND FL.	3RD FL.	4TH FL.	5TH FL.	TOTAL
PARKING STRUCTURE	22,243.39	3,018.21	0	0	0	25,261.60
STAIRS	1,872.48	1,872.48	1,872.48	0	0	5,617.44
STAIRS	648.00	648.00	648.00	0	0	1,944.00
TOTAL	24,763.87	5,538.69	4,390.48	0	0	34,693.04
COMMON	0	0	0	0	0	0
COMMERCIAL	0	0	0	0	0	0
RESIDENTIAL	0	0	0	0	0	0
MECHANICAL	0	0	0	0	0	0
OPEN W/	0	0	0	0	0	0
TOTAL	24,763.87	5,538.69	4,390.48	0	0	34,693.04
COMMON	0	0	0	0	0	0
COMMERCIAL	0	0	0	0	0	0
RESIDENTIAL	0	0	0	0	0	0
MECHANICAL	0	0	0	0	0	0
OPEN W/	0	0	0	0	0	0
TOTAL	0	0	0	0	0	0
COMMON	0	0	0	0	0	0
COMMERCIAL	0	0	0	0	0	0
RESIDENTIAL	0	0	0	0	0	0
MECHANICAL	0	0	0	0	0	0
OPEN W/	0	0	0	0	0	0
TOTAL	0	0	0	0	0	0
COMMON	0	0	0	0	0	0
COMMERCIAL	0	0	0	0	0	0
RESIDENTIAL	0	0	0	0	0	0
MECHANICAL	0	0	0	0	0	0
OPEN W/	0	0	0	0	0	0
TOTAL	0	0	0	0	0	0

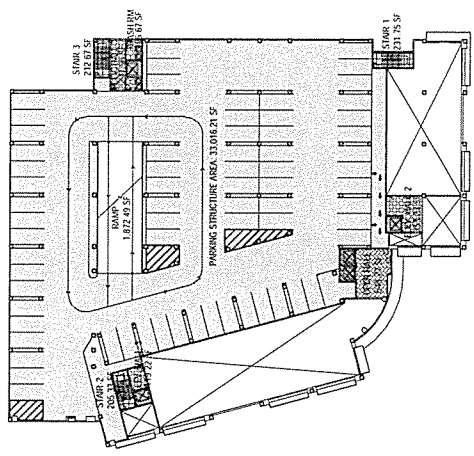
\* OPEN WALKWAY AND MECHANICAL ROOMS ARE INCLUDED IN TO THE TOTAL AREA.



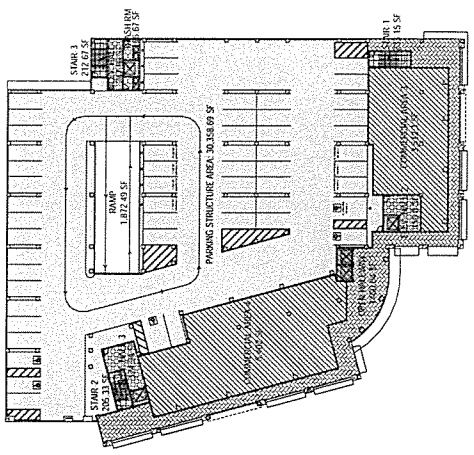
4 FOURTH FLOOR DIAGRAM  
 SCALE: 1/32" = 1' 0"



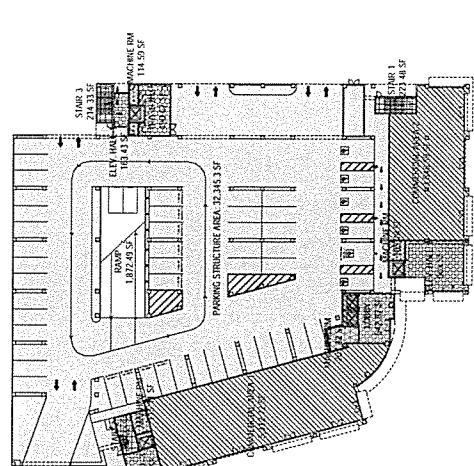
5 FIFTH FLOOR DIAGRAM  
 SCALE: 1/32" = 1' 0"



2 SECOND FLOOR DIAGRAM  
 SCALE: 1/32" = 1' 0"



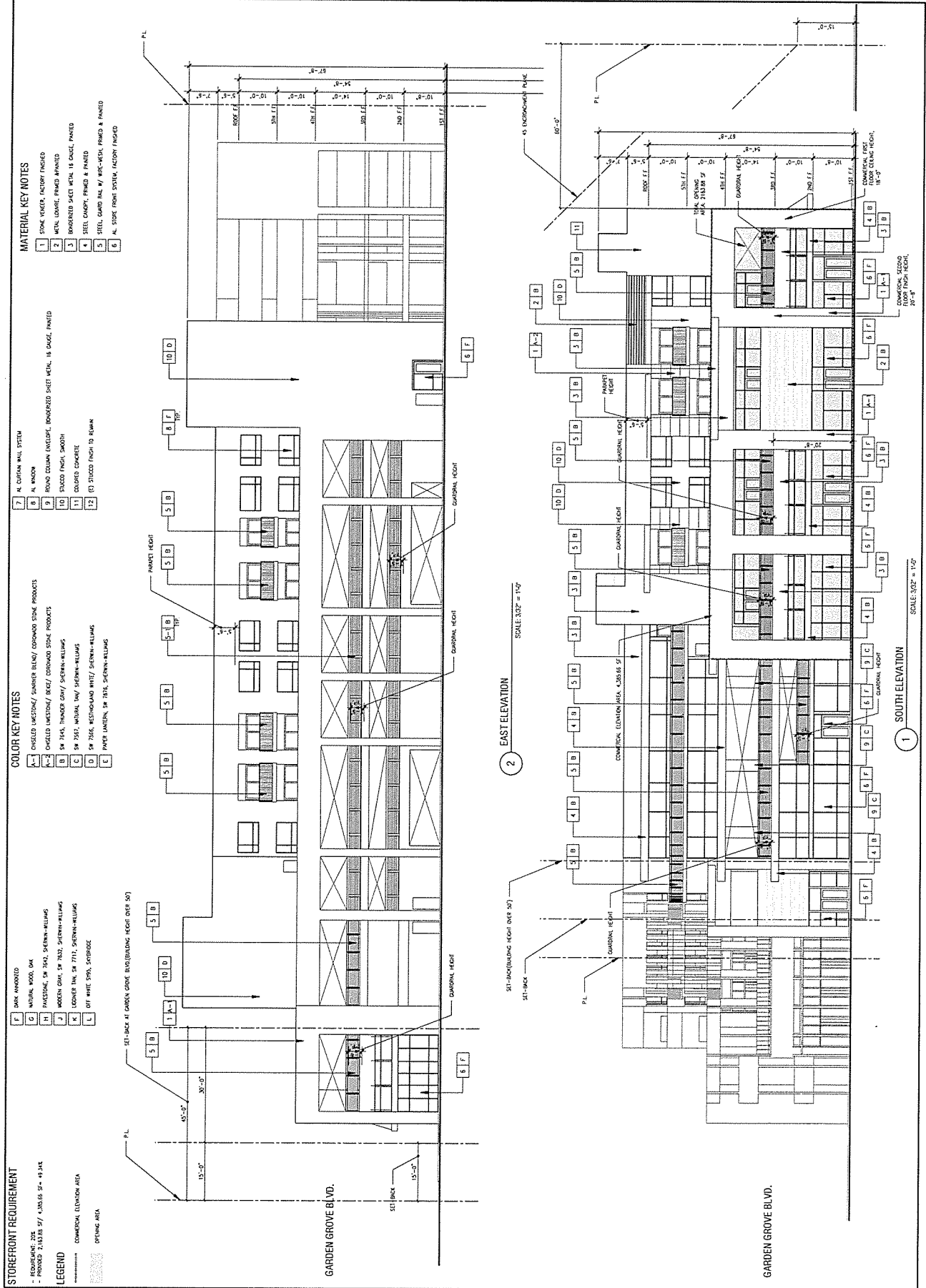
3 THIRD FLOOR DIAGRAM  
 SCALE: 1/32" = 1' 0"



1 FIRST FLOOR DIAGRAM  
 SCALE: 1/32" = 1' 0"

**FLOOR PLAN DIAGRAM**

DATE: 02/27/10  
 DRAWN BY: [Blank]  
 CHECKED BY: [Blank]  
 PROJECT NO.: [Blank]  
 SHEET NO.: [Blank]  
 OF [Blank] SHEETS  
 A-1.9



**STOREFRONT REQUIREMENT**  
- REQUIREMENT 205, 206, 207 / 3.05.05, 3.05.06, 3.05.07, 3.05.08, 3.05.09, 3.05.10, 3.05.11, 3.05.12, 3.05.13, 3.05.14, 3.05.15, 3.05.16, 3.05.17, 3.05.18, 3.05.19, 3.05.20, 3.05.21, 3.05.22, 3.05.23, 3.05.24, 3.05.25, 3.05.26, 3.05.27, 3.05.28, 3.05.29, 3.05.30, 3.05.31, 3.05.32, 3.05.33, 3.05.34, 3.05.35, 3.05.36, 3.05.37, 3.05.38, 3.05.39, 3.05.40, 3.05.41, 3.05.42, 3.05.43, 3.05.44, 3.05.45, 3.05.46, 3.05.47, 3.05.48, 3.05.49, 3.05.50, 3.05.51, 3.05.52, 3.05.53, 3.05.54, 3.05.55, 3.05.56, 3.05.57, 3.05.58, 3.05.59, 3.05.60, 3.05.61, 3.05.62, 3.05.63, 3.05.64, 3.05.65, 3.05.66, 3.05.67, 3.05.68, 3.05.69, 3.05.70, 3.05.71, 3.05.72, 3.05.73, 3.05.74, 3.05.75, 3.05.76, 3.05.77, 3.05.78, 3.05.79, 3.05.80, 3.05.81, 3.05.82, 3.05.83, 3.05.84, 3.05.85, 3.05.86, 3.05.87, 3.05.88, 3.05.89, 3.05.90, 3.05.91, 3.05.92, 3.05.93, 3.05.94, 3.05.95, 3.05.96, 3.05.97, 3.05.98, 3.05.99, 3.05.100

**LEGEND**  
COMMERCIAL ELEVATION AREA  
SPRING AREA

**COLOR KEY NOTES**  
A-1 CHELSEA LACERNAE / SUMNER BLES / CORONADO STONE PRODUCTS  
A-2 CHELSEA LACERNAE / BUCK / CORONADO STONE PRODUCTS  
B SW 7554, METAL TAY / SHERWIN-WILLIAMS  
C SW 7557, METAL TAY / SHERWIN-WILLIAMS  
D SW 7554, WESTHOLAND WHITE / SHERWIN-WILLIAMS  
E PAPER LANTERN, SW 7513, SHERWIN-WILLIAMS

**COLOR KEY NOTES**  
7 A. CORIAN WALL SYSTEM  
8 A. INKON  
9 ROUND COLUMN ENVELOPE, HONORIZED SHEET METAL, 18 GAUGE, PAINTED  
10 STUCCO FINISH, SMOOTH  
11 COLORED CONCRETE  
12 (D) STUCCO FINISH TO BLANK

**MATERIAL KEY NOTES**  
1 STONE CRACKER, FACTORY FINISHED  
2 METAL LAMINATE, FINISHED PAINTED  
3 HONORIZED SHEET METAL, 18 GAUGE, PAINTED  
4 STEEL CRACKER, FINISHED & PAINTED  
5 STEEL CRACKER, FINISHED & PAINTED  
6 AL. STONE FRONT SYSTEM, FACTORY FINISHED

**SCALE: 3/32" = 1'-0"**

**STOREFRONT REQUIREMENT**  
 - REQUIREMENT 402  
 - PROVIDED 2,843.57 / 3,537.28 SF = 81.4%

**LEGEND**  
 COMMERCIAL ELEVATION AREA  
 OPENING AREA

**COLOR KEY NOTES**  
 A-1 ONCELOD UNLAKED/ SINTERED BLENK/ CORONADO STONE PRODUCTS  
 A-2 ONCELOD UNLAKED/ BLENK/ CORONADO STONE PRODUCTS  
 B SK 745, THUNDER GRAY/ SIENNA-KELLAS  
 C SK 754, NATURAL LAY/ SIENNA-KELLAS  
 D SK 756, WESTHOLAND WHITE/ SIENNA-KELLAS  
 E PAPER LANTERN, SK 745, SIENNA-KELLAS

**MATERIAL KEY NOTES**  
 1 STONE VENEER, FACTORY FINISH  
 2 METAL LAMINATE, FINISH PAINTED  
 3 BONDINGED SHEET METAL, 16 GAUGE, PAINTED  
 4 STEEL SHEET, FINISH PAINTED  
 5 STEEL SHEET, FINISH PAINTED  
 6 AL. STEEL TIGHT SYSTEM, FACTORY FINISH

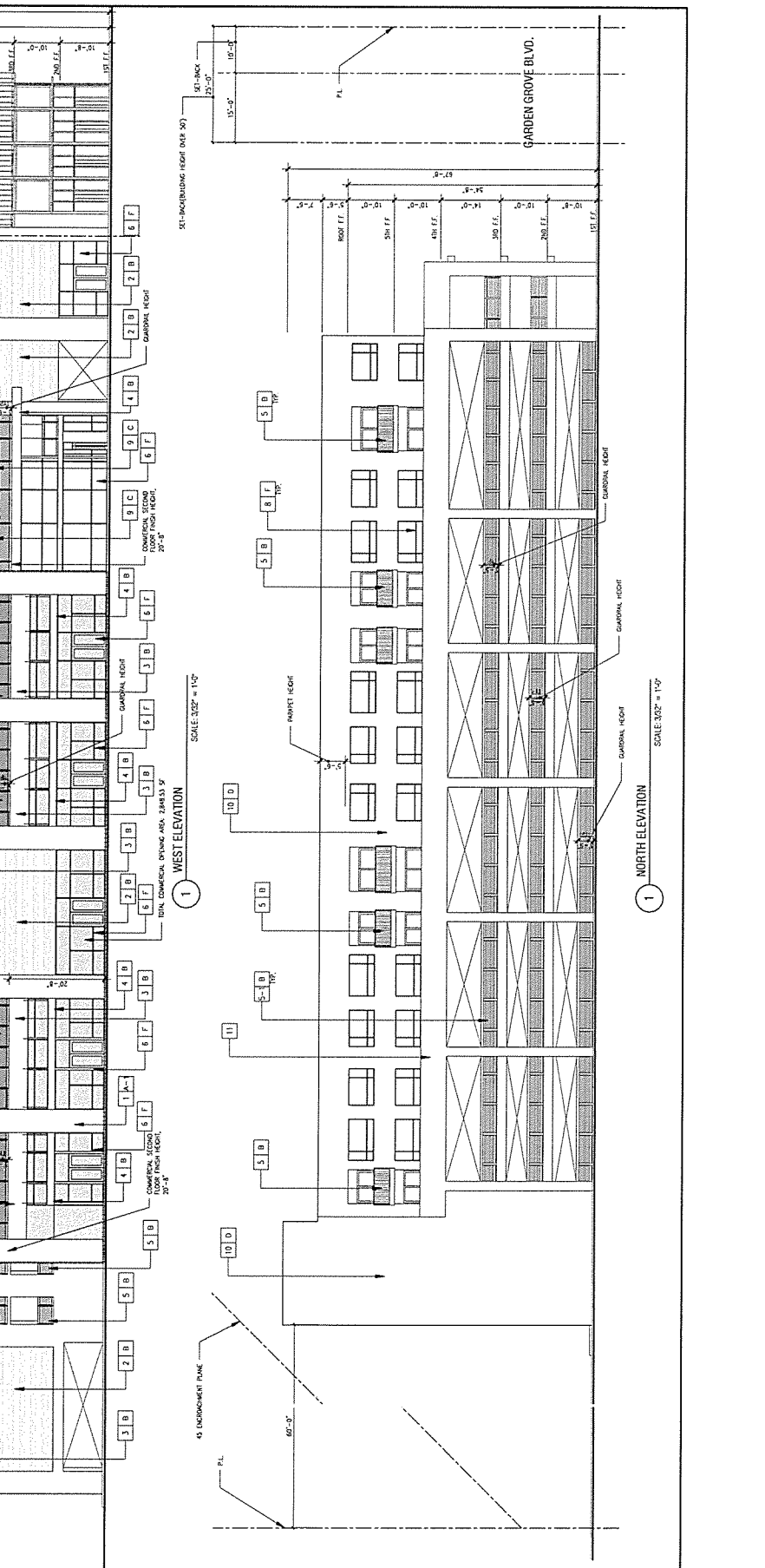
**COLOR KEY NOTES**  
 F BARK ANGSTADT  
 G NATURAL WOOD, OAK  
 H WESTSIDE, SK 745, SIENNA-KELLAS  
 J WESTSIDE, SK 745, SIENNA-KELLAS  
 K LONER, SK 745, SIENNA-KELLAS  
 L OFF WHITE SPYS, SINTERED

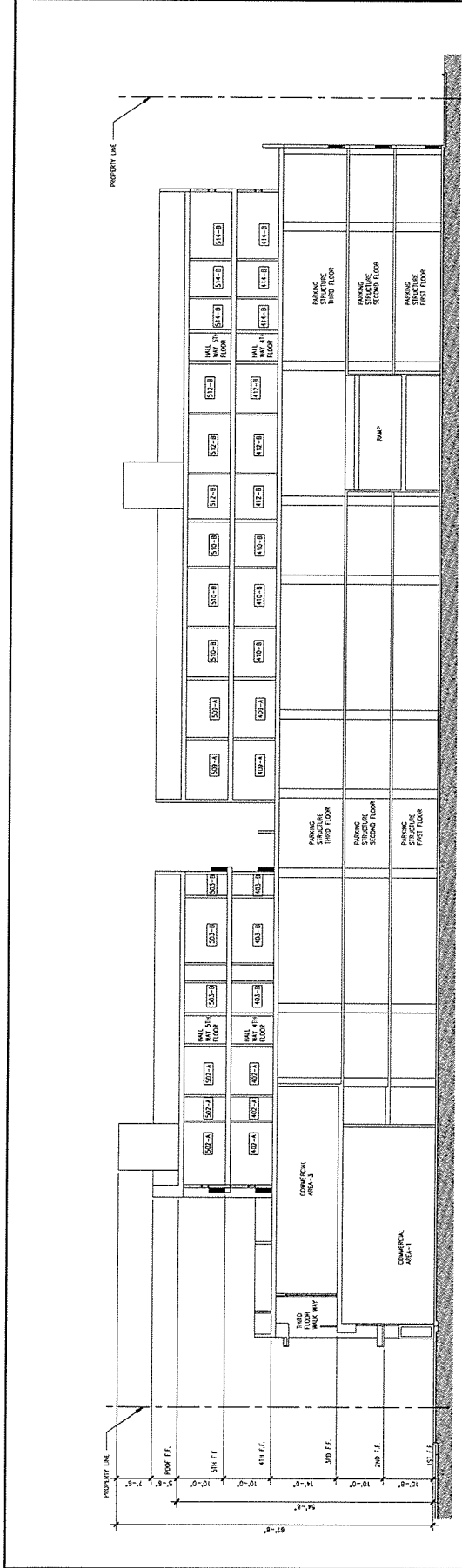
**MATERIAL KEY NOTES**  
 7 AL. CURTAIN WALL SYSTEM  
 8 AL. WINDOW  
 9 ROUND COLUMN ENVELOPE, BONDINGED SHEET METAL, 16 GAUGE, PAINTED  
 10 STUCCO FINISH, WHITE  
 11 COLORED CONCRETE  
 12 (R) STUCCO FINISH TO REFINISH

**MATERIAL KEY NOTES**  
 13 STUCCO FINISH TO REFINISH  
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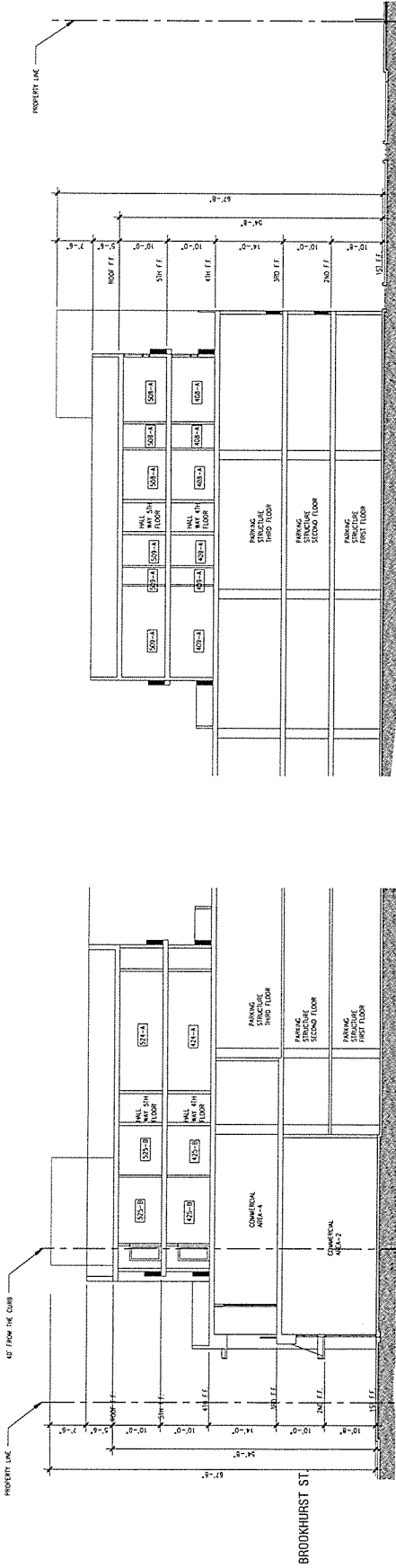
**WEST ELEVATION**  
 SCALE: 3/32" = 1'-0"  
 TOTAL COMMERCIAL OPENING AREA: 2,843.57 SF  
 TOTAL COMMERCIAL OPENING AREA: 3,537.28 SF

**NORTH ELEVATION**  
 SCALE: 3/32" = 1'-0"





2 SECTION-2  
SCALE: 3/32" = 1'-0"



1 SECTION-1  
SCALE: 3/32" = 1'-0"

1-1 SECTION-2  
SCALE: 3/32" = 1'-0"





CONSTRUCTION  
CONSULTANTS  
**SOURCE**  
ARCHITECTURE

10001 K. 10231 GARDEN GROVE ALVA  
GARDEN GROVE, CA 92843-1024  
MR. MICHAEL DAO  
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DATE: \_\_\_\_\_ BY: \_\_\_\_\_  
DRAWN BY: \_\_\_\_\_ APPROVED: \_\_\_\_\_  
DATE: \_\_\_\_\_ SCALE: \_\_\_\_\_  
SHEET: \_\_\_\_\_ OF \_\_\_\_\_ SHEETS

MR. MICHAEL DAO

MIXED-USE BUILDING FOR DR. DAO  
10001 K. 10231 GARDEN GROVE ALVA  
GARDEN GROVE, CA 92843-1024

OWNER

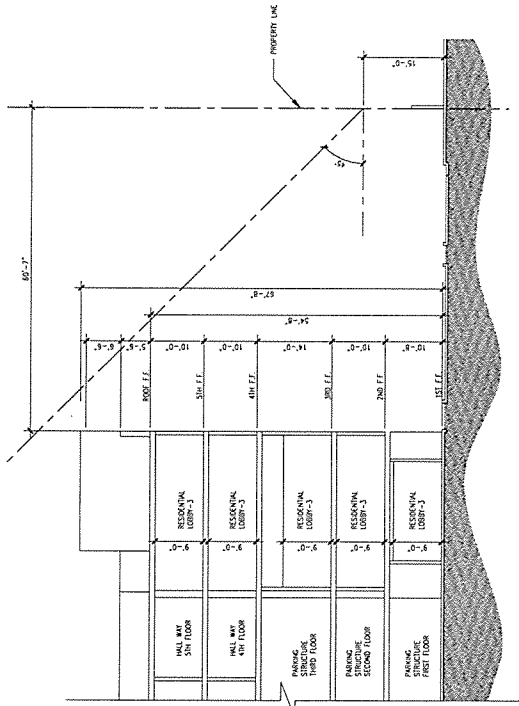
PROJECT

TITLE

SECTIONS-2

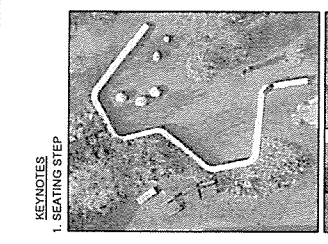
DATE	REVISION	BY

A-3.2

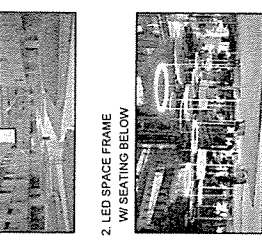


1 SECTION

SCALE 3/32" = 1'-0"



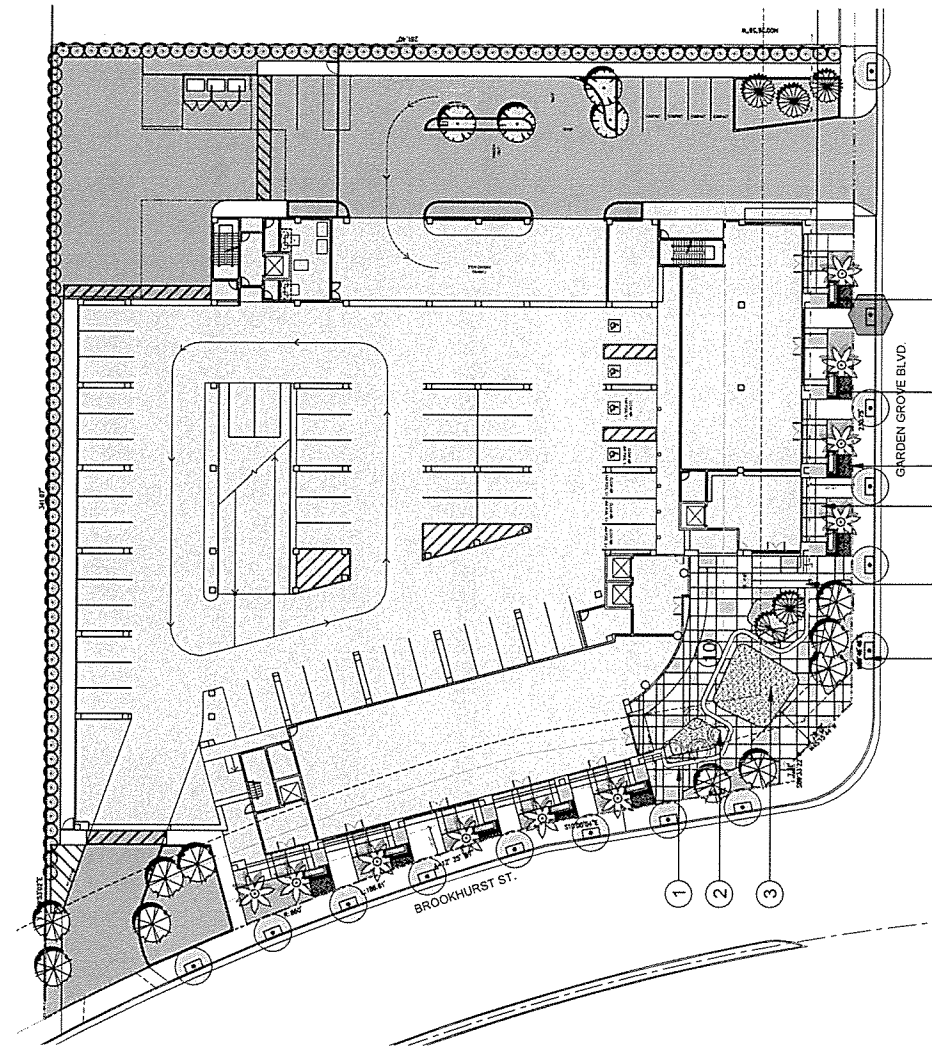
KEYNOTES  
 1. SEATING STEP



2. LED SPACE FRAME  
 W/ SEATING BELOW

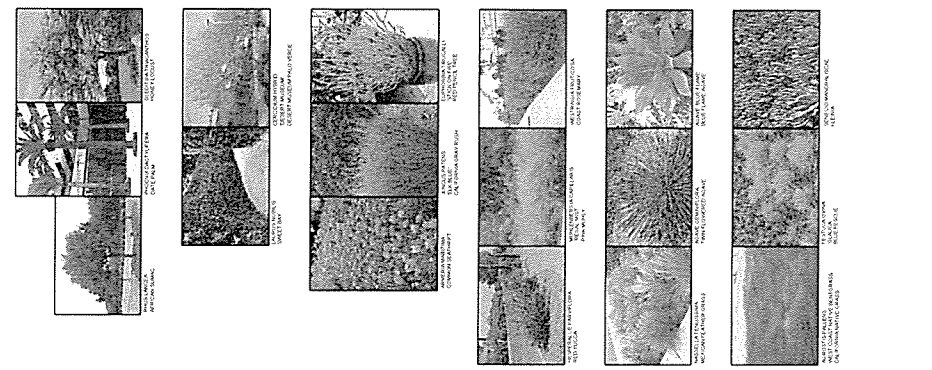
3. CENTRAL LAWN AREA
4. 3x6 TREE WELL CUT-OUT W/ NEW STREET TREE
5. RAMP
6. COLORED CONCRETE PAVING
7. GRASSPAVE W/ NATIVE BENTGRASS
8. BUILT-IN BENCH
9. 30" HT. CONC. PLANTER
10. DECORATIVE TILE PAVING

MAIN PLAZA: LANDSCAPE PROVIDED:	3,367 SF. 890 SF. (27%)
SETBACKS - LANDSCAPE TABULATIONS	
BROOKHURST ST. FRONTAGE: LANDSCAPE PROVIDED:	2,771 SF. 1,385 SF. (50%)
GARDEN GROVE BLVD. FRONTAGE: LANDSCAPE PROVIDED:	3,860 SF. 1,211 SF. (31%)
EAST SIDEWALK: LANDSCAPE PROVIDED:	2,665 SF. 1,643 SF. (62%)

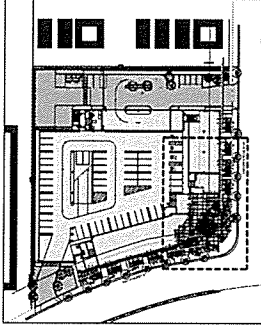


**PRELIMINARY LANDSCAPE PLAN**  
 SCALE: 1" = 20'-0"

- |  |   |   |
|--|---|---|
| <b>TREES</b>   | <b>SHRUBS &amp; GROUND COVERS</b>   | <b>WOODS</b>  |
| <ul style="list-style-type: none"> <li>LAURUS NOBILIS SWEET BAY</li> <li>NEW STREET TREE PER CITY OF GARDEN GROVE URBAN FORESTRY STREET TREE DIV.</li> <li>EXISTING STREET TREE TO REMAIN</li> </ul> | <ul style="list-style-type: none"> <li>SHRUBS &amp; GROUND COVERS</li> <li>ELIPHOBBIA THRUCCALI</li> <li>SHALICA</li> <li>RED PENCIL TREE</li> <li>AGAVE GEMINIFLORA</li> <li>TWIN-FLOWERED AGAVE</li> <li>MEXICAN FENNER GRASS</li> <li>MASSELLA TENJISSIMIMA</li> <li>ARMERIA MARTIMA</li> <li>COMMON SEATHRIFT</li> <li>SENECIO MACROPALISCAE</li> <li>KLEBERIA</li> </ul> | <ul style="list-style-type: none"> <li>WOODS</li> <li>LOW</li> </ul>                              |
| <b>PLANTING LEGEND</b>   | <b>SIZE &amp; QUANT.</b>  | <b>WOODS</b>  |
| <ul style="list-style-type: none"> <li>RHUS LAMPEA</li> <li>AFRICAN SUMAC</li> <li>CERCIDILUM HYBRID</li> <li>DESERT MUEBBLIK</li> <li>DESERT MUSEUM PALO VERDE</li> <li>DATE PALM</li> </ul>        | <ul style="list-style-type: none"> <li>15 GAL / 110 EA.</li> <li>24" BOX / 4 EA.</li> <li>24" BOX / 6 EA.</li> <li>20" TRUNK HT / 10 EA.</li> </ul>   | <ul style="list-style-type: none"> <li>MODERATE</li> <li>LOW</li> <li>LOW</li> <li>LOW</li> </ul> |



<b>PLANTING LEGEND</b>	<b>SIZE &amp; QUANT.</b>	<b>WOODS</b>
<ul style="list-style-type: none"> <li>RHUS LAMPEA</li> <li>AFRICAN SUMAC</li> <li>CERCIDILUM HYBRID</li> <li>DESERT MUEBBLIK</li> <li>DESERT MUSEUM PALO VERDE</li> <li>DATE PALM</li> </ul>	<ul style="list-style-type: none"> <li>24" BOX / 10 EA.</li> <li>24" BOX / 4 EA.</li> <li>24" BOX / 6 EA.</li> <li>20" TRUNK HT / 10 EA.</li> </ul>	<ul style="list-style-type: none"> <li>MODERATE</li> <li>LOW</li> <li>LOW</li> <li>LOW</li> </ul>



SITE PLAN - INTS.

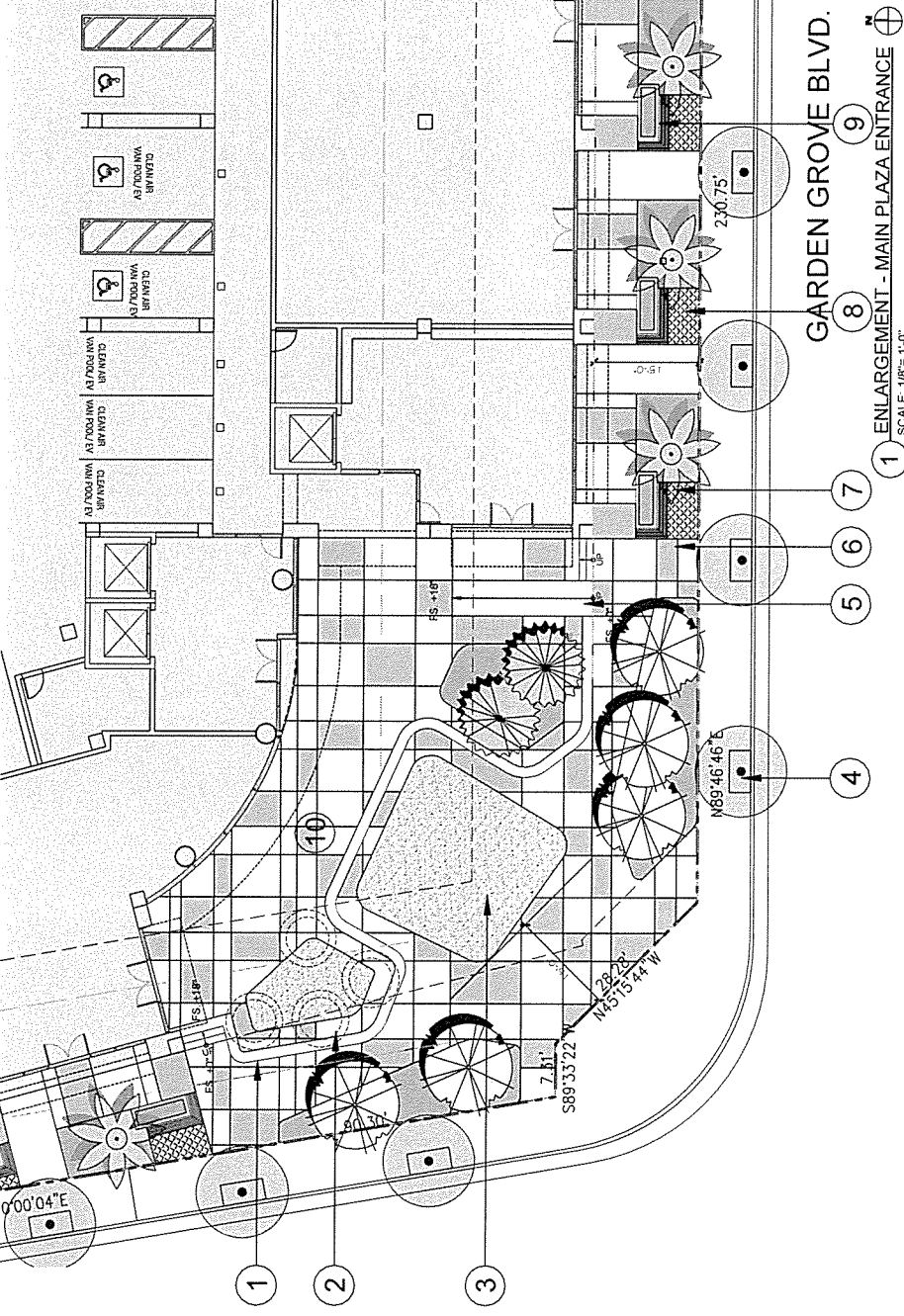
KEYNOTES  
1. SEATING STEP



2. LED SPACE FRAME  
W/ SEATING BELOW



3. CENTRAL LAWN AREA
4. 3x6 TREE WELL CUT-OUT  
W/ NEW STREET TREE
5. RAMP
6. COLORED CONCRETE PAVING
7. GRASSPAVE W/ NATIVE BENTGRASS
8. BUILT-IN BENCH
9. 30" HT. CONC. PLANTER
10. DECORATIVE TILE PAVING



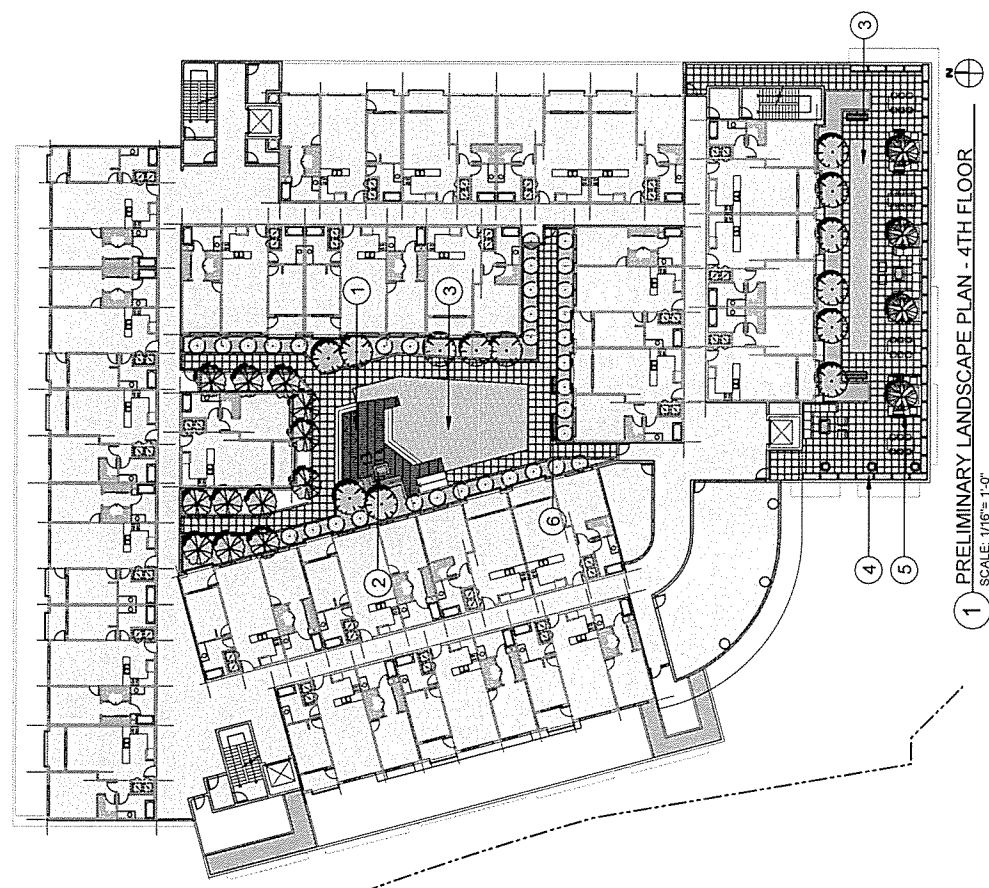
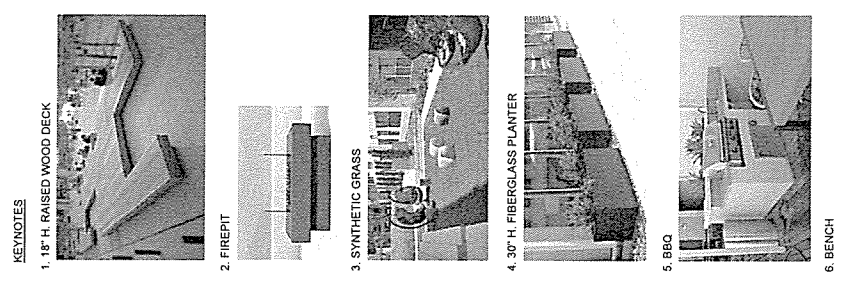
**PLANTING LEGEND**

TREES	SIZE & QUAN.	WINDS	TREES	SIZE & QUAN.	WINDS
GLEDITSIA TRIACANTHOS HONEY LOCUST	24" BOX / 10 EA.	MODERATE	LAURUS NIBILIS SWEET BAY	15 GAL / 113 EA.	LOW
RHUS LANCEA AFRICAN SUMAC	24" BOX / 4 EA.	LOW	NEW STREET TREE PER CITY OF GARDEN GROVE URBAN FORESTRY STREET TREE DV.		
CERDIDIUM HYBRID 'DESERT MUSEUM' DESERT MUSEUM PALO VERDE	24" BOX / 6 EA.	LOW	EXISTING STREET TREE TO REMAIN		
RHODEN DACTYLIFERA DATE PALM	20" TRUNK HT / 10 EA.	LOW			

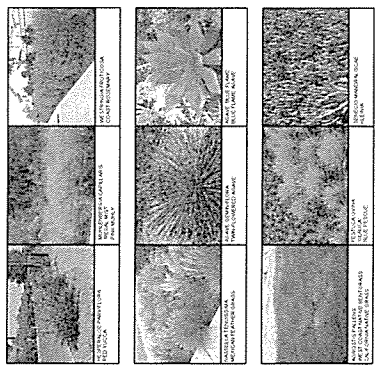
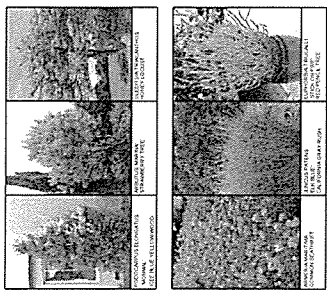
  

SHRUBS & GROUND COVERS	SHRUBS & GROUND COVERS
AGAVE GEMINIFLORA TWIN-FLOWERED AGAVE	SHRUBS & GROUND COVERS
NASSELLA TENJUSISIMA MEXICAN FEATHER GRASS	MICALEBERGIA CAPILLARIS PINK MILK-Y
ARMERIA MARITIMA COMMON SEATHRIFT	WESTRINGIA FRUTICOSA COAST ROSEMARY
SENECIO HANDBALUSCAE KLEINA	JUNCUS PATENS FLA BLUE
FERTUCA OVINA BLUE FESCUE	CALIFORNIA GREY RUSH
AGROSTIS PALLES WEST COAST NATIVE BENTGRASS	LANGSCAPE AREA HESPERALOE PARVIFLORA RED YUCCA
AVAILABLE FROM WEST COAST NATIVE	AGAVE BLUE FLAME BLUE FLAME AGAVE
	EUPHORBIA TIRUCALLI STICK ON FIRE
	RED PENCIL TREE

**1 ENLARGEMENT - MAIN PLAZA ENTRANCE**  
SCALE: 1/8" = 1'-0"



PLANTING LEGEND	TREES	SIZE & QUANTITY	WILDCULS	SHRUBS & GROUND COVERS
	QUERCUS TRICANTHOS HONEY LOCUST	24" BOX / 16 EA.	MODERATE	FESTUCA OVINA GLAUKA BLUE FESCUE
	ARJUNUS 'MARIUK' STRAWBERRY TREE	24" BOX / 12 EA.	LOW	AGROSTIS PALLENS WEST COAST NATIVE BENTGRASS AVAILABLE FROM WEST COAST NATIVE
	CERCIDIAM HYBRID DESERT MUSEUM DESERT MUSEUM PALM VERDE	24" BOX / 33 EA.	LOW	LANDSCAPE AREA PROPOSED 4TH FLOOR: 2,918 SF.



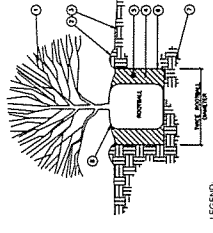
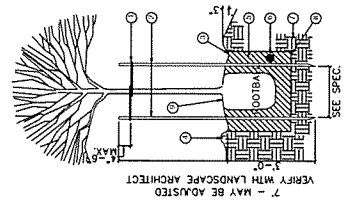
SHRUBS & GROUND COVERS	SHRUBS & GROUND COVERS	SHRUBS & GROUND COVERS	SHRUBS & GROUND COVERS
AGAVE BLUE FLAME BLUE FLAME AGAVE	AGAVE SEAMLESS OVA WESTRINGIA FRUTICOSA COAST ROSEMARY INKLE PALMS CALIFORNIA GREY FUSH LANDSCAPE AREA RED YUCCA	ALPHITONIA CAPILLARIS REGAL WST PINK MURRAY WESTRINGIA FRUTICOSA COAST ROSEMARY INKLE PALMS CALIFORNIA GREY FUSH LANDSCAPE AREA RED YUCCA	EUPHORBIA TRIUCULLI STICK ON FIRE RED PENCIL TREE AGAVE SEAMLESS OVA TYNNE COVERED OIGAVE MASSELLA TENJUSSIMMA MEXICAN FEATHER GRASS AMERICA MARTIMA COMMON SEATRIFT SENECIO MANDRALISCAE KLEINIA



- LEGEND:**
1. 2" CORRUGATED SEE SPECIFICATIONS.
  2. 1/2" DIA. (10 LOGS PER LINE STAKE)
  3. 2" DIA. (10 LOGS PER LINE STAKE)
  4. 4" DIA. (10 LOGS PER LINE STAKE)
  5. 6" DIA. (10 LOGS PER LINE STAKE)
  6. 8" DIA. (10 LOGS PER LINE STAKE)
  7. 10" DIA. (10 LOGS PER LINE STAKE)
  8. 12" DIA. (10 LOGS PER LINE STAKE)
  9. 14" DIA. (10 LOGS PER LINE STAKE)
  10. 16" DIA. (10 LOGS PER LINE STAKE)
  11. 18" DIA. (10 LOGS PER LINE STAKE)
  12. 20" DIA. (10 LOGS PER LINE STAKE)
  13. 22" DIA. (10 LOGS PER LINE STAKE)
  14. 24" DIA. (10 LOGS PER LINE STAKE)
  15. 26" DIA. (10 LOGS PER LINE STAKE)
  16. 28" DIA. (10 LOGS PER LINE STAKE)
  17. 30" DIA. (10 LOGS PER LINE STAKE)
  18. 32" DIA. (10 LOGS PER LINE STAKE)
  19. 34" DIA. (10 LOGS PER LINE STAKE)
  20. 36" DIA. (10 LOGS PER LINE STAKE)
  21. 38" DIA. (10 LOGS PER LINE STAKE)
  22. 40" DIA. (10 LOGS PER LINE STAKE)
  23. 42" DIA. (10 LOGS PER LINE STAKE)
  24. 44" DIA. (10 LOGS PER LINE STAKE)
  25. 46" DIA. (10 LOGS PER LINE STAKE)
  26. 48" DIA. (10 LOGS PER LINE STAKE)
  27. 50" DIA. (10 LOGS PER LINE STAKE)
  28. 52" DIA. (10 LOGS PER LINE STAKE)
  29. 54" DIA. (10 LOGS PER LINE STAKE)
  30. 56" DIA. (10 LOGS PER LINE STAKE)
  31. 58" DIA. (10 LOGS PER LINE STAKE)
  32. 60" DIA. (10 LOGS PER LINE STAKE)
  33. 62" DIA. (10 LOGS PER LINE STAKE)
  34. 64" DIA. (10 LOGS PER LINE STAKE)
  35. 66" DIA. (10 LOGS PER LINE STAKE)
  36. 68" DIA. (10 LOGS PER LINE STAKE)
  37. 70" DIA. (10 LOGS PER LINE STAKE)
  38. 72" DIA. (10 LOGS PER LINE STAKE)
  39. 74" DIA. (10 LOGS PER LINE STAKE)
  40. 76" DIA. (10 LOGS PER LINE STAKE)
  41. 78" DIA. (10 LOGS PER LINE STAKE)
  42. 80" DIA. (10 LOGS PER LINE STAKE)
  43. 82" DIA. (10 LOGS PER LINE STAKE)
  44. 84" DIA. (10 LOGS PER LINE STAKE)
  45. 86" DIA. (10 LOGS PER LINE STAKE)
  46. 88" DIA. (10 LOGS PER LINE STAKE)
  47. 90" DIA. (10 LOGS PER LINE STAKE)
  48. 92" DIA. (10 LOGS PER LINE STAKE)
  49. 94" DIA. (10 LOGS PER LINE STAKE)
  50. 96" DIA. (10 LOGS PER LINE STAKE)
  51. 98" DIA. (10 LOGS PER LINE STAKE)
  52. 100" DIA. (10 LOGS PER LINE STAKE)

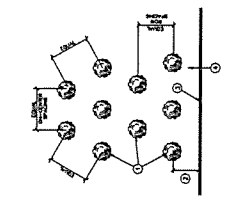
**NOTE:** 1. MAINTAIN TURF 2" CLEAR FROM TREE TRUNK.  
2. BE DOUBLE STAKED.

**1 TREE PLANTING-DOUBLE STAKING**  
SCALE: N.T.S.



- LEGEND:**
1. SHRUB - CENTER IN PIT.
  2. DEEP WATERING BASIN, SEE SPECIFICATIONS.
  3. FINISH GRADE.
  4. AMENDED BACKFILL - SEE SPECIFICATIONS.
  5. 1/2" DIA. (10 LOGS PER LINE STAKE) PLACE IN PIT 2" SLIP FROM PIT BOTTOM - SEE SPECIFICATIONS.
  6. SCARIFY SIDES AND BOTTOM OF PLANTING PIT.
  7. UNDISTURBED NATIVE SOIL.
  8. SET TOP OF FOOTBALL 1" ABOVE SURROUNDING GRADE AND SLOPE FOR DRAINAGE.

**2 SHRUB PLANTING**  
SCALE: N.T.S.



- LEGEND:**
1. LOCATE PLANTS WITH EQUAL SPACING AS INDICATED.
  2. 12" ON CENTER SPACING.
  3. PARKING, CURB, BUILDING, OR HEADER - SHOWING PLANTING AREA LIMIT.
  4. PROVIDE A MINIMUM OF 3" DEPTH MULCH LAYER IN ALL LANDSCAPE AREAS.

**3 SHRUB/GROUND COVER SPACING**  
SCALE: N.T.S.

**LANDSCAPE PLANTING NOTES**

1. THE LANDSCAPE CONTRACTOR SHALL FURNISH ALL LABOR EQUIPMENT, MATERIALS AND SERVICES FOR THE COMPLETE INSTALLATION AS DESCRIBED BY THE LANDSCAPE DRAWINGS.
2. ANY DEVIATION FROM THE PLANS IS TO HAVE PRIOR WRITTEN APPROVAL BY THE OWNER OR HIS REPRESENTATIVE.
3. THE LANDSCAPE CONTRACTOR IS TO REMOVE ALL WEEDS AND OR GRASSES (INCLUDING THE ROOTS) EXISTING IN THE PROPOSED GROUND COVER AREA.
4. ALL PROPOSED GROUND COVER AREA SHALL RECEIVE THE PRE-EMERGENT HERBICIDE APPLICATION AS SPECIFIED. THE APPLICATION SHALL BE DONE BY PERSONNEL LICENSED TO HANDLE AGRICULTURAL CHEMICALS.
5. EROSION GRADING OTHER THAN THAT NOTED ON THE LANDSCAPE FINISH GRADING IS THE RESPONSIBILITY OF THE GENERAL CONTRACTOR.
6. FINISH GRADING WILL CONSIST OF RAKING ALL AREAS TO A SMOOTH GRADE, LOOSENING THE SOIL TO A DEPTH OF 6" AND REMOVING ALL ROCKS OR CLODS OF 2" DIAMETER OR LARGER. FINISH GRADE IS TO BE 2" BELOW TOP OF ADJACENT DRIVE AND SIDEWALKS.
7. SOIL PREPARATION FOR ALL LANDSCAPE AREAS PLEASE SEE WALLACE LAB RECOMMENDATION.
8. ALL ROCK OR UNBROKEN SOIL CLODS OVER 1" IN DIAMETER BROUGHT TO THE SURFACE ARE TO BE REMOVED FROM THE SITE.
9. THE LANDSCAPE CONTRACTOR SHALL BE RESPONSIBLE FOR THE HORTICULTURAL SOILS FERTILITY REPORT PRIOR TO SOIL PREPARATION AND PLANT INSTALLATION. THE REPORT SHALL BE SUBMITTED TO WALLACE LABORATORIES, LLC RECOMMENDATIONS, WALLACE LAB (310) 615-0116, 3865 CORAL CIRCLE, EL SEGUNDO, CA 90245.
10. GROUNDCOVERS ARE TO BE PLANTED SO THAT AFTER SETTLING, THE GROWN OF THE PLANT IS EVEN WITH FINISH GRADE. ROOTS FULLY COVERED WITH SOIL AND FRAMED.
11. WATERING OF PLANTS IS TO TAKE PLACE IMMEDIATELY AFTER PLANTING.
12. MULCH ALL SHRUB AND GROUND COVER AREAS WITH A 3" MIN. LAYER OF 1/2" TO 3/4" REDWOOD BARK.
13. THE COMPLETION OF ALL PLANTING OPERATIONS, THE PREMISES ARE TO BE LEFT NEAT AND CLEAN. ALL EXCESS MULCH, NURSERY POTS AND WASTE ARE TO BE REMOVED FROM THE SITE.
14. THE LANDSCAPE CONTRACTOR IS TO MAINTAIN ALL LANDSCAPE AREAS FOR A PERIOD OF THIRTY CALENDAR DAYS FROM THE DATE OF COMPLETION, ESTABLISHED BY THE OWNER OR HIS REPRESENTATIVE. ALL AREAS ARE TO BE KEPT WELL WATERED, FREE OF GRASSES AND TRASH DURING THIS MAINTENANCE PERIOD.
15. SITE MAINTENANCE (PLEASE SEE WALLACE LAB RECOMMENDATION) IS TO BE MADE JUST PRIOR TO THE COMPLETION OF THE MAINTENANCE PERIOD, OR AT 30-DAY INTERVALS IF MAINTENANCE PERIOD IS GREATER THAN 30 DAYS.
16. ALL WEEDS, SHRUBS AND PLANT MATERIAL (OTHER THAN PLANTED MATERIAL) LESS THAN 1" IN DIAMETER SHALL BE GUARANTEED FOR A PERIOD OF 90 DAYS. ALL MATERIAL LARGER THAN 1" IN DIAMETER SHALL BE GUARANTEED FOR A PERIOD OF 1 YEAR.

RESOLUTION NO. 6041-22

RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF GARDEN GROVE APPROVING SITE PLAN NO. SP-111-2022 AND TENTATIVE PARCEL MAP NO. PM-2021-190, FOR PROPERTY LOCATED ON THE NORTHEAST CORNER OF GARDEN GROVE BOULEVARD AND BROOKHURST STREET, AT 10201 AND 10231 GARDEN GROVE BOULEVARD, ASSESSOR'S PARCEL NOS. 089-072-53 AND 089-072-66.

BE IT RESOLVED that the Planning Commission of the City of Garden Grove, in a regular session assembled on May 19, 2022, hereby approves Site Plan No. SP-111-2022 and Tentative Parcel Map No. PM-2021-190, for a property located on the northeast corner Garden Grove Boulevard and Brookhurst Street, at 10201 and 10231 Garden Grove Boulevard, Assessor's Parcel Nos. 089-072-53 and 089-072-66.

BE IT FURTHER RESOLVED in the matter of Site Plan No. SP-111-2022 and Tentative Parcel Map No. PM-2021-190, the Planning Commission of the City of Garden Grove does hereby report as follows:

1. The subject case was initiated by Dr. Michael Dao.
2. The applicant is requesting Tentative Parcel Map approval to consolidate two (2) existing parcels into a single 1.86 acre parcel and Site Plan approval to construct a five-story mixed-use development on the 1.86-acre site consisting of 9,786 square feet of retail space, 9,270 square feet of medical office space, and 52 apartment units. Pursuant to the State Density Bonus law, in exchange for reserving three (3) of the apartment units for very low-income households, the applicant is requesting a density bonus of seven residential units (7) and the following concession/incentive and waivers from the GGMU-2 (Garden Grove Boulevard Mixed Use 2) zone development standards: (1) a concession to allow each unit to deviate from the minimum private balcony area and dimensions, (2) a waiver to deviate from the minimum active recreation dimensions and to allow the indoor and roof deck recreation areas to contribute to more than 50% of the required open space; (3) a waiver to allow the building to exceed the maximum building height of 50 feet or 4-stories; and (4) a waiver to deviate from the minimum 300 cubic feet of private storage per unit.
3. The City of Garden Grove Planning Commission hereby determines that the proposed project is categorically exempt from review under the California Environmental Quality Act ("CEQA") pursuant to Section 15332 (In-Fill Development Projects) of the State CEQA Guidelines (14 Cal. Code Regs., Section 15303). As set forth in the Class 32 exemption, the proposed project is: (1) consistent with the applicable general plan designation and all applicable general plan policies as well as with applicable zoning designation and regulations; (2) the proposed development occurs within City limits on a project site of no more than five acres substantially surrounded by urban uses;

- (3) the project site has no value as habitat for endangered, rare or threatened species; (4) approval of the project would not result in any significant effects relating to traffic, noise, air quality or water quality; and (5) the site can be adequately served by all required utilities and public services. The project is therefore exempt from CEQA review.
4. The property has a General Plan Land Use designation of Residential/Commercial Mixed Use 2 and is zoned GGMU-2 (Garden Grove Boulevard Mixed Use 2). The site is currently vacant. The property was previously occupied by a furniture store and a used car dealership business. The building structures associated with these uses were demolished in 2019.
  5. Existing land use, zoning, and General Plan designation of property in the vicinity of the subject property have been reviewed.
  6. Report submitted by the City staff was reviewed.
  7. Pursuant to a legal notice, a public hearing was held on May 19, 2022, and all interested persons were given an opportunity to be heard.
  8. The Planning Commission gave due and careful consideration to the matter during its meeting on May 19, 2022; and

BE IT FURTHER RESOLVED, FOUND AND DETERMINED that the facts and reasons supporting the conclusion of the Planning Commission, as required under Municipal Code Section 9.04.030 are as follows:

FACTS:

The project site is comprised of two (2) parcels with a combined land area of 1.86-acres located on the northeast corner of Garden Grove Boulevard and Brookhurst Street. The subject site has a General Plan Land Use designation of Residential/Commercial Mixed Use 2, and is zoned GGMU-2 (Garden Grove Boulevard Mixed Use 2). The Residential/Commercial Mixed Use 2 land use designation is intended to allow a mix of residential and commercial uses around older and underutilized commercial development, while the GGMU-2 zone allows for mixed-use developments.

The project site is located in an area developed with commercial and residential developments. The project site abuts a GGMU-2 zoned property to the north developed with a four-story office professional building and a one-story restaurant building; a Planned Unit Development (PUD-101-95) zoned residential development to the east, developed with 104 detached, three-story, residential homes; GGMU-3 zoned properties to the south, across Garden Grove Boulevard, developed with a Wienerschnitzel restaurant and an auto body shop; and PUD-123-09 zoned parcels to the west, across Brookhurst Street, entitled for the Brookhurst Triangle Project.

The project site is also located in close proximity to two mixed-use projects: the Brookhurst Triangle Project and the Garden Brook Senior Village Project (10032 Garden Grove Boulevard).

The project site is currently vacant and secured with a perimeter chain-link fence. The project site was previously developed and occupied by a furniture store (10201 Garden Grove Boulevard) and a used car dealership (10231 Garden Grove Boulevard) since the late 1950s. In 2019, building permits were issued to demolish all on-site building structures.

The applicant proposes to develop the project site with a five-story, mixed-use development consisting of both commercial lease space and residential units. The project will consist of 19,056 square feet of commercial spaces (9,786 square feet for retail and 9,270 square feet for medical office), and 52 apartment units. The Project will reserve three (3) of the apartment units for "very low-income" households. A Density Bonus Housing Agreement will be recorded to restrict the affordability of the units for 55-years.

The applicant has requested State Density Bonus allowances for density, parking, and concessions and waivers to deviate from the GGMU-2 development standards.

The applicant is requesting one (1) concession and three (3) waivers to deviate from the GGMU-2 (Garden Grove Boulevard Mixed Use 2) zone development standards: (1) a concession to allow each unit to deviate from the minimum private balcony area and dimensions (Municipal Code Section 9.18.110.030.F.1), (2) a waiver to deviate from the minimum active recreation dimensions and to allow the indoor and roof deck recreation areas to contribute to more than 50% of the required open space (Municipal Code Section 9.18.110.030.F.2); (3) a waiver to allow the building to exceed the maximum building height of 50 feet or 4-stories (Municipal Code Section 9.18.090.020); and (4) a waiver to deviate from the minimum 300 cubic feet of private storage per unit (Municipal Code Section 9.18.110.030.H.2).

FINDINGS AND REASONS:

**SITE PLAN**

1. The Site Plan complies with the spirit and intent of the provisions, conditions, and requirements of the Municipal Code and other applicable ordinances and is consistent with the General Plan.

The General Plan Land Use Designation of the subject site is Residential/Commercial Mixed Use 2 (RC 2), and the site is zoned Garden Grove Boulevard Mixed Use 2 (GGMU-2). The RC 2 land use designation is intended to provide a mix of residential and commercial uses mostly around older underutilized developments, with the residential serving as a catalyst to



encourage revitalization of these sites. In addition, the GGMU-2 zone implements the RC 2 land use designation and allows for commercial and residential uses to be developed as integrated developments with lower residential densities that provide landscaping and enhanced pedestrian areas along Garden Grove Boulevard.

The proposed Project is a mixed-use development consisting of approximately 19,056 square feet of commercial space and 52 residential apartment units. The GGMU-2 zone allows a maximum commercial floor area ratio (FAR) of 0.5, and a maximum residential density of 24 units per acre. However, the Project qualifies for a "density bonus" of 22.5% pursuant to the State Density Bonus because the applicant has offered to reserve three (3) of the apartment units for very low-income households. The proposed Project will provide a commercial FAR of 0.235, and a residential density of 27.5 units per acre. The base density of the project is 45 units, and through the State Density Bonus Law, the project will provide seven (7) additional units above the base density. The project will reserve three (3) units for "very low-income" households as required by the State Law. It should be noted that a 22.5% density bonus entitles the project to eleven (11) additional units above the base density; however, the applicant only proposes to construct seven (7) units above the base density.

The proposed Project has been designed to comply with all applicable development standards of the GGMU-2 zone, with exception of parking, density, and the concession and waivers requested pursuant to the State Density Bonus Law. The project applicant has requested a concession to allow each unit to deviate from the minimum private balcony area and dimensions, and has requested three (3) waivers to (1) deviate from the minimum active recreation dimensions and to allow the indoor and roof deck recreation areas to contribute to more than 50% of the required open space; (2) to allow the building to exceed the maximum building height of 50 feet or 4-stories; and (3) to deviate from the minimum 300 cubic feet of private storage per unit.

In addition, the proposed Project is consistent with the goals and policies of the General Plan, including:

- (a) *Policy LU-1.2 to encourage modern residences in areas designated as Mixed Use.*

The proposed Project is a mixed-use development with approximately 19,056 square feet of commercial space and 52 residential apartment units. The residential units are well integrated into the design of the project that features an urban and modern building concept that will offer commercial services conveniently located to the residential units. Each unit is designed as either a 1-bedroom or 2-bedroom unit, and will include amenities, such as a washer/dryer and a private balcony, and

communal recreation areas that are secured and conveniently located to the residential units.

- (b) *Policy LU-1.3 to support the production of housing citywide that is affordable to lower and moderate-income households consistent with the policies and targets set by the Housing Element AND Goal H-2 and Policy H-2.1 to encourage housing supply to accommodate housing needs at all affordability level and to preserve and expand the City's supply of affordable rental housing for lower-income households.*

The proposed Project will provide 52 residential apartment units integrated as part of a mixed-use development. The project includes a density bonus and will provide seven (7) additional units above the base density of 45 units. Based on the State Density Bonus provisions, the project is required to reserve three (3) units for "very low-income" households, which will increase the supply of affordable housing units in the City. Furthermore, the Project will contribute to meeting the City's Regional Housing Needs Allocation (RHNA), as well as the Housing Element policies related to affordability.

- (c) *Policy LU-1.4 to encourage a wide variety of retail and commercial uses, such as restaurant and cultural arts/entertainment, in appropriate locations AND Policy LU-6.2 to encourage a mixed of retail and commercial services along major corridors and in centers to meet the community needs.*

The project is located in a mixed-use zone that is bounded by two major streets, Garden Grove Boulevard and Brookhurst Street. The proposed project includes a commercial component of approximately 19,056 square feet; specifically, the commercial component will devote 9,786 square feet for retail use and 9,270 square feet for medical office use. The project will introduce new commercial uses to the area that will serve the needs of the community. The new permitted uses will be those allowed by the GGMU-2 zone, which are intended to be uses that provide goods and services to adjacent residential uses, including the development's residential uses.

- (d) *Policy LU-1.5 to encourage active and inviting pedestrian friendly street environments that include a variety of uses within commercial and mixed-use areas.*

The GGMU-2 zone includes specific design requirements to encourage active and pedestrian friendly streets. The project includes decorative walkways that connect directly to each commercial tenant storefront from the public rights-of-ways along Garden Grove Boulevard and Brookhurst Street, as well as on-site walkways to encourage pedestrian

activity. The project includes a garden plaza as required by the GGMU-2 zone that will serve as a gathering place to promote pedestrian activity, as well as a passive recreation area for the residential component. The garden plaza will be enhanced with seating, lighting, and landscaping. Landscaping is also an important element to visually enhance, activate, and promote pedestrian friendly streets. The proposed Project will provide canopy trees along the Garden Grove Boulevard and Brookhurst Street rights-of-ways, as well as on-site trees and landscaping to create shade and a comfortable and appealing environment for pedestrians.

- (e) *Policy LU-1.6 that encourages mixed use projects that:*
  - (i) *Create a pleasant walking environment to encourage pedestrian activity;*
  - (ii) *Create lively streetscape, interesting urban spaces and attractive landscaping;*
  - (iii) *Provide convenient shopping opportunities for residents close to their residents*

The project is designed to comply with the GGMU-2 zoning requirements to encourage pedestrian activity. Specifically, the project includes walkways that connect the site from the public rights-of-way along Garden Grove Boulevard and Brookhurst Street, as well as providing on-site walkways that connect each tenant and the garden plaza to encourage pedestrian activity. The project includes landscaping that will create a visually and aesthetically pleasing environment. The project will feature canopy trees along the Garden Grove Boulevard and Brookhurst Street rights-of-ways, as well as landscape planters with a variety of trees, shrubs, and ground cover within the street setback areas to create a lively streetscape that is urban in design; that creates an attractive environment for pedestrians; and that is visually appealing to passing motorists. In addition, the commercial component will provide retail and medical uses that will provide new shopping and job opportunities to local residents.

- (f) *Policy LU 2.2 to strive to provide a diverse mix of housing types AND Goal H-3 to encourage a range of available housing types, densities and affordability levels to meet diverse community needs.*

The proposed project will provide 52 residential apartment units as part of a mixed-use development that will introduce new rental housing units to the local housing market, and will reserve three (3) units for very low-income households to expand the number of affordable housing units in the City to meet the community needs. Furthermore, the proposed project will contribute to the City's meeting its RHNA obligation.

- (g) *Policy LU-3.1 to encourage new residential development and allowing mixed use in older or underutilized commercial centers. Residential housing provides convenient access to jobs and activities and supplies a resident clientele to support commercial sales and services in mixed-use areas.*

The project site is currently vacant and secured with a chain-link fence. The project site was previously developed and occupied by furniture store and a used car dealership that were in operation since the late 1950s. Building permits were issued in 2019 to demolish all on-site structures. The proposed project will revitalize and redevelop the project site with a mixed-use development that includes approximately 19,056 square feet of commercial space and 52 residential apartment units. As a mixed-use project, the Project will provide housing that can support the commercial component. In addition, the residential use is conveniently located to potential employment centers and other commercial services in the area.

- (h) *Goal LU-4 that seeks to develop uses that are compatible with one another AND Policy LU 4.5 that requires that commercial developments adjoining residential uses be adequately screened and buffered from residential areas.*

The project site is located in an area that is developed with commercial and residential uses, including two (2) new mixed-use projects. The two (2) new mixed-use projects are located in close proximity to the project site: the Brookhurst Triangle Project and the Garden Brook Senior Village. The Brookhurst Triangle project is a horizontal mixed-use project located to the west of the project site, across Brookhurst Street, and will include up to 600 residential units and up to 200,000 square feet of commercial/retail space, while the Garden Brook Senior Village (10032 Garden Grove Boulevard) is an eight-story mixed-use project with 11,041 square feet of commercial space and 394 senior units. The proposed mixed-use project will be compatible with the existing commercial developments in the area and with the two (2) mixed-use projects that are under construction.

Furthermore, the project is designed to comply with the encroachment plane requirement of the mixed-use zone to minimize impacts to the residential development located to the east of the project site. The encroachment plane requires the building to maintain a 45-degree angle that commences 15 feet above the grade of the property line located adjacent to the residential use to buffer the residential use from taller and denser developments. The project maintains the required encroachment plane distance as measured from the easterly property

line. The proposed building will maintain a 60'-3" setback from the eastern property line to screen, buffer, and to minimize impacts to the adjacent residential use from the first three floors, and a 68'-3" setback from the fourth and fifth floors.

2. The proposed development does not adversely affect essential on-site facilities such as off-street parking, loading and unloading areas, traffic circulation, and points of vehicular and pedestrian access.

The project site will be accessed from a new drive approach located on Garden Grove Boulevard, and a new drive approach located on Brookhurst Street. The project is designed with a three (3) level parking structure. The on-site circulation is designed with two-way drive aisles that provide access to the parking structure and parking spaces. The site is also designed with the required turn-around area that allows for trash truck and emergency vehicle access. The appropriate red curbs and "No Parking" signs will be installed to allow maneuvering of trash trucks and emergency vehicles within the turn-around area.

The parking structure will be secured with vehicular gates, and during normal business hours, the gates will remain open. During non-business hours, the parking structure gates will be closed, and residents will access the parking structure with a gate remote from the northerly entrance located on the east side of the property. The project will also include a designated vehicle lane with an intercom box where guests can call a unit directly to gain access to the parking structure after-hours. The project includes appropriate turn-around areas in front of the parking structure gates to allow guests to safely maneuver off of the site if access to the parking structure is unavailable.

The project provides pedestrian walkways to access the site from the public right-of-way, including walkways from Garden Grove Boulevard and Brookhurst Street. The project also provides on-site walkways that access the commercial tenants, the garden plaza, and the residential units.

The project has been designed to provide the required number of parking spaces to serve the commercial and residential uses. A total of 169 parking spaces are required for the project, including 104 parking spaces for the commercial component and 65 parking spaces for the residential component. The project provides 182 parking spaces, which includes 13 additional parking spaces to serve the commercial use. The parking distribution includes 175 parking spaces within the parking structure, and seven (7) parking spaces located on the ground-level outside of the parking structure. The commercial parking will accommodate 9,786 square feet of retail, which requires 49 parking spaces, and 9,270 square feet of medical office, which requires 55 parking spaces.

Sixty-five (65) residential parking spaces are provided, which complies with the maximum parking requirement that may be imposed under the State Density Bonus Law. The parking for the development will be located primarily within the parking structure, with the commercial parking spaces located on the ground-level and inside the parking structure on the first and second levels, while the residential parking spaces will be located on the second and third levels of the parking structure. The residential parking spaces will be clearly labeled, and during non-business hours, the residents can use the commercial parking spaces for overflow parking.

A Traffic Study was prepared for the project that reviewed the Project's traffic impact, including impacts to the Level of Service (LOS) of existing intersections, and Vehicles Miles Traveled (VMT) analysis. The study concluded that the project will not have any significant impacts to the LOS, and the project meets the Low VMT Area screening criteria. The Traffic Engineering Division reviewed and confirmed the conclusions of the study.

The City's Traffic Engineering Division has reviewed the proposed project, and all appropriate conditions of approval have been incorporated to minimize any adverse impacts to surrounding streets.

3. The development, as proposed, will not adversely affect essential public facilities such as streets and alleys, utilities, and drainage channels.

The streets in the area will be adequate to accommodate the development once the developer provides the necessary improvements for the project. Utilities and drainage channels in the area are existing and are adequate to accommodate the development. The proposed development will provide landscaping and proper grading of the site, thereby, improving drainage in the area.

The City's Public Works Department has reviewed the proposed project, and all appropriate conditions of approval have been incorporated to minimize any adverse impacts to surrounding streets.

4. The project will not adversely impact the Public Works Department's ability to perform its required function.

The proposed Project will not adversely impact the Public Works Department ability to perform its required function. The City's Public Works Department has reviewed the project, and has incorporated all the appropriate conditions of approval to minimize any adverse impacts.

5. The project is compatible with the physical, functional, and visual quality of the neighboring uses and desirable neighborhood characteristics.

The project is located in a mixed-use area that is developed with commercial and residential developments, including two (2) mixed-use projects that are currently under construction, the Brookhurst Triangle Project and the Garden Brook Senior Village (10032 Garden Grove Boulevard). The Brookhurst Triangle includes up to 600 residential units and up to 200,000 square feet of commercial/retail space on 13.9-acres, and the Garden Brook Senior Village includes 11,041 square feet of commercial space and 394 senior units on 3.1-acres.

The proposed project is a five-story mixed-use development with approximately 19,056 square feet of commercial space, and 52 residential apartment units with a State Density Bonus on 1.86-acres. The proposed project will revitalize a site that is currently vacant with new commercial and residential uses to serve the community.

The proposed architectural design of the building will feature an attractive contemporary style that includes enhanced articulation, detailing, and varied building massing, including storefronts that are oriented toward Garden Grove Boulevard and Brookhurst Street. The project also includes pedestrian-friendly features and landscaping to enhance and activate the streetscape along Garden Grove Boulevard and Brookhurst Street. The project includes decorative walkways that connect directly to each commercial tenant storefront and the garden plaza from Garden Grove Boulevard and Brookhurst Street, as well as on-site walkways that connect each tenant and the garden plaza to encourage pedestrian activity. The garden plaza will serve both as a gathering place to encourage pedestrian activity, and a passive recreation area for the project's residential use. The garden plaza will be enhanced with seating, lighting, and landscaping.

The project will feature canopy trees along the Garden Grove Boulevard and Brookhurst Street rights-of-ways, and on-site landscape planters to create a lively streetscape that is urban in design; that creates an attractive environment for pedestrians; and that is visually appealing to passing motorists.

The proposed project, including the building architecture and landscaping, will be compatible and compliment other projects in the immediate vicinity; will continue to promote mixed-use developments in the GGMU-2 zone; and will assist with implementation of the General Plan that encourages the revitalization of underutilized parcels and mixed-use developments.

6. Through the planning and design of buildings and building replacement, the provision of open space landscaping and other site amenities will attain an attractive environment for the occupants of the property.

The proposed Project will revitalize two (2) older and underutilized commercial properties with a vibrant and attractive mixed-use development. The proposed architectural design of the building will feature an attractive contemporary style that includes enhanced articulation, detailing, and varied building massing. The mixed-use zone includes specific design requirements for garden plazas to encourage the activation of pedestrian-friendly streets. The project includes decorative walkways that connect directly to each commercial tenant storefront and the garden plaza from Garden Grove Boulevard and Brookhurst Street, and on-site walkways to encourage pedestrian activity. The garden plaza will serve as a gathering place to encourage pedestrian activity, and as a passive recreation area for the residential use. The garden plaza will be enhanced with seating, lighting, and landscaping. The proposed landscaping will visually enhance, activate, and promote pedestrian friendly streets. The project will feature new canopy trees along the Garden Grove Boulevard and Brookhurst Street sidewalks, and will also include landscape areas within the street setbacks to create a lively streetscape that is urban in design; that creates an attractive environment for pedestrians; and that is visually appealing to passing motorists.

In addition, the project will provide recreation areas that will serve the residential use. The project includes indoor and outdoor recreation areas located on the fourth and fifth floors of the development. The project includes indoor recreation areas for a gym and lounge, and roof gardens that will incorporate seating, artificial turf, and landscaping.

### **TENTATIVE PARCEL MAP**

1. The proposed map is consistent with the General Plan.

The proposed map is consistent with the provisions of the General Plan Land Use Designation of the Residential/Commercial Mixed Use 2 (RC 2). The RC 2 land use designation is intended to provide a mix of residential and commercial uses mostly around older underutilized developments, with residential serving as a catalyst to encourage revitalization of these sites.

The proposed project is a mixed-use development with approximately 19,056 square feet of commercial space, and 52 residential apartment units with a State Density Bonus. The RC 2 land use designation allows for residential densities up to 24 units per acre. With the proposed State Density Bonus, the residential density for the project will be 27.95 units per acre. The project will provide seven (7) additional units above the base density of 45 units, and will reserve three (3) units for "very low-income" households.

The project site currently consists of two (2) parcels each with a lot size of 31,560 square feet (10201 Garden Grove Boulevard) and 49,272 square feet (10231 Garden Grove Boulevard). A Tentative Parcel Map is being processed



to consolidate the project site into one single-lot with an area of 1.86-acres. The City has determined that a parcel map is the most appropriate instrument to consolidate the parcels due to existing utility easements that must to be relocated and realigned to accommodate the proposed project.

In addition, the proposed Project is consistent with the goals and policies of the General Plan, including:

- (a) *Policy LU-1.2 to encourage modern residences in areas designated as Mixed Use.*

The proposed Project is a mixed-use development with approximately 19,056 square feet of commercial space and 52 residential apartment units. The residential units are well integrated into the design of the project that features an urban and modern building concept that will offer commercial services conveniently located to the residential units. Each unit is designed as either a 1-bedroom or 2-bedroom unit, and will include amenities, such as a washer/dryer and a private balcony, and communal recreation areas that are secured and conveniently located to the residential units.

- (b) *Policy LU-1.3 to support the production of housing citywide that is affordable to lower and moderate-income households consistent with the policies and targets set by the Housing Element AND Goal H-2 and Policy H-2.1 to encourage housing supply to accommodate housing needs at all affordability level and to preserve and expand the City's supply of affordable rental housing for lower-income households.*

The proposed Project will provide 52 residential apartment units integrated as part of a mixed-use development. The project includes a density bonus and will provide seven (7) additional units above the base density of 45 units. Based on the State Density Bonus provisions, the project is required to reserve three (3) units for "very low-income" households, which will increase the supply of affordable housing units in the City. Furthermore, the Project will contribute to meeting the City's Regional Housing Needs Allocation (RHNA), as well as the Housing Element policies related to affordability.

- (c) *Policy LU-1.4 to encourage a wide variety of retail and commercial uses, such as restaurant and cultural arts/entertainment, in appropriate locations AND Policy LU-6.2 to encourage a mixed of retail and commercial services along major corridors and in centers to meet the community needs.*

The project is located in a mixed-use zone that is bounded by two major streets, Garden Grove Boulevard and Brookhurst Street. The proposed

project includes a commercial component of approximately 19,056 square feet; specifically, the commercial component will devote 9,786 square feet for retail use and 9,270 square feet for medical office use. The project will introduce new commercial uses to the area that will serve the needs of the community. The new permitted uses will be those allowed by the GGMU-2 zone, which are intended to be uses that provide goods and services to adjacent residential uses, including the development's residential uses.

- (d) *Policy LU-1.5 to encourage active and inviting pedestrian-friendly street environments that include a variety of uses within commercial and mixed-use areas.*

The GGMU-2 zone includes specific design requirements to encourage active and pedestrian friendly streets. The project includes decorative walkways that connect directly to each commercial tenant storefront from the public rights-of-ways along Garden Grove Boulevard and Brookhurst Street, as well as on-site walkways to encourage pedestrian activity. The project includes a garden plaza as required by the GGMU-2 zone that will serve as a gathering place to promote pedestrian activity, as well as a passive recreation area for the residential component. The garden plaza will be enhanced with seating, lighting, and landscaping. Landscaping is also an important element to visually enhance, activate, and promote pedestrian friendly streets. The proposed Project will provide canopy trees along the Garden Grove Boulevard and Brookhurst Street rights-of-ways, as well as on-site trees and landscaping to create shade and a comfortable and appealing environment for pedestrians.

- (e) *Policy LU-1.6 that encourages mixed use projects that:*

- (i) *Create a pleasant walking environment to encourage pedestrian activity;*
- (ii) *Create lively streetscape, interesting urban spaces and attractive landscaping;*
- (iii) *Provide convenient shopping opportunities for residents close to their residents*

The project is designed to comply with the GGMU-2 zoning requirements to encourage pedestrian activity. Specifically, the project includes walkways that connect the site from the public rights-of-way along Garden Grove Boulevard and Brookhurst Street, as well as providing on-site walkways that connect each tenant and the garden plaza to encourage pedestrian activity. The project includes landscaping that will create a visually and aesthetically pleasing environment. The project will feature canopy trees along the Garden Grove Boulevard and Brookhurst Street rights-of-ways, as well as landscape planters with a

variety of trees, shrubs, and ground cover within the street setback areas to create a lively streetscape that is urban in design; that creates an attractive environment for pedestrians; and that is visually appealing to passing motorists. In addition, the commercial component will provide retail and medical uses that will provide new shopping and job opportunities to local residents.

- (f) *Policy LU 2.2 to strive to provide a diverse mix of housing types AND Goal H-3 to encourage a range of available housing types, densities and affordability levels to meet diverse community needs.*

The proposed project will provide 52 residential apartment units as part of a mixed-use development that will introduce new rental housing units to the local housing market, and will reserve three (3) units for very low-income households to expand the number of affordable housing units in the City to meet the community needs. Furthermore, the proposed project will contribute to the City's meeting its RHNA obligation.

- (g) *Policy LU-3.1 to encourage new residential development and allowing mixed use in older or underutilized commercial center. Residential housing provides convenient access to jobs and activities and supplies a resident clientele to support commercial sales and services in mixed-use areas.*

The project site is currently vacant and secured with a chain-link fence. The project site was previously developed and occupied by furniture store and a used car dealership that were in operation since the late 1950s. Building permits were issued in 2019 to demolish all on-site structures. The proposed project will revitalize and redevelop the project site with a mixed-use development that includes approximately 19,056 square feet of commercial space and 52 residential apartment units. As a mixed-use project, the Project will provide housing that can support the commercial component. In addition, the residential use is conveniently located to potential employment centers and other commercial services in the area.

- (h) *Goal LU-4 that seeks to develop uses that are compatible with one another AND Policy LU 4.5 that requires that commercial developments adjoining residential uses be adequately screened and buffered from residential areas.*

The project site is located in an area that is developed with commercial and residential uses, including two (2) new mixed-use projects. The two (2) new mixed-use projects are located in close proximity to the project site: the Brookhurst Triangle Project and the Garden Brook

Senior Village. The Brookhurst Triangle project is a horizontal mixed-use project located to the west of the project site, across Brookhurst Street, and will include up to 600 residential units and up to 200,000 square feet of commercial/retail space, while the Garden Brook Senior Village (10032 Garden Grove Boulevard) is an eight-story mixed-use project with 11,041 square feet of commercial space and 394 senior units. The proposed mixed-use project will be compatible with the existing commercial developments in the area and with the two (2) mixed-use projects that are under construction.

Furthermore, the project is designed to comply with the encroachment plane requirement of the mixed-use zone to minimize impacts to the residential development located to the east of the project site. The encroachment plane requires the building to maintain a 45-degree angle that commences 15 feet above the grade of the property line located adjacent to the residential use to buffer the residential use from taller and denser developments. The project maintains the required encroachment plane distance as measured from the easterly property line. The proposed building will maintain a 60'-3" setback from the eastern property line to screen, buffer, and to minimize impacts to the adjacent residential use from the first three floors, and a 68'-3" setback from the fourth and fifth floors.

2. The design and improvement of the proposed subdivision are consistent with the General Plan.

The design and improvement of the proposed map is consistent with the General Plan land use designation of Residential/Commercial Mixed Use 2 (RC 2). The RC 2 land use designation is intended to provide a mix of residential and commercial uses mostly around older underutilized developments, with residential serving as the catalyst to encourage revitalization of these sites.

The project site currently consists of two (2) parcels each with a lot size of 31,560 square feet (10201 Garden Grove Boulevard) and 49,272 square feet (10231 Garden Grove Boulevard). A Tentative Parcel Map is being processed to consolidate the project site into one single-lot with a lot size of 1.86-acres. The City has determined that a parcel map is the most appropriate instrument to consolidate the parcels due to existing utility easements that must to be relocated and realigned to accommodate the proposed project.

The proposed subdivision will allow the project site to be developed with a mixed-use project with approximately 19,056 square feet of commercial space, and 52 residential apartment units with a State Density Bonus. The RC 2 land use designation allows for residential densities up to 24 units per acre. With the State Density Bonus, the project will have a residential density of 27.5 units per acre. The project will include seven (7) additional units above the

base density of 45 units. It should be noted that while the project applicant has requested a density bonus of 22.5%, which entitles the project to eleven (11) additional units above the base density for total of 56 units, the applicant only proposes to construct seven (7) units above the base density.

The proposed parcel configuration and design of the project is compatible with existing commercial and residential developments in the area, including two (2) mixed-use projects that are currently being developed, the Brookhurst Triangle Project and Garden Brook Senior Villages.

In addition, the proposed project will further the goals and policies of the General Plan Land Use Element and Housing Element by redeveloping a vacant and underutilized site with a mixed-use development that will introduce new commercial uses to serve the needs of the community, that will encourage a pedestrian-friendly environment, and that will introduce new housing stock to the community, including affordable units, to meet the City's regional housing needs. The proposed project is also consistent with several goals and policies of the General Plan, as described in Finding No. 1, above.

3. The site is physically suitable for the type of development and complies with the spirit and intent of the Municipal Code.

The site is physically suitable for the proposed development. The Tentative Parcel Map will consolidate the project site, which currently consists of two (2) parcels, into one lot with a combined lot area of 1.86 acres. The site will have sufficient land area to accommodate the proposed mixed-use project with approximately 19,056 square feet of commercial space, and 52 residential apartment units with a State Density Bonus. The site design will accommodate the required drive approaches and drive-aisles to access the site designed to the City's standard, and will also provide adequate access and a turn-around areas for trash trucks and emergency vehicles.

Furthermore, the project has been designed to comply with all applicable development standards of the GGMU-2 zone, with exception of parking, density, and the concession and waivers requested pursuant to the State Density Bonus. The project applicant has requested a State Density Bonus concession to allow each unit to deviate from the minimum private balcony area and dimensions, and three (3) waivers to (1) deviate from the minimum active recreation dimensions and to allow the indoor and roof deck recreation areas to contribute to more than 50% of the required open space; (2) to allow the building to exceed the maximum building height of 50 feet or 4-stories; and (3) to deviate from the minimum 300 cubic feet of private storage per unit.

Therefore, the proposed project complies with the spirit and intent of the Municipal Code.

4. The design of the subdivision and the proposed improvements are not likely to cause substantial environmental damage or substantially and avoidably injure fish or wildlife or their habitat, and the requirements of the California Environmental Quality Act have been satisfied.

The proposed project site is located in an urbanized area that is not habitat for fish or wildlife, and the project is categorically exempt from CEQA pursuant to Section 15332 (In-Fill Development Projects) of the CEQA Guidelines (14 Cal. Code Regs., Section 15332). The proposed project is characterized as in-fill development meeting conditions described in Section 15332.

5. The site is physically suitable for the proposed density of the development.

The site is physically suitable for the density proposed by the developer. The Residential/Commercial Mixed Use 2 land use designation allows for mixed-use projects. The maximum floor area ratio (FAR) allowed for the commercial component is 0.5, and the maximum residential density allowed is 24 units per acre. The project will provide a commercial FAR of 0.235, and through a State Density Bonus, the project will provide a residential density of 27.5 units per acre. The proposed project will consist of approximately 19,056 square feet of commercial space and 52 residential apartment units. The project will provide seven (7) additional units above the base density.

It should be noted that while the project applicant has requested a density bonus of 22.5%, which entitles the project to eleven (11) additional units above the base density for total of 56 units, the applicant only proposes to construct seven (7) units above the base density.

The proposed Tentative Parcel Map will consolidate two (2) existing parcels, each with a lot size of 31,560 square feet (10201 Garden Grove Boulevard) and 49,272 square feet (10231 Garden Grove Boulevard), into one single-lot with a combined land area of 1.86-acres to facilitate the development of the proposed project. The Tentative Parcel Map is the best instrument to consolidate the parcels due to existing utilities easements that have to be relocated and realigned to facilitate the development.

Therefore, the subject site is sufficient in size to accommodate the proposed development, and complies with all applicable provisions of the City of Garden Grove Municipal Code, with exception of the parking, density and concession and waivers requested pursuant to the State Density Bonus Law.

6. The design of the subdivision and the proposed improvements are not likely to cause serious public health problems.

The design of the subdivision and the proposed improvements are not likely to cause serious public health problems since the conditions of approval will be

in place to safeguard the public health. The proposed subdivision has been designed to comply with the development standards of the GGMU-2 zone, with exception of the parking, density, and concession and waivers requested pursuant to the State Density Bonus Law. City Departments, including Traffic Division, Water Division, Engineering Division, Planning Division, and the Orange County Fire Authority (OCFA) have reviewed the proposed development and have applied conditions of approval to minimize potential negative impacts that the project may have on the community. The conditions of approval for on and off-site improvements will safeguard the public health.

7. The design of the project and the proposed improvements will not conflict with easements of record or easements established by court judgment acquired by the public at large for access through or use of property within the subdivision; or, if such easements exist, alternate easements for access or for use will be provided and these will be substantially equivalent to the ones previously acquired by the public.

The design of the subdivision and the proposed improvements will not conflict with easements of record or easements established by court judgment acquired by the public at large for access through or use of property within the proposed subdivision. The project has been designed to avoid development over existing easements.

8. The design and improvement of the proposed subdivision are suitable for the uses proposed and the subdivision can be developed in compliance with the applicable zoning regulations.

The design of the subdivision is suitable for the proposed mixed-use project. The proposed Tentative Parcel Map will consolidate two (2) existing parcels, each with a lot size of 31,560 square feet (10201 Garden Grove Boulevard) and 49,272 square feet (10231 Garden Grove Boulevard), into one-parcel with a combined lot area of 1.86-acres. The proposed project is a mixed-use project with approximately 19,056 square feet of commercial space and 52 residential apartment units with a State Density Bonus.

The subdivision complies with the spirit and intent of the General Plan, and the Subdivision Map Act. The project has also been designed to comply with the GGMU-2 development standards, with exception of the parking, density, and concession and waivers requested pursuant to the State Density Bonus Law.

9. The design of the subdivision provides, to the extent feasible, for future passive or natural heating and cooling opportunities in the subdivision.

To the extent feasible, the project has been designed in accordance with Government Code Section 66473.1, such as to allow for passive or natural heating opportunities in the subdivision design, to encourage the orientation

of structures to take advantage of shade and prevailing breezes, to allow solar access for passive heating and opportunities for placement of shade trees and other vegetation for cooling.

10. The design, density, and configuration of the subdivision strike a balance between the affect of the subdivision on the housing needs of the region and public service needs. The character of the subdivision is compatible with the design of existing structures, and the lot sizes of the subdivision are substantially compatible with the lot sizes within the general area.

The project has been reviewed in relation to the housing needs and goals of the City and is compatible with other projects in the vicinity. The project is located in a mixed-use area that is developed with commercial and residential uses, including two (2) mixed-use projects that are currently under construction, the Brookhurst Triangle Project and the Garden Brook Senior Village (10032 Garden Grove Boulevard). The Brookhurst Triangle Project, which is a horizontal mixed-use project located to the west of the project site, across Brookhurst Street, includes up to 600 residential units and up to 200,000 square feet of commercial/retail space on 13.9-acres, while the Garden Brook Senior Village is an eight-story mixed-use project with 11,041 square feet of commercial space and 394 senior living units on 3.1-acres.

The proposed project is a mixed-used development with approximately 19,056 square feet of commercial space and 52 residential apartment units with a State Density Bonus on 1.86-acres. Through the proposed Tentative Parcel Map, the project will combined two (2) existing lots into one-single lot. The proposed project is compatible with the design of existing structures and developments in the area, and the subdivision is also compatible with the lot sizes in the general area.

The proposed project will introduce new commercial uses to serve the needs of the community, and will introduce new housing stock to the community, including affordable units, to meet the City's regional housing needs. The project will further the goals and policies of the City's Land Use Element and Housing Element.

11. The subject property is not located within a state responsibility area or a very high fire hazard severity zone, the proposed subdivision is served by local fire suppression services, and the proposed subdivision meets applicable design, location, and ingress-egress requirements.
12. The discharge of waste from the proposed subdivision into the existing sewer system will not result in violation of existing requirements prescribed by the California Regional Water Quality Control Board. The conditions of approval for on and off-site improvements will ensure permitted capacity of the public sewer system is not exceeded.



**NO NET LOSS (GOVERNMENT CODE SECTION 65863) FINDINGS**

1. If approval of the proposed Project will result in development of the Site at a lower residential density, the reduction in residential density is consistent with the adopted general plan, including the housing element.

The project site is comprised of two (2) parcels with a combined land area of 1.86-acres. The parcel located at 10201 Garden Grove Boulevard (APN # 089-072-53) is 0.72 acres in size. The parcel located at 10231 Garden Grove Boulevard (APN #089-072-66) is 1.13 acres in size. Both parcels have a General Plan Land Use designation of Residential/Commercial Mixed Use 2 and are zoned GGMU-2 (Garden Grove Boulevard Mixed Use 2). The combined project site is identified in the City’s Housing Element sites inventory as having a realistic capacity to accommodate a total of 54 “above moderate income” units, consisting of 25 units on 10201 Garden Grove Boulevard and 29 units on 10231 Garden Grove Boulevard. The project proposes a total of 52 units, consisting of 49 “above moderate income” units and three (3) “very low-income” units. The maximum allowable residential density for the site under the General Plan Land Use Element and Municipal Code is 24 units per acre, or 45 units. The applicant is proposing to develop the site at the maximum permitted density. In addition, the project includes a request for a density bonus of seven (7) additional units, for a total of 52 units. This equates to a density of 27.5 units per acre. The City cannot require the applicant to seek a larger density bonus. Therefore, even though the proposed project results in two fewer units than identified in the Housing Element site inventory, the 52 units proposed is consistent with the density limits of the General Plan. The proposed project is also consistent with several goals and policies of the General Plan Land Use Element, as noted in the findings above.

2. The remaining sites identified in the housing element are adequate to meet the requirements of Government Code Section 65583.2 and to accommodate the City’s share of the regional housing need pursuant to Government Code Section 65584; or, if not, the City has, or will within 180 days, identify and make available additional adequate sites to accommodate the City’s share of the regional housing need by income level.

The City’s 6<sup>th</sup> Cycle RHNA requires the City to plan for 19,168 housing units for all income levels. A component of preparing the City’s Housing Element is the identification of vacant and underutilized sites suitable for residential development, and an evaluation of the housing development potential of these sites in fulfilling the City’s RHNA. The combined project site is identified in the City’s Housing Element sites inventory as having a realistic capacity to accommodate a total of 54 “above moderate income” units, consisting of 25 units on 10201 Garden Grove Boulevard and 29 units on 10231 Garden Grove

Boulevard. The project proposes a total of 52 units, consisting of 49 “above moderate income” units and three (3) “very low-income” units. Although the proposed project includes fewer units than shown in the Housing Element sites inventory, the remaining Housing Element inventory sites have sufficient capacity to accommodate the City’s remaining unmet RHNA by income level if the project is approved as proposed.

At the time the Housing Element was prepared and adopted, the City’s *unmet* RHNA was calculated to be 18,208 units, broken down as follows: 6,567 low and very low-income units, 3,087 moderate-income units, and 8,554 above moderate-income units. The sites identified in the adopted Housing Element were determined adequate to accommodate a total of 18,291 units, including 401 more units than the City’s unmet RHNA in the low and very low-income categories and 240 more units than the City’s unmet RHNA for the moderate-income category. Sites deemed adequate to accommodate the lower and moderate-income categories are also adequate to accommodate above-moderate income units. Overall, the sites inventory in the adopted Housing Element reflected a total capacity surplus of 83 units.

Excluding ADUs, 17 new housing units new housing units not accounted for in the Housing Element site capacity analysis have recently been permitted or approved. These include two (2) deed restricted units in the lower income categories (one very low-income unit and one low-income unit) and 15 above moderate-income units. After accounting for these permitted and entitled units, and the 52 units in the proposed project, the City’s total remaining unmet RHNA would be 18,139 units, including 6,562 low and very low-income units, 3,087 moderate-income units, and 8,490 above-moderate income units. The remaining capacity of the sites identified in the Housing Element would be 18,226 units, resulting in a total capacity surplus of 87 units. The remaining sites are also adequate to accommodate a surplus of low and very low-income units (403) and moderate-income units (240), and such sites are also adequate to accommodate the City’s remaining unmet above moderate-income RHNA. Due to this surplus, the overall residential capacity on sites identified in the Housing Element will still be sufficient to accommodate the City’s total remaining unmet RHNA if the proposed project is approved.

INCORPORATION OF FACTS AND FINDINGS SET FORTH IN STAFF REPORT

In addition to the foregoing, the Planning Commission incorporates herein by this reference, the facts and findings set forth in the staff report.

BE IT FURTHER RESOLVED that the Planning Commission does conclude:

1. The Site Plan and Tentative Parcel Map possess characteristics that would justify the request in accordance with Municipal Code Section No. 9.32.030.3 (Site Plan) and Section 9.40.060 (Tentative Maps).

2. In order to fulfill the purpose and intent of the Municipal Code and thereby promote the health, safety, and general welfare, the attached Conditions of Approval (Exhibit "A") shall apply to Site Plan No. SP-111-2022 and Tentative Parcel Map No. PM-2021-190.

## **EXHIBIT "A"**

**Site Plan No. SP-111-2022**

**Tentative Parcel Map No. PM-2021-190**

10201 and 10231 Garden Grove Boulevard

### **CONDITIONS OF APPROVAL**

#### **GENERAL CONDITIONS**

1. Each owner of the property shall execute, and the applicant shall record against the property, a "Notice of Discretionary Permit Approval and Agreement with Conditions of Approval" as prepared by the City Attorney's Office, on the property. Proof of such recordation is required prior to issuance of building permits.
2. All Conditions of Approval set forth herein shall be binding on and enforceable against each of the following, and whenever used herein, the term "applicant" shall mean and refer to each of the following: the project applicant, Dr. Michael Dao, the developer of the project, the owner(s) and tenants(s) of the property, and each of their respective successors and assigns. All conditions of approval are required to be adhered to for the life of the project, regardless of property ownership.
3. Approval of this Site Plan and Tentative Parcel Map shall not be construed to mean any waiver of applicable and appropriate zoning and other regulations; and wherein not otherwise specified, all requirements of the City of Garden Grove Municipal Code shall apply.
4. Minor modifications to the Site Plan, Tentative Parcel Map, and/or these Conditions of Approval may be approved by the Community and Economic Development Director, in his or her discretion. Proposed modifications to the project and/or these Conditions of Approval determined by the Community and Economic Development Director not to be minor in nature shall be subject to approval of new and/or amended land use entitlements by the applicable City hearing body.
5. All conditions of approval shall be implemented at the applicant's expense, except where specified in the individual condition.

#### **Public Works Engineering Division**

##### Project Design

6. A geotechnical study prepared by a registered geotechnical engineer is required. The report shall analyze the liquefaction potential of the site and

make recommendations. The report shall analyze sub-surface issues related to the past uses of the site, including sub-surface tanks and basement and septic facilities. Any soil or groundwater contamination shall be remediated prior to the issuance of a building permit per the requirements of the Orange County Health Department and the mitigation requirements of governing regulatory requirements. The report shall make recommendations for foundations and pavement structural section design of interior streets and parking spaces. The report shall also test and analyze soil conditions for LID (Low Impact Development) principles and the implementation of water quality for storm water run-off, including potential infiltration alternatives, soil compaction, saturation, permeability, and groundwater levels.

7. Prior to the issuance of any grading or building permits, the applicant shall submit to the City for review and approval a final design Water Quality Management Plan that:
  - a. Addresses required mitigation Site Design Best Management Practices (BMPs) based upon the latest Santa Ana Regional Water Quality Control Board (SARWQCB) approved Drainage Area Management Plan (DAMP) as identified in the geotechnical report recommendations and findings, including, but not limited to, infiltration minimizing impervious areas, maximizing permeability, minimizing directly connected impervious areas, creating reduced or "zero discharge" areas, and conserving natural areas as required by the latest adopted County of Orange Technical Guidance Document (TGD).
  - b. BMP's shall be sized per the requirements of the latest Technical Guidance Documents.
  - c. Incorporates the applicable Routine Source Control BMPs as defined in the DAMP.
  - d. Incorporates structural and Treatment Control BMPs as defined in the DAMP.
  - e. Generally describes the long-term operation and maintenance requirements for the Treatment Control BMPs.
  - f. Identifies the entity that will be responsible for long-term operation and maintenance of the Treatment Control BMPs.
  - g. Describes the mechanism for funding the long-term operation and maintenance of the Treatment Control BMPs.
  - h. Provides a hydrological analysis with scaled map as well as hydrologic and hydraulic calculations to size storm drains per the Orange County RDMD standards.

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8. Parkway culverts shall be designed per City of Garden Grove Standard Plan B-209. Storm drain lateral pipe connections to City maintained storm drains within City right-of-way shall be RCP with a minimum diameter of 18-inches.
9. Grading and Street improvement plans prepared by a registered Civil Engineer are required. As required under Section 107 of the California Building Code (CBC), the grading plan shall be based on a current survey of the site, including a boundary survey, topography on adjacent properties up to 30' outside the boundary, and designed to preclude cross-lot drainage. Minimum grades shall be 0.50% for concrete flow lines and 1.25% for asphalt. The grading plan shall also include water and sewer improvements. The grading plan shall include a coordinated utility plan showing all existing utility facilities, easements and proposed utility facilities. All on-site improvements shall be tied by horizontal dimensional control to the property boundary as established by survey. A minimum uninterrupted 20-foot wide throat access to the site is required from the street for the multi-residential projects and shall meet the requirements of the California Fire Code throughout the site. Vehicle maneuvering, as demonstrated by Auto Turn along private streets and access ways, shall be demonstrated on the grading plan. Street improvement plans shall conform to all format and design requirements of the City Standard Drawings & Specifications.
10. All vehicular access drives to the site shall be provided in locations approved by the City Traffic Engineer. (Policies and Procedures – TE-17)
11. The applicant shall coordinate with Planning Services and Orange County Fire Authority to identify proper emergency vehicle access to the site and shall provide the Engineering Division a copy of the approval letters upon first submittal of the grading and street improvement plans.
12. The applicant shall complete the following for the parcel map:
  - a. Prior to recordation of a final parcel or tract map, the surveyor/engineer preparing the map shall tie the boundary of the map into the Horizontal Control System established by the County Surveyor in a manner described in Sections 7-9-330 and 7-9-337 of the Orange County Subdivision Code and Orange County Subdivision Manual, Subarticle 18. The surveyor/engineer shall submit record information to the City on Auto Cad DWG format.
  - b. Prior to recordation of a final parcel or tract map, the surveyor/engineer preparing the map shall submit to the County Surveyor a digital graphics file of said map in a manner described in Sections 7-9-330 and 7-9-337 of the Orange County Subdivision Code and Orange County Subdivision Manual, Subarticle 18. The surveyor/engineer shall submit record information to the City on Auto Cad DWG format.

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- c. Prior to issuance of a grading permit, the applicant shall submit to the Planning Services Division an updated title report along with copies of the recorded instruments listed in the title report, reference maps used to prepare legal description and the plat for review and approval of the parcel map.
  - d. All subdivision mapping shall be concurrently reviewed by the City Engineering Division and the County of Orange Survey Department. The applicant shall forward all plan check comments received from the County of Orange Survey Department to the City of Garden Grove's Engineering Division upon receipt from the county.
13. Prior to the issuance of the street improvement encroachment permit and grading permit, provide subdivision completion bonds for all work constructed under the street improvements and grading permit for review and approval by the City Engineer, City Attorney, and City Finance Department (Risk Management). Alternate forms of security may be considered, solely at the discretion of the City Engineer and with the concurrence of the City Attorney and City Finance Department (Risk Management)
14. Any new drive approaches to the site shall be constructed in accordance with Garden Grove Standard B-120 (option #2) as they conform to land use and roadway designation.
15. Prior to issuance of a grading permit, the applicant shall design overhead street lighting within the frontage of the development in conformance with City specifications and the approval of the City's Lighting Administrator. Location of lighting poles shall be shown on all the improvement plans and grading plan.
16. The grading plan shall depict an accessibility route for the ADA pathway in conformance with the requirements of the Department of Justice standards, latest edition and section 1110A of the California Building Code.
17. All trash container areas shall meet the following requirements per City of Garden Grove Standard B-502 and State mandated commercial organic recycling law-AB 1826, including any other applicable State recycling laws related to refuse, recyclables, and/or organics:
  - a. Paved with an impervious surface, designed not to allow run-on mixing of drainage from adjoining areas, designed to divert drainage from adjoining roofs and pavements to be directed around the area for trash roll out, and screened or walled to prevent off-site transport of trash by water or wind.
  - b. Provide solid roof or awning to prevent direct precipitation into the enclosure.

- c. Connection of trash area drains to the municipal storm drain system is prohibited. Drainage from the enclosure may be directed to a conforming grease or contaminant interceptor.
  - d. Potential conflicts with fire code access requirements and garbage pickup routing for access activities shall be considered in implementation of design and source control. See CASQA Storm Water Handbook Section 3.2.9 and BMP Fact Sheet SD-32 for additional information.
  - e. The trash enclosure and containers shall be located to allow pick-up and maneuvering, including turn-arounds, in the area of enclosures, and concrete aprons for roll-out areas.
  - f. Pursuant to state mandated commercial organic recycling law-AB 1826, the applicant is required to coordinate storage and removal of the organics waste with local recycling/trash company.
  - g. Pursuant to applicable state mandated laws, the applicant is required to contact and coordinate with the operations manager of the local recycling/trash company (Republic Services, 800-700-8610) to ensure the trash enclosure includes the appropriate size and number of containers for the disposal of items such as, but may not limited to, municipal solid waste (MSW), recyclables, and organic green waste.
  - h. Based on the amount of waste disposed, per week, the applicant shall coordinate with the local recycling/trash company to ensure the adequate frequency of trash pick-up is serviced to the site for municipal solid waste (MSW), recyclables, and organic green waste, including any other type of waste.
  - i. The applicant shall ensure large bulk items, intended for coordinated and scheduled pick-up by the local recycling/trash company, are not placed in areas that encroach into drive-aisles, parking spaces, pedestrian pathways, or areas in the front of the property including areas public right-of-way (e.g., street, sidewalk), during and after construction. Any large bulk items shall be out of public vantage points.
  - j. The requirements for the trash enclosure and design criteria are bound and coordinated with the Water Quality Management Plan (WQMP), when required, as depicted on the project grading plan, which shall be incorporated into the WQMP by narrative description, exhibits and an Operation and Maintenance Plan (O&M).
18. Any new or required block walls and/or retaining walls shall be shown on the grading plans, both in plan-view and cross sections. Cross sections shall show



vertical and horizontal relations of improvements (existing and proposed) on both sides of property lines. Required wall heights shall be measured vertically from the highest adjacent finished grade. Block walls shall be designed in accordance to City of Garden Grove Standard B-504, B-505, B-506 & B-508 or designed by a professional registered engineer. In addition, the following shall apply:

- a. The color and material of all proposed block walls, columns, and wrought iron fencing shall be approved by the Planning Services Division Prior to installation.
  - b. Openings for drainage through walls shall be shown in section details and approved by the City Engineer. Cross-lot drainage is not allowed.
19. The applicant shall remove any existing substandard driveway approaches, curbs, sidewalks, ADA ramps, pavement sections, tree well and landscaping, and construct Garden Grove Boulevard frontage improvements as identified below. All landscape, irrigation, sidewalk, and lighting improvements installed within the public rights-of-way shall require the approval of the City Engineer, Street Division, and Planning Services Division and shall be maintained by the applicant for the life of the Project.
- a. A separate street improvement plan shall be prepared and submitted to the Engineering Division for the proposed improvements within the public right-of-way Garden Grove Boulevard/Brookhurst Street, which shall include any proposed landscaping and irrigation plans. All work shall be per City standards and specifications.
  - b. Existing substandard driveways (3 total) on Garden Grove Boulevard shall be removed and replaced with new curb, gutter, landscape, and sidewalk per City standards and specifications.
  - c. The new driveway approach to the site on Garden Grove Boulevard shall be constructed in accordance with Garden Grove Standard B-120 (option #2).
  - d. The applicant shall remove the existing damaged sidewalk panels fronting the project on Garden Grove Boulevard and replace it with new sidewalk panels in accordance with City of Garden Grove Standard B-106. The owner/contractor shall verify the removal and replacement sections of the sidewalk concrete panels with public works inspector prior to start of construction.
  - e. The new tree wells fronting the project on Garden Grove Boulevard shall be constructed in accordance with Garden Grove Standard B-123 and B-127.

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- f. The applicant shall plant total of six Columbia Sycamore tree (36-inch box). The applicant shall coordinate with City's public works division prior to order and placement of trees on Garden Grove Boulevard.
  - g. Construct curb and gutter when replacing any existing driveway approaches along the property frontage on Garden Grove Boulevard in accordance with City Standard Plan B-113.
  - h. The applicant shall locate all existing public utilities across the property frontage and within the property boundary of the project prior to commencement of grading operation and mobilization.
  - i. The applicant shall coordinate with the Planning Services Division and Public Works Street Division before placing any type of tree within public right-of-way and proposed landscape area.
  - j. Street signs shall be installed as required and approved by the City Traffic Engineer.
20. The applicant shall remove any existing substandard driveway approaches, curbs, sidewalks, ADA ramps, pavement sections, tree well and landscaping, and construct Brookhurst Street frontage improvements as identified below. All landscape, irrigation, sidewalk, and lighting improvements installed within the public rights-of-way shall require the approval of the City Engineer, Street Division, and Planning Division and shall be maintained by the applicant for the life of the Project.
- a. A separate street improvement plan shall be prepared and submitted to the Engineering Division for the proposed improvements within the public right-of-way Garden Grove Boulevard/Brookhurst Street, which shall include any proposed landscaping and irrigation plans. All work shall be per City standards and specifications.
  - b. Existing substandard driveway on Brookhurst Street shall be removed and replaced with new curb, gutter, landscape, and sidewalk per City standards and specifications.
  - c. The new driveway approach to the site on Brookhurst Street shall be constructed in accordance with Garden Grove Standard B-120 (option #2).
  - d. The applicant shall remove the existing damaged sidewalk panels fronting the project on Brookhurst Street and replace it with new sidewalk panels in accordance with City of Garden Grove Standard B-106. The owner/contractor shall verify the removal and replacement sections of the sidewalk concrete panels with public works inspector prior to start of construction.

- e. The applicant shall install delineator and striping to prevent southbound left turns into Brookhurst Street Driveway approach and also to prevent exiting left-turns onto southbound Brookhurst Street per approved site plan SP-111-2022.
  - f. The new tree wells fronting the project on Brookhurst Street shall be constructed in accordance with Garden Grove Standard B-123 and B-127.
  - g. The applicant shall plant total of seven Columbia Sycamore tree (36-inch box). The applicant shall coordinate with City's public works division prior to order and placement of trees on Brookhurst Street.
  - h. Construct curb and gutter when replacing any existing driveway approach along the property frontage on Brookhurst Street in accordance with City Standard Plan B-113.
  - i. The applicant shall locate all existing public utilities across the property frontage and within the property boundary of the project prior to commencement of grading operation and mobilization.
  - j. The applicant shall coordinate with the Planning Services Division and Public Works Street Division before placing any type of tree within public right-of-way and proposed landscape area.
  - k. Street signs shall be installed as required and approved by the City Traffic Engineer.
21. Any proposed new landscaping in public right-of-way shall be approved by Planning Services and maintained by the owner for the life of the Project.
  22. Driveway widths shall be in accordance with City's Traffic Engineering Policy TE-8 (Driveway Opening Policy).
  23. Sight Distance Standards shall be in accordance with City's Traffic Engineering Policy TE-13. All structures and walls shall be designed to ensure proper vision clearance for cars entering or leaving the driveway and parking areas. No structure, wall or fence shall cause an exceedance of the applicable site distance standards set forth in City Traffic Engineering Policy TE 13.
  24. The Site Plan and Tentative Map shall comply with the completed Development Review and Comment Sheet prepared pursuant to City's Traffic Engineering Policy TE-17 and provided to the applicant.
  25. Private Property Tow Away Sign Design shall be in accordance with City's Traffic Policy & Procedures TE-19.

26. No Parking Fire Lane Sign Design shall be in accordance with City's Traffic Policy & Procedures TE-20.
27. Traffic Impact Mitigation Fees / Transportation Facilities Fees shall be in accordance with Garden Grove City Council Resolution 9401-16 and Chapter 10.110 of the Garden Grove Municipal Code.
28. Parking lot layout shall be in accordance with City Standard B-311 & B-312.
29. The design, placement, and construction of off-street parking areas and spaces, driveways, maneuvering areas, and garages and carports shall conform to the standards set forth in Section 7-9-70.3 of the Orange County Code of Ordinances (Off-street parking requirements for residential uses A minimum three-foot-by-three-foot-wide maneuvering area shall be provided at the end of a dead-end parking aisle and shall consists of a ten-foot-by-nineteen-foot-wide turn-around space.
30. For any security gate that restricts vehicular access onto any multi-family residential development, the following requirements shall apply:
  - a. All vehicular security gates shall be constructed of material that is not view-obscuring.
  - b. All vehicular security gates shall be automatically operated by remote control devices. Furthermore, such vehicular gates shall not be permitted to swing-in or out.
  - c. **Pedestrian Access.** A separate pedestrian walkway leading from the public sidewalk into a development shall be provided for any development with a security gate across a vehicular access driveway. Furthermore, any security gate that restricts pedestrian access to dwelling units of any multi-family residential development, an intercom or telephone system shall be provided at the gate for each dwelling unit with restricted access as a condition of approval of the security gate.
  - d. **Gate Width.** Vehicular entry gates shall open a minimum 20 feet to completely clear the minimum driveway width. Additional width may be required by the City for access or circulation purposes.
  - e. **Vehicular Turn-around.** A turn-around area outside of the gate shall be required for any development.
  - f. **Trash Pickup.** A means of entry, to be approved by the City-franchised trash disposal service, shall be provided which automatically opens the vehicular gate(s) for trash pickup.

Permit Issuance

31. The applicant shall be subject to Traffic Mitigation Fees, In-Lieu Park Fees Drainage Facilities Fees, Water Assessment Fees, and other applicable mitigation fees identified in Chapter 9.44 of the Garden Grove Municipal Code, along with all other applicable fees duly adopted by the City. The amount of said fees shall be calculated based on the City's current fee schedule at the time of permit issuance.
32. A separate street permit is required for work performed within the public right-of-way.
33. Grading fees shall be calculated based on the current fee schedule at the time of permit issuance.
34. The applicant shall identify a temporary parking site(s) for construction crew and construction trailers office staff prior to issuance of a grading permit. No construction parking is allowed on local streets. Construction vehicles should be parked off traveled roadways in a designated parking area. Parking areas, whether on-site or off-site, shall be included and covered by the erosion control plans.
35. Prior to issuance of a grading permit, the applicant shall submit and obtain approval of a work-site traffic control plan for all the proposed improvements within public right-of-way, and shall be subject to the review and approval of the City Traffic Engineer.
36. In accordance to City of Garden Grove Municipal Code (Chapter 9.48.030), the applicant is required to underground all existing and proposed on-site and off-site utility facilities fronting the project, which the developer is developing or redeveloping. All existing improvements and utilities shall be shown as part of the grading submittal package in the topography section.

Project Construction/Operation

37. The applicant shall coordinate with City's Public Works Department (engineering, water services and streets division) and setup appointments for pre-construction inspections for all the on-site and off-site improvements prior to commencement of grading operation and mobilization.
38. In accordance with the Orange County Storm Water Program manual, the applicant and/or its contractors shall provide dumpsters on-site during construction unless an Encroachment Permit is obtained for placement in street.
39. The applicant and his contractor shall be responsible for protecting all existing horizontal and vertical survey controls, monuments, ties (centerline and

- corner) and benchmarks located within the limits of the project. If any of the above require removal; relocation or resetting, the Contractor shall, prior to any construction work, and under the supervision of a California licensed Land Surveyor, establish sufficient temporary ties and benchmarks to enable the points to be re-set after completion of construction. Any ties, monuments and bench marks disturbed during construction shall be reset per Orange County Surveyor Standards after construction. Applicant and his contractor shall also re-set the tie monuments where curb or curb ramps are removed and replaced or new ramps are installed. The Applicant and his contractor shall be liable for, at his expense, any resurvey required due to his negligence in protecting existing ties, monuments, benchmarks or any such horizontal and vertical controls. Temporary Benchmarks shall not be used for Vertical control. Benchmarks shall be to the National Geodetic Vertical Datum (NGVD).
40. Heavy construction truck traffic and hauling trips, and any required lane closures shall occur outside peak travel periods. Peak travel periods are considered to be from 7 a.m. to 9 a.m. and 4 p.m. to 6 p.m.
41. Prior to grading or building permit closeout and/or the issuance of a certificate of use or a certificate of occupancy, the applicant shall:
- a. Demonstrate that all structural best management practices (BMPs) described in the Project WQMP have been constructed and installed in conformance with approved plans and specifications.
  - b. Demonstrate that the applicant is prepared to implement and maintain all non-structural BMPs described in the Project WQMP.
  - c. Demonstrate that an adequate number of copies of the approved Project WQMP are available on-site.
  - d. Submit for review and approval by the City an Operations and Maintenance (O&M) Plan for all structural BMPs.

### **Public Works Water Services Division**

42. New water connections may be taken off the 8" water main on the east side of Brookhurst Street, or off the 10" water main on Garden Grove Boulevard.
43. New water service installations 2" and smaller, may be installed by the City of Garden Grove at owner's/developer's expense. Installation shall be scheduled upon payment of applicable fees, unless otherwise noted. Fire services and larger water services 3" and larger, shall be installed by applicant's contractor per City Standards.
44. Water meters shall be located within the City right-of-way. Fire services and large water services 3" and larger shall be installed by applicant's contractor

possessing a Class A or C-34 license, per City water standards and inspected by approved Public Works inspection.

45. A Reduced Pressure Principle Device (RPPD) backflow prevention device shall be installed for meter protection. The landscape system shall also have RPPD device. Any carbonation dispensing equipment shall have a RPPD device. Installation shall be per City Standards and shall be tested by a certified backflow device tester immediately after installation. Cross-connection inspector shall be notified for inspection after the installation is completed. Property owner shall have RPPD device tested once a year thereafter by a certified backflow device tester and the test shall be submitted to Public Works, Water Services Division. Property owner must open a water account upon installation of RPPD device.
46. It shall be the responsibility of owner/developer to abandon any existing private water well(s) per Orange County Health Department requirements. Abandonment(s) shall be inspected by Orange County Health Department inspector after permits have been obtained.
47. A composite utility site plan shall be part of the water plan approval.
48. There shall be a minimum 15-foot clearance of building footings from the water main. Clearances less than 15 feet shall be reviewed and approved by Water Engineering.
49. The project site has an existing water main easement located on the east side of the property. There shall be no structures or utilities built on, or crossing, the water or sewer main easements.
50. New utilities shall have a minimum 5-foot horizontal and a minimum 1-foot vertical clearance from water main and appurtenances.
51. There shall be a minimum clearance from sewer main and water main of 10 feet from outside-of-pipe to outside-of-pipe.
52. Any new or existing water valve located within new concrete driveway or sidewalk construction shall be reconstructed per City Standard B-753.
53. City shall determine if existing water services(s) is/are usable and meets current City Standards. Any existing meter and service located within new driveway(s) shall be relocated at applicant's expense.
54. Fire service and private fire hydrant connections shall have above-ground backflow device with a double-check valve assembly per City standard plan B-773. Device(s) shall be tested immediately after installation and once a year thereafter by a certified backflow device tester and the results to be submitted to Public Works, Water Services Division. Device shall be on private property

and is the responsibility of the property owner. The above-ground assembly shall be screened from public view as required by the Planning Division.

55. No permanent structures, trees or deep-rooted plants shall be placed over sewer main or water main.
56. Location and number of fire hydrants shall be as required by Water Services Division and OCFA.
57. Commercial food use of any type shall require the installation of an approved grease interceptor prior to obtaining a business license. Plumbing plan for grease interceptor shall be routed to environmental services for review.
58. A properly sized grease interceptor shall be installed on the sewer lateral and maintained by the property owner. There shall be a separate sanitary waste line that will connect to the sewer lateral downstream of the grease interceptor. All other waste lines shall be drained through the grease interceptor. Grease interceptor shall be located outside of the building and accessible for routine maintenance. Owner shall maintain comprehensive grease interceptor maintenance records and shall make them available to the City of Garden Grove upon demand.
59. Food grinders (garbage disposal devices) are prohibited per Ordinance 6 of the Garden Grove Sanitary District Code of Regulations. Existing units are to be removed.
60. Applicant shall install new private sewer lateral with clean out at right-of-way line. Lateral in public right-of-way shall be 6" minimum diameter, extra strength VCP with wedgelock joints.
61. Should the water main be exposed during installation of the new sewer lateral, section of water main will be required to be removed and replaced with 20-foot section of PVC C-900 DR14 Class 305, centered at the crossing and size in kind.
62. Should the new sewer lateral be proposed to cross over the water main, a variance from the State Water Resources Control Board (SWRCB) is required.
63. Applicant shall abandon all existing sewer laterals. The 8" sewer main on Garden Grove Blvd directly in front of the property shall also be abandoned in its entirety up to the manhole aligning with the easterly property line.

#### **Building and Safety Division**

64. The project shall comply with the requirements of the 2019 California Building Code (CBC), the California Green Building Code, and all California Model Codes, including, that the buildings shall be solar ready.



65. All works shall comply with the latest California (CA) Building Standards Code (CBC) at time of permit application.
66. The project shall comply with the allowable building areas/heights/number of stories per CBC Chapter 5. The project shall provide a complete analysis on the title sheet and clearly indicate any area/height/number of story increase proposed, and shall reference the Code Section(s) being used.
67. The project shall demonstrate that the allowable number of stories for group R-2 in Type V-A construction complies with per Table 504.4 of the CBC with the exception of Section 510.4.
68. The project shall, on the title sheet, clearly identify type of automatic fire sprinkler system that will be used for the project.
69. The building shall comply with the latest CA Green Building Standards. The project shall provide a complete cross reference of each item from the Green Code checklist to the plans and sheets where compliance can be found.
70. Markings and identifications of future Electric Vehicle (EV) spaces shall be as required by the Green Building Code and shall comply with CBC Section 11B-228.3.
71. All residential areas and common use areas shall comply with CBC Chapter 11A.
72. Multi-story dwelling units in a building with one or more elevators shall comply with CBC Section 1102A.3.2.
73. All public use areas shall comply with CBC Chapter 11B.
74. The project shall demonstrate that the occupied roof complies with CBC Section 503.1.4, and the occupancy of the roof is an occupancy that is permitted by Table 504.4 for the story immediately below the roof.
75. All roof decks shall meet all requirements of CBC Section 2304.11.4. The project shall provide complete construction details for the roof deck construction, including third party approval number.
76. Cross ventilations of roof deck shall be provided per CBC Section 2304.12.2.6
77. All egress and exiting shall meet all requirements of CBC Chapter 10.
78. Fire-rated constructions and opening and penetration protections shall comply with CBC Chapter 7.

79. A soil investigation report per CBC Chapter 18 shall be provided at time of permit application for building plan reviews.
80. Solar zone shall be provided per CA Building Energy Efficiency Standards.
81. Building Commissioning is required for non-residential buildings over 10,000 square feet per Energy Standard Section 120.8.
82. Building Commissioning when required, shall comply with all requirements of Section 120.8(a) through 120.8(i). At time of permit application, all required commissioning items shall be submit for review or on the plans, if not provided, the plans will be deemed incomplete and no further plan review will be provided until all commissioning items are submitted.

### **Fire Department**

83. The applicant shall comply with all applicable Orange County Fire Authority requirements, including, but not limited to the Fire Master Plan.

### **Planning Services Division**

84. This approval is for the construction of five-story, mixed-use development with 19,056 square feet of commercial lease space designated for 9,786 square feet of retail use and 9,270 square feet of medical use, and fifty-two (52) residential apartment units.
85. Only uses listed as permitted or conditionally permitted within the Land Use Chart, Table 9.18-1 of Section 9.18.020.030 of Title 9 of the City's Municipal Code for the GGMU-2 zone shall be permitted, and subject to the parking requirements of the Municipal Code.
86. Written notification shall be provided to all occupants and users that the surrounding area may be subject to levels of noise, dust, fumes, or other effects associated with commercial uses at higher levels than would be expected in strictly residential areas. State and federal health regulations notwithstanding, noise and other standards shall be those applicable to commercial properties in the applicable zone.
87. The applicant shall submit detailed plans, showing the proposed location of utilities and mechanical equipment to the Community and Economic Development Department, Planning Services Division for review and approval prior to submitting plans into the Building and Safety Division Plan Check process. The project shall also be subject to the following:
  - a. All on-site and off-site utilities (off-site refers to the areas within public right-of-way to the center line of the streets adjacent to the subject property), within the perimeter of the site and to the centerline of the

adjacent streets, shall be installed or relocated underground. All on-site and off-site utilities pertaining to the improvements proposed under this Site Plan and Tentative Parcel Map shall be installed or relocated underground.

- b. All above-ground utility equipment (e.g. electrical, gas, telephone, cable TV) shall not be located in any street setback, within the common areas, or any parking areas, and shall be screened to the satisfaction of the Community and Economic Development Director.
  - c. No roof-mounted mechanical equipment including, but not limited to dish antennas, shall be permitted unless a method of screening complementary to the architecture of the building is approved by the Community and Economic Development Department prior to the issuance of building permits. Screening shall block visibility of any roof-mounted mechanical equipment from view of public streets and surrounding properties.
  - d. All ground or wall-mounted mechanical equipment shall be screened from view from any place on or off the site.
  - e. No exterior piping, plumbing, or mechanical ductwork shall be permitted on any exterior façade and/or be visible from any public right-of-way or adjoining property. Roof rain gutters are permitted. The rain gutters shall follow the natural architectural lines of the building.
88. The applicant shall submit a complete and detailed landscaping plan with irrigation systems included for review and approval by the Community and Economic Development Department prior to the issuance of a building permit. Drought tolerant plantings are encouraged. The landscape plan shall include the type (both common and botanical names), size, location, and quantity of all proposed plant material. All proposed landscaping shall be planted prior to the finalization of the building permit. The plan shall be consistent with the landscape requirements set forth and/or incorporated in the Garden Grove Municipal Code. All landscape irrigation shall comply with the City's Landscape Ordinance, associated Water Efficiency Guidelines and all recent applicable revisions from the State of California on water conservation measures shall be to the landscape plans. The landscape plan is also subject to the following:
- a. A complete, permanent, automatic remote control irrigation system shall be provided for all landscaping areas shown on the plan. Subsurface systems are encouraged. The irrigation plan for any new trees shall have a deep-water irrigation system that shall be specified on the landscape plan. A detail of the deep-water irrigation system shall be provided for review. If sprinklers are used, they shall be low flow/precipitation sprinkler heads for water conservation.

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- b. The plan shall provide a mixture of a minimum of ten percent (10%) of the trees at 48-inch box, ten percent (10%) of the trees at 36-inch box, fifteen percent (15%) of the trees at 24-inch box, and sixty percent (60%) of the trees at 15-gallon, the remaining five percent (5%) may be of any size. These trees shall be incorporated into the landscaped frontages of all streets, and within the outdoor recreation areas. Where clinging vines are considered for covering walls, Boston Ivy shall be used.
- c. Clinging vines shall be installed within the landscape planters along the perimeter block walls to deter graffiti.
- d. Trees planted within 10-feet of any public right-of-way shall be planted in a root barrier shield. All landscaping along street frontages adjacent to driveways shall be of the low height variety to ensure safe sight clearance.
- e. Landscaping along Garden Grove Boulevard and Brookhurst Street shall comply with the landscape requirements of the Garden Grove Mixed Use Zones. Off-site landscaping shall include planters with canopy trees spaced 30'-0" apart on center with an under planting of shrubs and flowering ground cover. Plant materials within the public rights-of-ways shall be determined by the City's Public Work's Department. On-site landscaping shall include both columnar and canopy trees. Columnar trees (minimum height at maturity of 45'-0") shall be planted within 10'-0 of the public right-of-way and shall be placed at regular intervals and no more than 40'-0" on center. On-site canopy trees shall be planted at a ratio of at least one tree for every 50'-0" of the Garden Grove Boulevard street frontage. The on-site front yard landscape area shall also include shrubs and flowering ground covers. All on and off-site canopy trees shall be a minimum size of 24-inch box. Columnar trees shall be a minimum of 25'-0" tall. Should palm trees be proposed, the brown trunk height of the palm trees shall be 25'-0".
- f. All landscape areas, including the areas located within the public rights-of-ways along Garden Grove Boulevard and Brookhurst Street that abuts the subject property, are the responsibility of the applicant/property owner(s).
- g. Landscaping within the roof deck gardens shall incorporate artificial turf, and planters with canopy trees, shrubs, and flowering plants.
- h. The landscape plan shall incorporate and maintain for the life of the project those means and methods to address water run-off also identified as Low Impact Development provisions, which address water run-off. This is to also to be inclusive of any application of Water Quality Management Plans (WQMP), Drainage Area Management Plans (DAMP)

- and any other water conservation measures applicable to this type of development.
- i. At the time of irrigation installation, the irrigation system shall comply with all applicable provisions of the City's Water Conservation Ordinance, the City's Municipal Code landscape provisions, and all applicable state regulations.
  - j. All above-ground utilities (e.g. water backflow devices, electrical transformers, irrigation equipment) shall be shown on the landscaping plan in order to ensure proper landscape screening will be provided.
89. The common recreation area improvements shall be reviewed and approved by the Community and Economic Development Department, Planning Services Division prior to issuance of building permits. The improvements to the outdoor roof gardens shall include at a minimum, a raised wood deck with a fire pit, synthetic grass, benches, and landscaping in Roof Garden No. 1; outdoor furniture, including tables, chairs, patio and lounge furniture, barbeque grills, benches, synthetic turf, fiberglass planters, and landscaping in Roof Garden No. 2; and landscaping and benches in Roof Gardens No. 3 and No. 4. All interior recreation areas shall incorporate the appropriate indoor furniture and amenities for the proposed use of the room. The indoor gyms shall incorporate exercise equipment or similar equipment, and the lounge areas shall be furnished with chairs, sofas, and coffee tables or other similar furniture. Lighting in the common recreation areas shall be provided at a maximum one-foot light candle during the hours of darkness, and shall be restricted to low decorative type wall-mounted lights or ground lighting systems.
90. The garden plaza shall incorporate amenities to encourage the pedestrian use, and shall incorporate landscaping, seating, public art and other amenities as required by the Mixed Use code for Boulevard Garden Plazas, including the amenities proposed on the approved plans, such as step seating and freestanding LED lighting.
91. Enhanced concrete treatment shall be provided within the front 20-feet for each of the driveways along Garden Grove Boulevard and Brookhurst Street, subject to the Community and Economic Development Department, Planning Services Division's approval. Such enhanced concrete treatment may include decorative stamped concrete or interlocking pavers, or other enhanced treatment, excluding scored and/or colored concrete. The final design and configuration shall be shown on the final site plan, grading plan, and landscape plans.
92. All lighting structures shall be placed so as to confine direct rays to the subject property. All exterior lights shall be reviewed and approved by the City's Planning Division. Lighting adjacent to residential properties shall be restricted

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to low decorative type wall-mounted lights, or a ground lighting system. Lighting shall be provided throughout all drive aisles, the parking structure, and entrances to the development per City standards for on-site lighting. Lighting in the common areas shall be directed, positioned, or shielded in such manner so as not to unreasonably illuminate the window area of nearby residences. The applicant shall submit a light plan (photometric plan) to Planning Services Division for review.

93. Hours and days of construction and grading shall be as set forth in Chapter 8.47 of the City of Garden Grove Municipal Code, except as follows:
- a. Monday through Saturday - not before 7 a.m. and not after 8 p.m. (of the same day).
  - b. Sunday and Federal Holidays - may work same hours, but subject to noise restrictions as established in Chapter 8.47 of the Municipal Code.

Construction activities shall adhere to SCAQMD Rule 403 (Fugitive Dust), which includes dust minimization measures, the use of electricity from power poles rather than diesel or gasoline powered generators, the use of methanol, natural gas, propane, or butane vehicles instead of gasoline or diesel powered equipment, where feasible, the use of solar or low-emission water heaters, and the use of low-sodium parking lot lights, to ensure compliance with Title 24.

94. During construction, if paleontological or archaeological resources are found, all attempts will be made to preserve in place or leave in an undisturbed state in compliance with applicable law. In the event that fossil specimens or cultural resources are encountered on the site during construction and cannot be preserved in place, the applicant shall contact and retain, at applicant's expense, a qualified paleontologist or archaeologist, as applicable, acceptable to the City to evaluate and determine appropriate treatment for the specimen or resource, and work in the vicinity of the discovery shall halt until appropriate assessment and treatment of the specimen or resource is determined by the paleontologist or archeologist (work can continue elsewhere on the project site). Any mitigation, monitoring, collection, and specimen/resource treatment measures recommended by the paleontologist/archaeologist shall be implemented by the applicant at its own cost.
95. The applicant shall comply with the Migratory Bird Treaty Act (MBTA), and Sections 3503, 3503.5 and 3515 of the California Fish and Game Code, which require the protection of active nests of all bird species, prior to the removal of any on-site landscaping, including the removal of existing trees.
96. All signage comply with Chapter 20 of Title 9 of the City's Municipal Code. A sign program shall be established for the development prior to Certificate of Occupancy.

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97. Prior to the finalization of working drawings for Planning Services Division, Engineering Division, and Building and Safety Division Plan Check, the applicant shall submit to the Community and Economic Development Department detailed and dimensioned plot plans, floor plans, exterior elevations, and landscape plans which reflect the Conditions of Approval. The plans shall indicate cross-sections of all streets within the development, landscape materials, wall materials, and building materials proposed for the project.
98. Building color and material samples shall be submitted to the Planning Services Division for review and approval prior to issuance of building permits. The building shall incorporate all the architectural detailing identified on the approval plans. Specifically, the south and west building elevations shall incorporate a storefront system, chiseled limestone stone veneer, mounted horizontal metal louvers, and steel canopies over the entrance to several of the ground-level tenants of the commercial component, and chiseled limestone veneer, horizontal metal louvers, and a white smooth stucco finish on the upper elevation for the residential component.
99. The project shall provide a separate trash enclosure to serve the commercial component, and shall comply with the following:
  - a. The proposed trash enclosure shall be designed to comply with the City's B-502 trash enclosure standard, or with an alternative design approved by the Public Works Engineering Division.
  - b. The trash enclosure shall have unifying color and exterior finish that matches, and are integrated, with the proposed development. The proposed roof design of the trash enclosure shall be architecturally compatible with the design of the development. The Planning Services Division shall review and approve the design of the proposed roof and the material(s). The proposed roof and materials shall also comply with the building code requirements.
  - c. The trash bins shall be kept inside the trash enclosures, and gates closed at all times, except during disposal and pick-up. The property owner shall provide sufficient trash bins and pick-up to accommodate the site.
100. The project shall provide a separate trash room for the residential use with a trash shoot that serves each of the residential floors. The trash bins shall be kept inside the trash room at all times, except during disposal and pick-up. The property owner shall provide sufficient trash bins and pick-up to accommodate the site.
101. Prior to the issuance of grading permits, a temporary project identification sign shall be erected on the site in a secure and visible manner. The sign shall be conspicuously posted at the site and remain in place until occupancy of the

- project. The sign shall include the name and address of the development, and the developer's name, address, and a 24-hour emergency telephone number.
102. Litter shall be removed daily from the project site, including adjacent public sidewalks and all parking areas under the control of the applicant. The areas shall be swept or cleaned, either mechanically or manually, on a weekly basis, to control debris.
  103. The applicant shall abate all graffiti vandalism within the premises. The applicant shall implement best management practices to prevent and abate graffiti vandalism within the premises throughout the life of the project, including, but not limited to, timely removal of all graffiti, the use of graffiti resistant coatings and surfaces, the installation of vegetation screening of frequent graffiti sites, and the installation of signage, lighting, and/or security cameras, as necessary. Graffiti shall be removed/eliminated by the applicant as soon as reasonably possible after it is discovered, but not later than 72 hours after discovery.
  104. There shall be no deliveries from or to the premises before 7:00 a.m. and after 10:00 p.m., seven days a week.
  105. All new block walls, and/or retaining wall(s), including existing block walls to remain, shall be shown on the grading plans. Cross sections shall show vertical and horizontal relations of improvements and property line. Block walls shall be designed in accordance to City standards or designed by a professional registered engineer, and shall be measured from on-site finished grade. The project shall also comply with the following:
    - a. The project shall maintain perimeter block walls with a minimum height 6 feet to a maximum height of 7 feet, as measured from highest point of the on-site finished grade along the east and north property lines.
    - b. If new block walls are proposed, the new block walls shall be constructed of decorative split-face masonry with decorative caps, subject to the Community and Economic Development Department's approval.
    - c. All existing block walls that will remain shall be modified, as necessary, to comply with the minimum block wall height requirement. The type, texture, and color of the block wall shall match any existing block wall that will remain.
    - d. All block walls shall comply with the requirements of the Chapter 9.18 of the Municipal Code. Where allowed, no walls greater than 36-inches in height shall be construction with the driveway vision clearance area of the project's entrance.



- e. The applicant shall work with the existing adjacent property owners along the project perimeter in designing and constructing the required perimeter block walls. This requirement is to avoid having double walls and to minimize any impact that it might cause to the existing landscaping on the neighbor's side as much as possible. The perimeter block wall shall be constructed and situated entirely within the subject property. In the event that the applicant cannot obtain approval from the adjacent property owners, the applicant shall construct the new wall with a decorative cap to be placed between the new and existing walls. In the event the location of a new wall adjacent to an existing wall or fence has the potential to affect the landscape planter, then the applicant shall work with City Staff to address this situation. The Community and Economic Development Director shall be authorized to approve minor alterations to the size and/or location of the landscape planter to accommodate the placement of such wall.
106. The applicant shall ensure that the project complies with the following requirements and stipulations for the life of the project:
- a. The proposed development will consist of the construction of 52 apartment units, and three (3) units shall be reserved for very low-income households.
  - b. The project shall designate, and clearly identify, the commercial and residential parking spaces. A total of 65 parking spaces shall be designated, clearly labeled, and made available to the residential units at all times. The designated residential parking spaces shall be located within the secured parking structure. During non-business hours, the commercial parking spaces may be utilized by the residents of the development.
  - c. The parking structure gates shall remain open during normal business hours when the commercial tenants are in operation. The parking structure gates shall close during non-business hours, and residents shall access the parking structure with a gate remote from a designated vehicular entrance.
  - d. All parking structure vehicular gates and the drive-aisle sliding gate shall be of automatic type.
  - e. There shall be no parking allowed along any drive-aisle, except within the designated parking areas. All curbs not designated as parking areas shall be painted red. The applicant shall post "No Parking" signs along the drive aisles.

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- f. Residents shall not park or store vehicles anywhere on the site except within the designated parking spaces; however, the commercial parking spaces may be used for overflow parking during non-business hours.
- g. Best Management Practices shall be implemented to deter and/or abate any graffiti vandalism that occurs on the project site, including, but not limited to, timely removal of all graffiti, the use of graffiti resistant coatings and surfaces, the installation of vegetation screening of frequent graffiti sites, the installation of signage, lighting, and/or security cameras, as necessary, and the installation and maintenance of clinging vines within the landscape planters along the perimeter block walls. Graffiti shall be removed/eliminated by the applicant as soon as reasonably possible after it is discovered, but not later than 72 hours after discovery.
- h. Each residence shall be utilized as one (1) dwelling unit. No portion of any residence shall be utilized or rented as a separate dwelling unit.
- i. All balconies shall remain open and shall not be enclosed at any time. There shall be no storage allowed in the balconies at any time.
- j. Parking and storage of boats, recreational vehicles, or commercial vehicles on the property is prohibited.
- k. The maintenance of the drive aisles, storm drains, sewer system, and open space areas is the responsibility of the applicant and property owner, including the common recreation area, and the common landscape areas.
- l. Each unit shall be provided with an air conditioning condensing unit and/or system so that there are no wall-mounted, or window mounted units. If units are located on the roof, an architectural design of the roof areas shall be done to effectively screen such units from adjacent properties and the public right-of-way.
- m. Mailboxes shall be provided and installed by the applicant. The local postmaster shall approve the design and location.
- n. Each unit shall have phone jacks and cable-TV outlets in all rooms, except in the hallways and bathrooms.
- o. Each unit shall be provided with washer and dryer hook-ups.
- p. Each unit shall provide a separate storage area of 70.83 cubic feet for each 2-bedroom unit, and 58.5 cubic feet for each 1-bedroom unit.

Site Plan No. SP-111-2022 and Tentative Parcel Map No. PM-2021-190  
Conditions of Approval

107. Final Tentative Parcel Map No. PM-2021-190 shall be approved by the City and recorded by the applicant prior to issuance of building permits for the proposed mixed-use development.
108. The applicant/property owner shall submit signed letters acknowledging receipt of the decision approving Site Plan No. SP-111-2022 and Tentative Parcel Map No. PM-2021-190, and his/her/its agreement with all conditions of approval.
109. The applicant shall, as a condition of project approval, at its sole expense, defend, indemnify and hold harmless the City, its officers, employees, agents and consultants from any claim, action, or proceeding against the City, its officers, agents, employees and/or consultants, which action seeks to set aside, void, annul or otherwise challenge any approval by the City Council, Planning Commission, or other City decision-making body, or City staff action concerning Site Plan No. SP-111-2022 and Tentative Parcel Map No. PM-2021-190. The applicant shall pay the City's defense costs, including attorney fees and all other litigation related expenses, and shall reimburse the City for court costs, which the City may be required to pay as a result of such defense. The applicant shall further pay any adverse financial award, which may issue against the City including, but not limited, to any award of attorney fees to a party challenging such project approval. The City shall retain the right to select its counsel of choice in any action referred to herein.
110. In accordance with Garden Grove Municipal Code Sections 9.32.160 and 9.40.070.A, respectively, the rights granted pursuant to Site Plan No. SP-111-2022 and Tentative Parcel Map No. PM-2021-190 shall be valid for a period of two years from the effective date of this approval. Unless a time extension is granted pursuant to Section 9.32.030.D.9 of the Municipal Code, the rights conferred by Site Plan No. SP-111-2022 shall become null and void if the subject development and construction necessary and incidental thereto is not commenced within two (2) years of the expiration of the appeal period and thereafter diligently advanced until completion of the project. In the event construction of the project is commenced but not diligently advanced until completion, the rights granted pursuant to Site Plan No. SP-111-2022 and Tentative Parcel Map No. PM-2021-190 shall expire if the building permits for the project expire.
111. The Conditions of Approval set forth herein include certain development impact fees and other exactions. Pursuant to Government Code §66020(d), these Conditions of Approval constitute written notice of the amount of such fees. The applicant is hereby notified that the 90-day protest period, commencing from the effective date of approval of Site Plan No. SP-111-2022 and Tentative Parcel Map No. PM-2021-190, has begun.

112. At applicant's request, applicant has been granted a density bonus of seven (7) units, one (1) incentive, and three (3) waivers in exchange for the applicant's agreement to reserve three (3) dwelling units in the project for occupancy by very low-income households for a period of 55 years commencing with the issuance of the certificate of occupancy for the project. The applicant shall at all times during the term of the affordability period comply with the requirement to rent the three (3) target units to very low-income households at an affordable rent as required by the Garden Grove Municipal Code and State Law. Landlords receive fair market rent from tenants who are recipients of subsidies under Section 8 of the U.S. Housing Act of 1937, which do not qualify as affordable rent for purposes of the three (3) target units set aside for low-income households. Pursuant to State law, the Garden Grove Municipal Code, and the City's Density Bonus Agreement Guidelines, the record owner or owners of the subject property shall enter into a Density Bonus Housing Agreement with the City in a form approved by the City Attorney. The Density Bonus Housing Agreement shall be prepared by the City at the applicant/owner's expense. The Density Bonus Housing Agreement shall be approved by the City and recorded prior to issuance of a building permit for any structure in the housing development. The Density Bonus Housing Agreement shall run with the land and bind on all future owners and successors in interest.

**Exhibit "A"**

**Density Bonus Application**



**CITY OF GARDEN GROVE  
 PLANNING SERVICES DIVISION  
 11222 ACACIA PARKWAY  
 GARDEN GROVE, CA 92840  
 TEL: (714) 741-5312 FAX: (714) 741-5578  
 ggcity.org**

**Density Bonus Application  
 (Government Code §65915 et seq.)**

*Housing development project applicants intending to request a density bonus, incentives or concessions, modifications or waivers, and/or reduced parking pursuant to the Section 65915 et seq. of the California Government (Density Bonuses and Other Incentives) must complete the following application. For additional information regarding density bonuses and affordability agreements, please refer to Section 9.12.030.070 of the Garden Grove Municipal Code, and to the Garden Grove Density Bonus Agreement Guidelines.*

Date Filed: \_\_\_\_\_

<b>DENSITY BONUS TYPE</b>	
<i>Please check one of the following (as proposed at the time of application submittal):</i>	
<input type="checkbox"/>	100% of all units in the development, including Total Units and density bonus units, but exclusive of a manager's unit or units, are for low income households, as defined by Section 50079.5 of the Health and Safety Code, except that up to 20 percent of the units in the development, including Total Units and density bonus units, may be for moderate income households, as defined in Section 50053 of the Health and Safety Code.
<input checked="" type="checkbox"/>	At least 5% of the Total Units for very low income households, as defined in Section 50105 of the California Health and Safety Code.
<input type="checkbox"/>	At least 10% of the Total Units for lower income households, as defined in Section 50079.5 of the California Health and Safety Code.
<input type="checkbox"/>	At least 10% of the Total Units for moderate income households, as defined in Section 50093 of the California Health and Safety Code (common interest development offered to the public for purchase unless on-site option for Impact Fees, see 15.72.100.B.4).
<input type="checkbox"/>	A senior citizen housing development, as defined in Sections 51.3 and 51.12 of the California Civil Code.
<input type="checkbox"/>	At least 10% of the Total Units for transitional foster youth, as defined in California Education Code section 66025.9 (very low income households as defined in Section 50105 of the California Health and Safety Code).
<input type="checkbox"/>	At least 10% of the Total Units for disabled veterans, as defined in California Government Code Section 18541 (very low income households as defined in Section 50105 of the California Health and Safety Code).
<input type="checkbox"/>	At least 10% of the Total Units for homeless persons, as defined in the federal McKinney-Vento Homeless Assistance Act (42 U.S.C. Sec. 11301 et seq.) (very low income households, as defined in Section 50105 of the California Health and Safety Code).
<input type="checkbox"/>	At least 20% of the Total Units for lower income students in a student housing development (that satisfies the requirements of California Government Code Section 65915(b)(1)(F)).
<input type="checkbox"/>	Land donation (at least one acre in size, or of sufficient size to permit development of at least 40 units and otherwise satisfies the requirements of California Government Code Section 65915(g).)
<input type="checkbox"/>	Child care facility (that satisfies the requirements of California Government Code Subsection 65915(h)).
<input type="checkbox"/>	Condominium Conversion (that satisfies the requirements of California Government Code 65915.5)).
<b>PRIMARY CONTACT INFORMATION</b>	
Name: Source Architecture, Paul Kim	
Contact Type: <input checked="" type="checkbox"/> Architect <input type="checkbox"/> Engineer <input type="checkbox"/> Property Owner <input type="checkbox"/> Representative <input type="checkbox"/> Other	
Mailing Address: 6101 Ball Road, STE #205	
City, State, Zip Code: Cypress, CA 90630	
Phone No.: (714)723-0419	
E-mail: pkim@source-architecture.com	
<b>PROPERTY OWNER CONTACT INFORMATION</b> (If different than Primary Contact)	
Name: Mr. Michael Dao	
Mailing Address: 9191 Westminster Blvd.	
City, State, Zip Code: Garden Grove, CA	
Phone No.: (714)504-2950	
E-mail: michaelmdao3kids@yahoo.com	

<b>PROJECT INFORMATION:</b>		
Project Address: 10201 & 10231 Garden Grove Blvd., Garden Grove, CA 92843-1041		
APN(s): 089-072-53 and 089-072-66		
Zoning & General Plan Land Use: GGMU-2		
Maximum Allowable Residential Density (before density bonus): 24 units/acre		
Total Base Number of Housing Units (before density bonus): 1.86 acre x 24 units/acre= 44.64 = 45 units		
Market Rate Base Housing Units (before density bonus): 45 units		
Affordable Base Housing Units (before density bonus): 0 units		
Size of Market Rate Units (# of Studios, 1 bedroom, 2 bedroom, etc.): 0 Studios, 24 one-bedroom, 24 two-bedroom units		
Size of Affordable Units (# of Studios, 1 bedroom, 2 bedroom, etc.): 0 studios, 2 one-bedroom, 2 two-bedroom units		
Proposed number of Very Low Income units : (5%) = 3 units		
Proposed number of Low Income units : 0		
Proposed number Moderate Income units : 0		
Percentage of Total Base Housing Units that are Affordable: 3/45 x 100% = 6.67%		
Maximum Density Bonus Percentage (See chart on page 4): 22.50%		
Number of Required Parking Spaces: 65 stalls		
Number of Parking Spaces Provided: 65 stalls		
Residential Tenure: Does the project propose rental or ownership units? No ownership. All rental apartment.		
<b>DENSITY BONUS REQUEST</b>		
Density Bonus Percentage (calculate using "Density Bonus Chart"): 22.50%		
Total Number of Density Bonus Units: 3		
Total Units in Development After Density Bonus is Applied: 52 units		
<i>If requesting a Density Bonus for the following project types, please check the appropriate box and provide the following information:</i>		
<input type="checkbox"/>	Land Donation	Address (or APN) of land to be dedicated:  Attach proof of site control. Attach evidence of meeting conditions for a land transfer density bonus as specified in the State Housing Density Bonuses and Incentives Law
<input type="checkbox"/>	Child-Care Facility	Address and APN of child-care facility:  Square footage of facility:  Attach evidence of meeting conditions for a child care facility density bonus or Incentive as specified in the State Housing Density Bonuses and Incentives Law.
<input type="checkbox"/>	Condominium Conversion	Attach evidence of meeting conditions for a condominium conversion Density Bonus as specified in the State Housing Density Bonuses and Incentives Law.

**INCENTIVES/CONCESSIONS REQUEST**

An applicant for a density bonus may also propose specific incentives/concessions pursuant to Subsection (d) of Government Code Section 65915. The number of incentives/concessions an applicant may receive is based on the number of affordable units and level of affordability provided. Use the Incentives/Concessions Calculator below to determine the number of incentives or concessions you are eligible for.

**INCENTIVES/CONCESSIONS CALCULATOR**

Affordability Level	Restricted Affordable Units Provided in Project	% of Base Project	Threshold for one (1) Incentive/Concession (# of units)		Threshold for two (2) Incentives/Concessions (# of units)		Threshold for three (3) Incentives/Concessions (# of units)		Threshold for four (4) Incentives/Concessions* (# of units)	
Very Low Income	3 units	6.67%	5%	X	10%		15%		100% affordable with ≥80% low income, ≤20% moderate	
Low Income			10%		17%		24%			
Moderate Income			10%		20%		30%			

\* If a 100% affordable project is located within 1/2 mile of a major transit stop, the project is eligible for a height increase of up to three (3) additional stories, or thirty-three feet (33'-0"); however, if the project also seeks a waiver from any maximum controls on density, the project cannot receive a waiver of any other development standards (but can still receive four incentives). If this allowance is sought, please describe/identify the major transit stop that is within 1/2 mile of the qualifying 100% affordable project:

**DESCRIPTION OF INCENTIVES/CONCESSIONS REQUESTED**

List all requested incentives/concessions. If a reduction in site development standards or a modification of zoning code requirements is sought, include references to specific Municipal Code Sections in question, and reference the requested incentives/concessions on the submitted plans.

1. Balcony area: Deviate from Section 9.18.110.030.F.1, code requires minimum balcony size, and square footage of 9 ft and 90 sq.ft. Project proposed balcony sizes and dimensions are 59.97 sq.ft and 44.71 sq.ft with minimum dimensions of 5'-8" and 5'-10" respectively.

Provide evidence substantiating the applicant's eligibility for each incentive/concession requested, including information that clearly demonstrates that the requested incentive/concession will result in identifiable and actual cost reductions to provide for affordable housing costs. The Applicant may attach additional documentation as required.

1. Balcony size - Required: 90 sf x 52 units = 4,680 sf  
 Proposed: 1 Bedrm 59.97 sf x 26 units = 1,559.22 sf  
 2 Bedrm 44.35 sf x 26 units = 1,153 sf  
 Total = 1,559.22 sf + 1,153.1 sf = 2,712.32 sf  
 Saved: 4,680 sf - 2,712.32 sf = 1,967.68 sf x \$300/sf = \$590,304  
 The proposed size for total balcony area of the overall building is 2,712.32 sf. This saves the cost of \$590,304 which is calculated at \$300 per square foot for 1,967.68 sf area saved for required balcony space.



**MODIFICATION/WAIVER REQUEST**

*Pursuant to Subsection (e) of Government Code Section 65915, an applicant may also propose the waiver or reduction of development standards that have the effect of physically precluding the construction of a housing development incorporating the density bonus and any incentives or concessions granted to the applicant.*

**DESCRIPTION OF MODIFICATIONS/WAIVERS REQUESTED**

*List all development standards for which you are seeking a waiver or reduction pursuant to Subsection (e) of Government Code Section 65915. Include references to specific Municipal Code Sections in question, and reference development standards to be modified or waived on the submitted plans.*

- 1. Recreation Area: Deviate from Section 9.18.110.030.F.2, code requires active recreation areas not to be less than 20 feet dimensions. The recreation area for the lounge and Roof Garden No. 1 and 2 include portions that are not 20 feet in one direction, and the dimensions are less than 20 feet in single or multiple directions. Also, code requires indoor or roof decks recreation areas not to contribute more than 50% of the required open space, recreation, and leisure areas. The proposed project will provide 100% of the required active areas as a combination of indoor and roof deck design.
- 2. Building Height: Deviate from Section 9.148.090.020, code requires maximum building height of 50 feet or 4 stories. The project proposes 5-stories with an overall building height of 54'-8" to roof, and 60'-2" to parapet. The overall building height is 67'-8" to accommodate elevator shaft above the 54'-8" allowance.
- 3. Storage Facilities: Deviate from Section 9.18.110.030.H.1, code requires each unit to provide minimum 300 cubic feet of storage per unit. The project proposes 70.83 cubic feet and 58.8 cubic feet per unit.

*Provide evidence substantiating the applicant's eligibility for each waiver or reduction of a development standard being requested, including documentation demonstrating that the waiver or reduction is physically necessary to construct the housing development with the additional density allowed pursuant to the density bonus and incorporating any incentives or concessions required to be granted. Where more than one modification or waiver is sought, the applicant should clearly demonstrate why the modifications/waivers are cumulatively necessary to prevent a development standard from physically precluding the construction of the development.*

- 1. Recreation areas are proposed at 4th floor deck & indoors so that the project building can have 48,060 sf of building footprint at the ground level to provide parking structure with 175 stalls for commercial and residential development.
- 2. The building is proposed 5 stories at 58' to provide 3 levels parking structure (175 stalls), and 2 story residential use (26 one-bedroom units and 26 two-bedroom units).
- 3. Storage - To provide for more room with adequate square footages in the living units, the proposed area for the storage saves 12,229.62cf and the cost of \$207,903.54 for the overall building.
 

Required :	300 cf x 52 units = 15,600 cf
Proposed:	1 Bedrm: 58.8 cf x 26 units = 1,528.8 cf
	2 Bedrm; 70.83 cf x 26 units = 1,841.58 cf
Saved:	15,600 cf - (1,528.8 cf + 1,841.58 cf) = 3,370.38) = 12,229.62 cf
	\$12,229.62 x \$17/cf = \$207,903.54

<b>PARKING RATIOS</b>	
Are you requesting application of the onsite vehicular parking ratios set forth in Subsection (p)(1) of Government Code Section 65915?	
<input type="checkbox"/> Yes	<input type="checkbox"/> No
<b>SPECIAL PARKING REQUIREMENTS</b>	
If you are requesting application of a reduced onsite parking ratio pursuant to Subsections (p)(2), (p)(3), or (p)(4) of Government Code Section 65915, select the onsite parking standard requested per the appropriate development type:	
<input type="checkbox"/>	Rental/for sale projects with at least 11% very low income or 20% lower income units, within 1/2 mile of accessible major transit stop** - 0.5 spaces per unit
<input type="checkbox"/>	Rental projects 100% affordable to lower income, within 1/2 mile of accessible major transit stop** - 0 spaces per unit
<input type="checkbox"/>	Rental senior projects 100% affordable to lower income, either with paratransit service or within 1/2 half mile of accessible bus route** (operating ≥8 times per day) - 0 spaces per unit
<input type="checkbox"/>	Rental special needs projects 100% affordable to lower income households, either with paratransit service or within 1/2 half mile of accessible bus route** (operating ≥8 times per day) - 0 spaces per unit
<input type="checkbox"/>	Rental supportive housing developments 100% affordable to lower income households - 0 spaces
** If applicable, please describe/identify the major transit stop or accessible bus route that is within 1/2 mile of the project.	
<b>ASSOCIATED HOUSING DEVELOPMENT FORMS &amp; APPLICATIONS</b>	
Dependent upon the nature of the request, and the design of the project, the following forms may also be required:	
<input type="checkbox"/> Replacement Unit Determination	<input type="checkbox"/> SB 330 Housing Development Pre-Application
<input type="checkbox"/> SB 35 Housing Streamlining Eligibility Checklist	<input type="checkbox"/> Preliminary Development Review Application

**CERTIFICATION:**

I certify and declare under penalty of perjury under the laws of the State of California that the answers furnished above, and in any attached exhibits, and that the facts, statements, and information presented are true and correct to the best of my knowledge and belief. I further understand that additional information may be required by the City of Garden Grove to complete my review. Furthermore, developments requesting a density bonus shall enter into a density bonus housing agreement with the City. A density bonus housing agreement shall be made a condition of the discretionary planning permits for all housing developments, and shall be recorded as a restriction on any parcels on which the target units or density bonus units will be constructed. The density bonus housing agreement shall be recorded prior to final or parcel map approval, or, where the housing development does not include a map, prior to issuance of a building permit for any structure in the housing development. The density bonus housing agreement shall run with the land and bind on all future owners and successors in interest.

Michael M. [Signature]  
Applicant Signature

3/15/2022  
Date

Michael M. [Signature]  
Property Owner Signature

3/15/2022  
Date

**Exhibit "B"**

**Technical Studies**

Traffic Impact Study

Noise Impact Study

Air Quality and GHG Impact Analysis



**K2 TRAFFIC ENGINEERING, Inc.**  
Traffic Control . Signal . Synchronization . Parking . Study

# TRAFFIC IMPACT STUDY

---

Mixed-Use Development

10201 & 10231 Garden Grove Boulevard

In the City of Garden Grove

Date: March 18, 2021

*Prepared For:*

**AMD Medical Plaza**

9191 Westminster Blvd, Ste 205

Garden Grove, CA 92844

*Prepared By:*

**K2 Traffic Engineering, Inc.**

1442 Irvine Blvd, Suite 210

Tustin, CA 92780

(714) 832-2116

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Traffic Impact Study for Mixed-Use Development  
10201 & 10231 Garden Grove Boulevard  
In the City of Garden Grove



Prepared under the supervision of

A handwritten signature in black ink, appearing to read "Jende Kay Hsu".

Jende Kay Hsu, P.E., T. E.

Lic. # T2285



## **EXECUTIVE SUMMARY**

The purpose of this study is to evaluate traffic impact of the proposed mixed-use development located at 10201 & 10231 Garden Grove Boulevard in the City of Garden Grove. The project site is currently vacant. The proposed mixed-use development includes a new 52-unit apartment, 9,927-square-foot medical offices and 9,004-square-foot retail space.

In accordance with Senate Bill (SB) 743 and the new California Environmental Quality Act (CEQA) requirements, "*City of Garden Grove Traffic Impact Analysis Guidelines for Vehicle Miles Traveled and Level of Service Assessment, dated May 2020*" has identified Vehicle Miles Traveled (VMT) as the preferred metric to assess transportation impacts. The project meets the Low VMT Area Screening and may be presumed to have a less than significant VMT impact. Subject to final determination by City Traffic Engineer, the complete project-level VMT analysis and forecasting through regional model is not required.

The City guidelines further define the applicable general plan consistency requirements related to Level of Service (LOS) for analyzing the potential transportation impacts of proposed development projects. With pass-by consideration, the project is expected to have a NET trip generation of 54 trips in the AM peak hour, including 30 inbound and 24 outbound trips, 77 trips in the PM peak hour, including 35 inbound and 42 outbound trips, and 959 daily trips. The project does not generate any significant impact and mitigation measure is not required.

The project provides two access driveways: one on Garden Grove Boulevard and the other on Brookhurst Street. With the presence of an existing raised median, the driveway on Garden Grove Boulevard is for right-in-right-out only. The driveway on Brookhurst Street currently faces an existing median opening. However, the median opening on Brookhurst Street will be closed off as part of the Brookhurst Triangle Development on the west side of Brookhurst Street. Therefore, the driveway on

Brookhurst Street will also become right-in-right-out only. In the event that the Brookhurst Triangle Development has not closed off the southbound left turn at the subject driveway prior to project completion, the project will be required to develop a plan and construct accordingly to restripe and add delineators to prevent left turns in and out from Brookhurst Street.

Both driveways provide normal access to the parking structure for retail, office, and residential uses during business hours. All entrance gates will be closed at night prohibiting non-residential access. The only access point for residents at night times will be through the south gate from Garden Grove Boulevard which is controlled by coded cards and intercom and provided with a turn-around area in front of the gate. The rollup gate at Brookhurst Street will allow sensor-activated exit only. An internally illuminated "No Entry" sign will turn on at night and access from this entrance is prohibited after business hours. In addition, a turn-around area is provided for vehicles inadvertently turning into the driveway on Brookhurst Street. It is necessary that the height of shrubs, planting, and other visual obstructions be limited to a maximum height of thirty inches to maintain sufficient corner sight distance at the driveway.

Stacking distances from the gate to the driveway are approximately 180 feet from Garden Grove Boulevard and 60 feet from Brookhurst Street. Estimated stacking capacities are eight (8) cars from Garden Grove Boulevard and three (3) cars from Brookhurst Street. Both access gates provide adequate stacking distance to effectively avoid excessive queuing backing up onto public streets.

The proposed development does not plan to make any change to the existing public transit, bicycle, or pedestrian facilities. The project does not result in any conflict with adopted policies, plans, or programs regarding public transit, bicycle, or pedestrian facilities, or otherwise decreases the performance or safety of such facilities. The project is not anticipated to have a significant impact to the active transportation and public transit system.

## INTRODUCTION

The purpose of this study is to evaluate traffic impact of the proposed mixed-use development located at 10201 & 10231 Garden Grove Boulevard in the City of Garden Grove. Vicinity map is shown in **Exhibit 1**.

The project site is currently vacant. The proposed mixed-use development includes a new 52-unit apartment, 9,927-square-foot medical offices and 9,004-square-foot retail space. The proposed site plan is shown in **Exhibit 2**.



**EXHIBIT 1. VICINTY MAP**  
No Scale

10000 17th Street, Suite 100  
San Diego, CA 92161  
Tel: 619.594.1100  
www.elcitysource.com

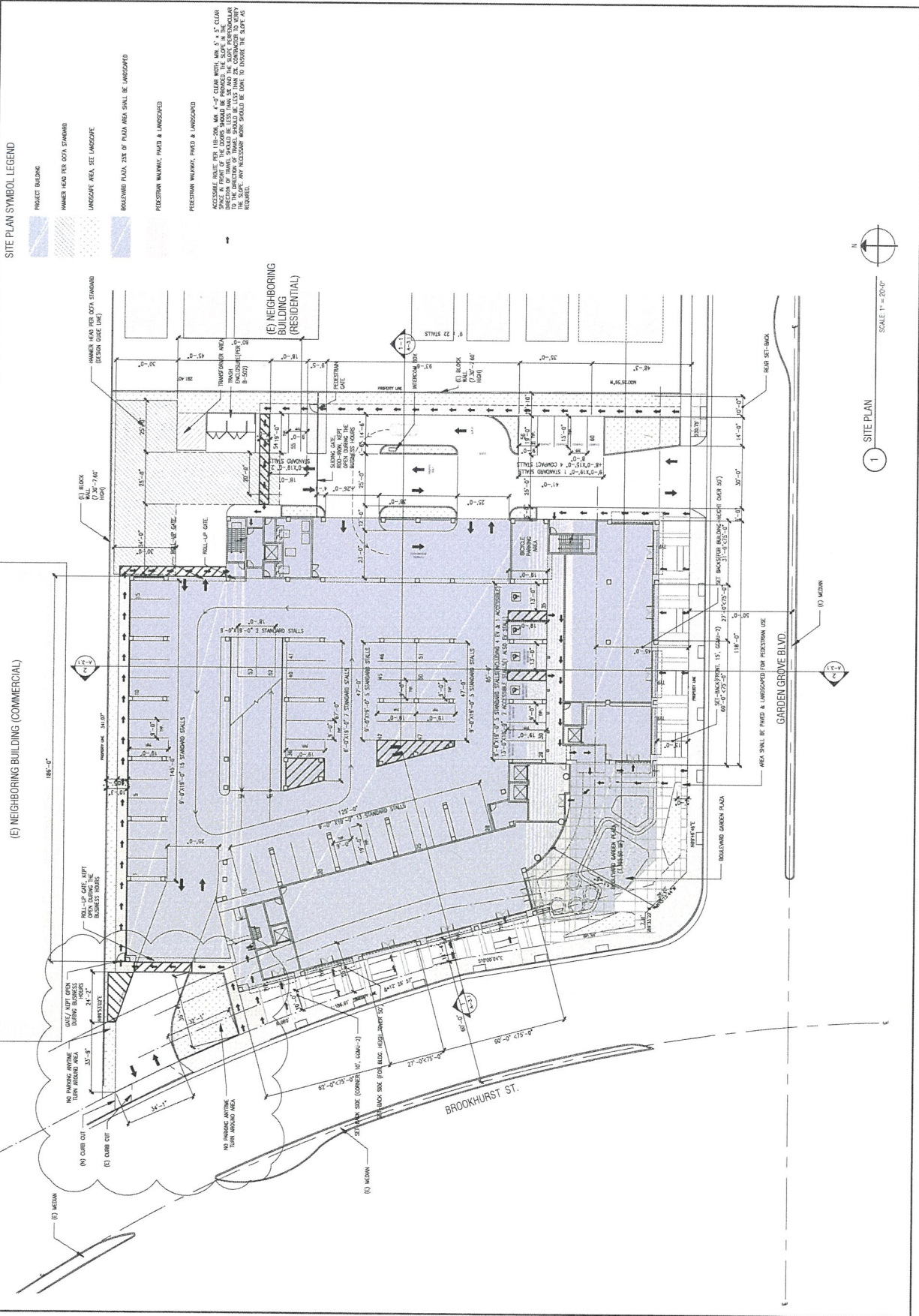
THIS PLAN IS THE PROPERTY OF SOURCE ARCHITECTURE. IT IS TO BE USED ONLY FOR THE PROJECT AND SITE SPECIFICALLY IDENTIFIED HEREON. NO PART OF THIS PLAN IS TO BE REPRODUCED, COPIED, OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, INCLUDING PHOTOCOPYING, RECORDING, OR BY ANY INFORMATION STORAGE AND RETRIEVAL SYSTEM, WITHOUT THE WRITTEN PERMISSION OF SOURCE ARCHITECTURE. ANY VIOLATION OF THESE TERMS SHALL BE CONSIDERED A BREACH OF CONTRACT AND SUBJECT TO LEGAL ACTION.

DATE: 09/27/2015 1ST REVISION  
DRAWN BY: [Name]  
CHECKED BY: [Name]  
APPROVED BY: [Name]

MIXED-USE BUILDING FOR DR. DAO  
10231 & 10231 GARDEN GROVE BLVD  
GARDEN GROVE, CA 92643-1041  
MR. MICHAEL DAO

OWNER: [Name]  
PROJECT: [Name]  
TITLE: SITE PLAN

SCALE: 1" = 20'-0"  
SHEET: A-1.1  
OF: [Number] SHEETS



**SITE PLAN SYMBOL LEGEND**

- PROJECT BUILDING
- HATCHED AREA FOR ADA STAIRWELL
- LANDSCAPE AREA, SEE LANDSCAPE
- BOULEVARD PAV. SEE CP PLAN. AREA SHALL BE LANDSCAPED
- PEDESTRIAN WALKWAY, PAVED & LANDSCAPED
- PEDESTRIAN WALKWAY, PAVED & LANDSCAPED

ACCESSIBLE ROUTE PER 119-206, MIN. 4'-0" CLEAR WIDTH, MIN. 5' x 4' CLEAR SPACE AT EACH END OF ROUTE. THE ROUTE SHALL BE 100% ADA COMPLIANT. THE LOCATION OF THE ROUTE SHALL BE IDENTIFIED ON THE PLAN. THE LOCATION OF THE ROUTE SHALL BE IDENTIFIED ON THE PLAN. THE LOCATION OF THE ROUTE SHALL BE IDENTIFIED ON THE PLAN.



SCALE: 1" = 20'-0"  
1 SITE PLAN

**EXHIBIT 2. SITE PLAN**

## STUDY SCENARIOS

According to the scoping agreement (see **Appendix A**), the following intersections are included in this study for level of service analysis to evaluate the potential traffic impacts:

1. Garden Grove Boulevard at Brookhurst Way/Kerry Street
2. Garden Grove Boulevard at Brookhurst Street
3. Garden Grove Boulevard at Gilbert St
4. Brookhurst Street at Stanford Avenue
5. Brookhurst Street at Trask Avenue

All study intersections are currently controlled by traffic signals. In addition, the study will include analysis for the stop-controlled driveways proposed by the project to ensure site access is maintained at acceptable level of services upon project completion.

In compliance with the 2017 Congestion Management Program (CMP), dated October 2017, established by the Orange County Transportation Authority (OCTA), and the scoping agreement, the following scenarios are included in this analysis:

- i. Existing Conditions
- ii. Existing Conditions plus Project
- iii. Project Opening Year (2023) with Cumulative Developments
- iv. Project Opening Year (2023) with Cumulative Developments plus Project

For the signalized intersection, the Level of Service (LOS) analysis is based on Intersection Capacity Utilization (ICU). **Table 1** provides the definition for LOS associated with values of volume-to-capacity ratios (V/C).

**Table 1. LOS Definitions – Signalized Intersections (ICU Analysis)**

LOS	V/C Ratio
A	0.00 – 0.60
B	0.61 – 0.70
C	0.71 – 0.80
D	0.81 – 0.90
E	0.91- 1.00
F	> 1.00

For non-signalized driveways, the LOS analyses are performed using SYNCHRO software based on the methodologies prescribed in the Highway Capacity Manual (HCM 2010). **Table 2** provides the definition for LOS associated with average control delay.

**Table 2. LOS Definitions – Unsignalized Driveways (HCM Analysis)**

LOS	Average Control Delay of Minor Approach (seconds/vehicle)
A	0 - 10
B	>10 - 15
C	>15 - 25
D	>25 - 35
E	>35 - 50
F	>50

## **EXISTING CONDITIONS**

Project site is located at the northeast corner of Garden Grove Boulevard and Brookhurst Street. The site is currently vacant.

Garden Grove Boulevard is a primary highway in the east-west directions with three lanes in each direction divided by a raised median. The posted speed limit is 40 mph in the project vicinity.

Brookhurst Street is also a primary highway in the north-south directions with three lanes in each direction divided by a raised median. The posted speed limit is 40 mph in the project vicinity. The intersection of Garden Grove Boulevard and Brookhurst Street is controlled by traffic signals.

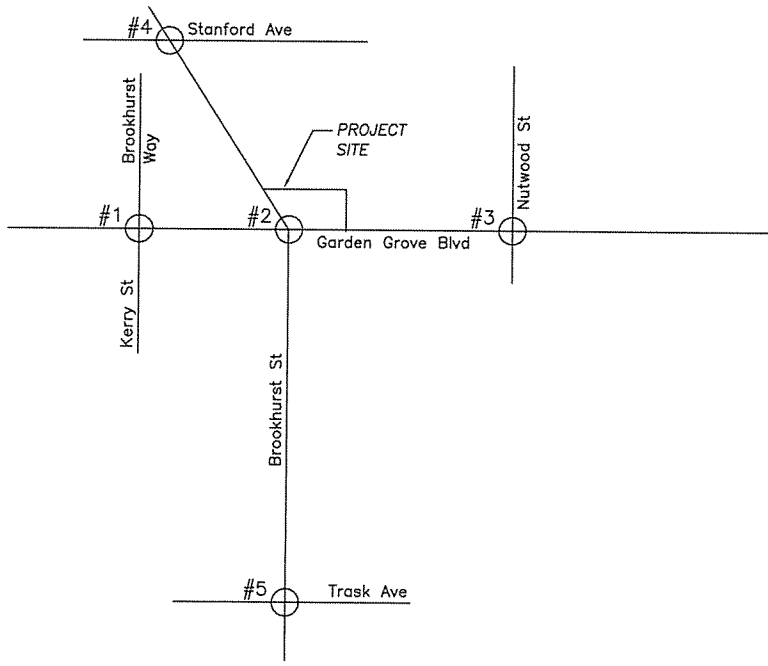
Pre-Covid traffic data for the AM and PM peak hour turning movements at study intersections collected in year 2018 and 2019 were provided by the City. Annual growth of one percent (1%) per year has been applied to each of these data to represent existing traffic volumes for this study. Lane configurations and existing traffic volumes at the study intersections are shown in **Exhibits 3** and **4**, respectively. Complete traffic data can be found in **Appendix B**.



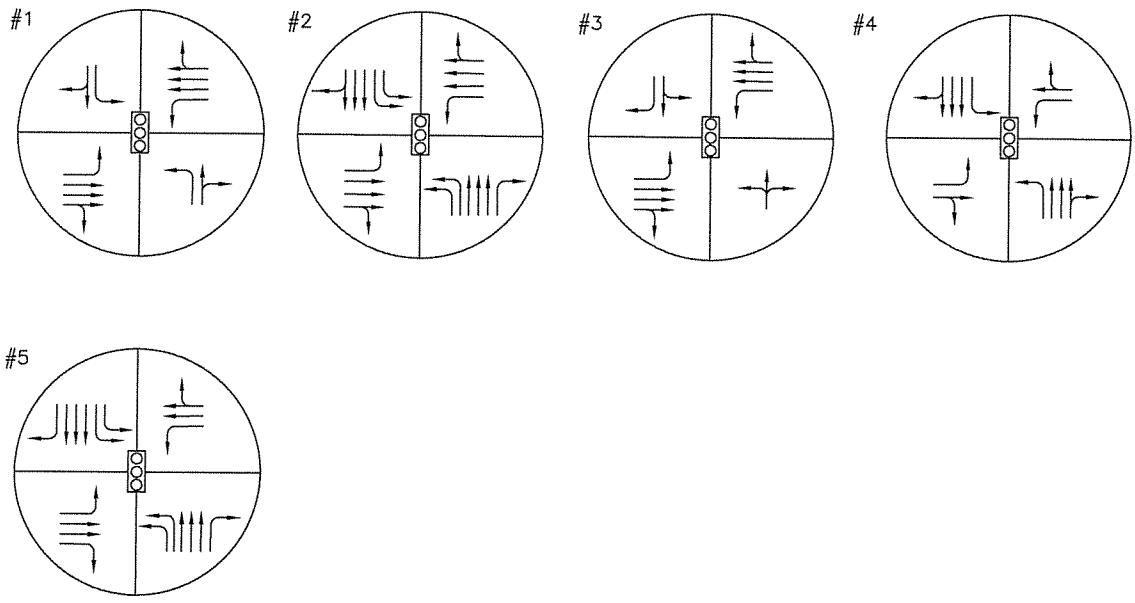
Level of service (LOS) and V/C ratio for existing conditions are shown in **Table 3**. The analysis worksheets can be found in **Appendix C**. All study intersections operate at acceptable LOS D or better in the AM and PM peak hours under existing condition.

**Table 3. Existing Conditions**

Intersection	AM Peak Hour		PM Peak Hour	
	LOS	V/C	LOS	V/C
1. Garden Grove Blvd at Brookhurst Way/Kerry St	A	0.360	A	0.353
2. Garden Grove Blvd at Brookhurst St	B	0.640	B	0.679
3. Garden Grove Blvd at Gilbert St	A	0.513	A	0.476
4. Brookhurst St at Stanford Ave	A	0.510	A	0.643
5. Brookhurst St at Trask Ave	C	0.757	D	0.903

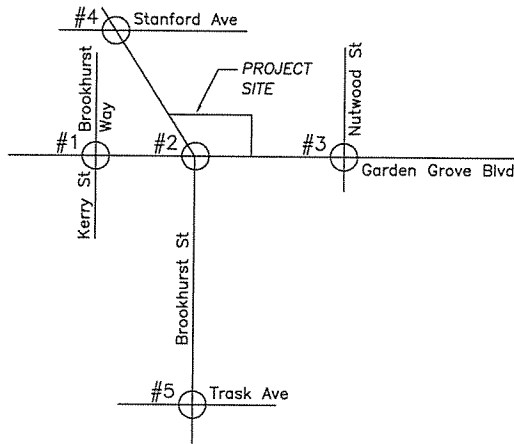


- LEGEND:**
- STUDY INTERSECTION
  - TRAFFIC SIGNAL
  - STOP SIGN



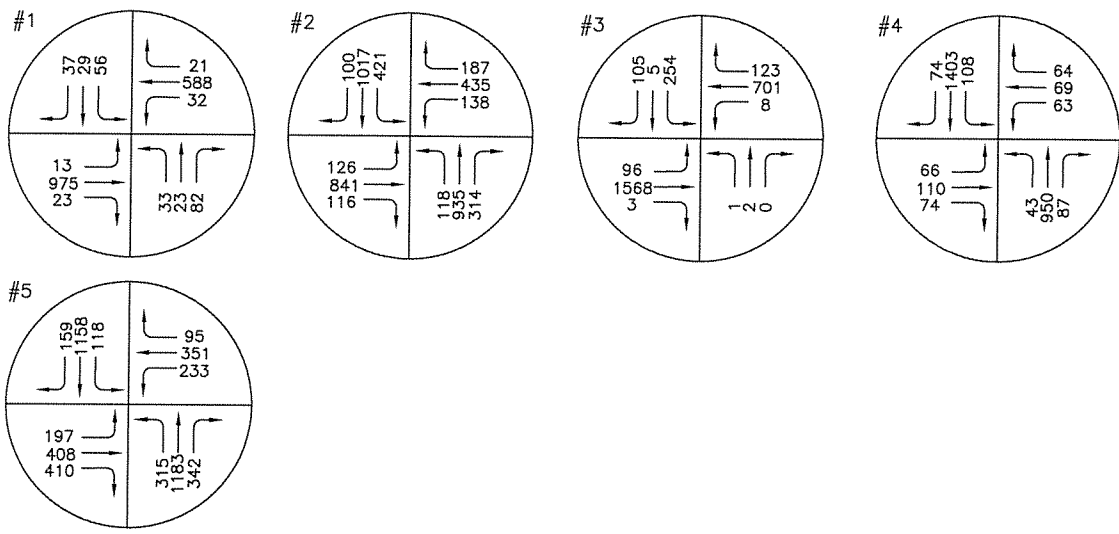
Garden Grove Mixed-Use  
10201 & 10231 Garden Grove Blvd, Garden Grove

**EXISTING LANE CONFIGURATION**

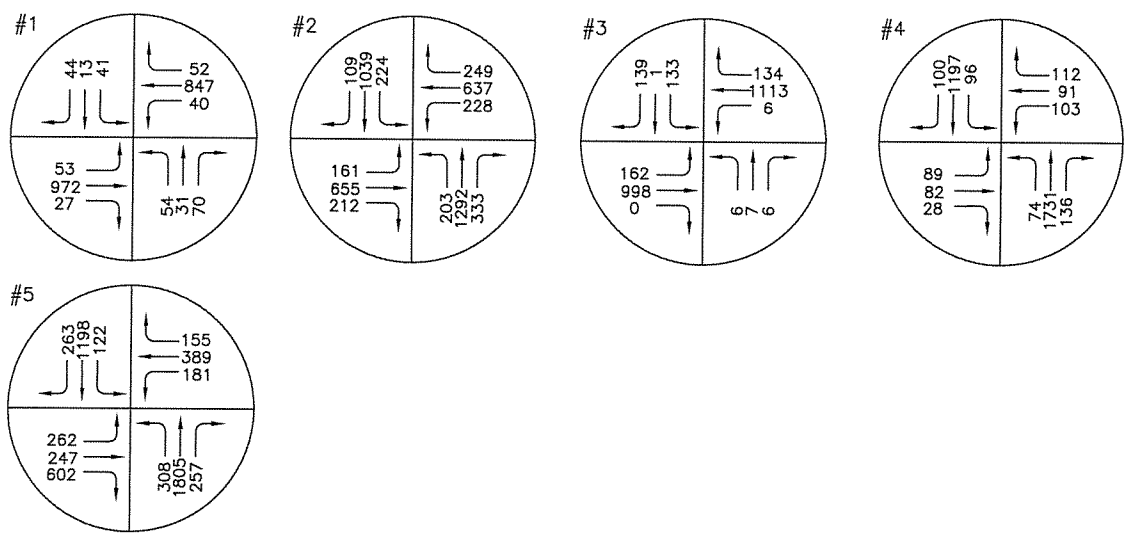


LEGEND:  
 ○ STUDY INTERSECTION

AM PEAK



PM PEAK



Garden Grove Mixed-Use  
 10201 & 10231 Garden Grove Blvd, Garden Grove

EXISTING TRAFFIC VOLUMES

## TRIP GENERATION

Trip generation represents the amount of traffic attracted and produced by the project development. Based upon the recommendations from *"Trip Generation, Tenth Edition"*, published by the Institute of Transportation Engineers (ITE), applicable trip generation rates are shown in **Table 4**.

**Table 4. Trip Generation Rate**

Land Use (ITE Code)	Unit	Daily	AM Peak Hour			PM Peak Hour		
			Rate	In	Out	Rate	In	Out
Multifamily Housing (Low-Rise) (220)	Dwelling Unit	7.32	0.46	23%	77%	0.56	63%	37%
Medical-Dental Office Building (720)	1000 Sq. Ft.	34.80	2.78	78%	22%	3.46	28%	72%
Shopping Center (820)	1000 Sq. Ft.	37.75	0.94	62%	38%	3.81	48%	52%

The study applied 34 percent of pass-by reduction for retail trips in the PM peak hour as recommended by *Trip Generation Handbook, Third Edition*. The study also applied a 10 percent internal trip credit as approved in the scoping agreement. Project trip generation were calculated and summarized in **Table 5**.

With pass-by consideration, the project is expected to have a NET trip generation of 54 trips in the AM peak hour, including 30 inbound and 24 outbound trips, 77 trips in the PM peak hour, including 35 inbound and 42 outbound trips, and 959 daily trips.

**Table 5. Project Trip Generation**

LAND USE	UNIT	Quantity	AM Peak Hour			PM Peak Hour			Daily
			Total	In	Out	Total	In	Out	
Multifamily Housing (Low-Rise) (220)	Dwelling Unit	52	24	6	18	29	18	11	381
Medical-Dental Office Building (720)	1,000 Sq. Ft.	9.927	28	22	6	34	10	24	345
Shopping Center (820)	1,000 Sq. Ft.	9.004	8	5	3	34	16	18	340
Subtotal			60	33	27	97	44	53	1066
Internal Trip Credit		10%	-6	-3	-3	-10	-4	-5	-107
Trip Generation Less Internal Trip Credit			54	30	24	88	40	48	959
Pass-by Trip Credit (Shopping Center Only)*		PM: 34%	-	-	-	-11	-5	-6	-
<b>NET Trip Generation</b>			<b>54</b>	<b>30</b>	<b>24</b>	<b>77</b>	<b>35</b>	<b>42</b>	<b>959</b>

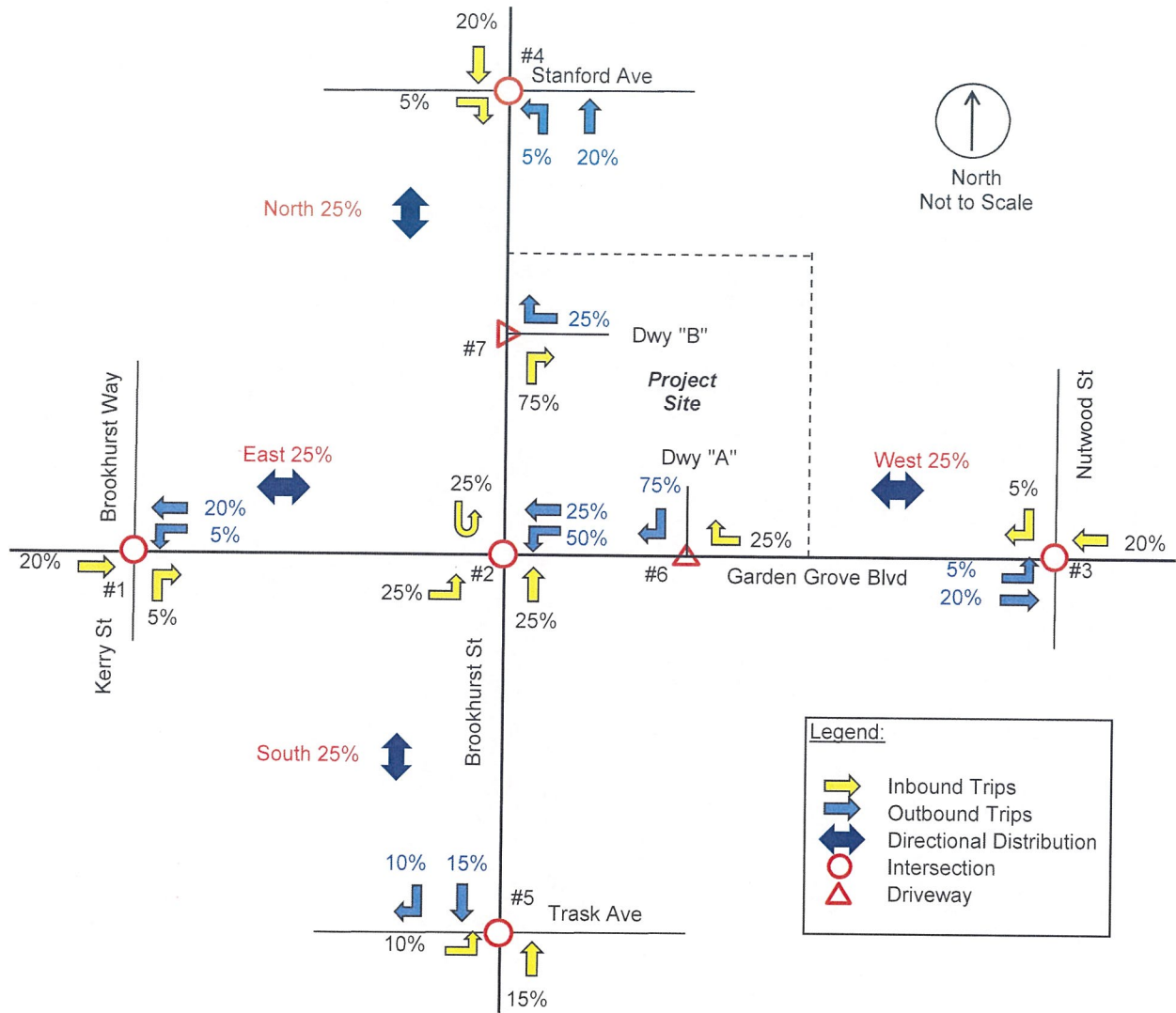
## **TRIP DISTRIBUTION**

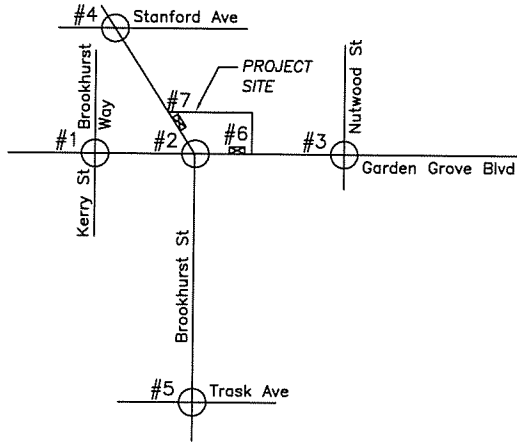
Trip distribution represents the directional orientation of traffic to and from the proposed project. Directional orientation is largely influenced by the geographical location of the site, among many other factors. The trip distribution pattern for the project is illustrated on **Exhibit 5**.

## **TRAFFIC ASSIGNMENT**

The traffic assignment to and from the site has been based upon the results of trip generation, trip distribution, and access layouts. **Exhibit 6** illustrates the traffic assignment of the proposed project in the AM and PM peak hour.

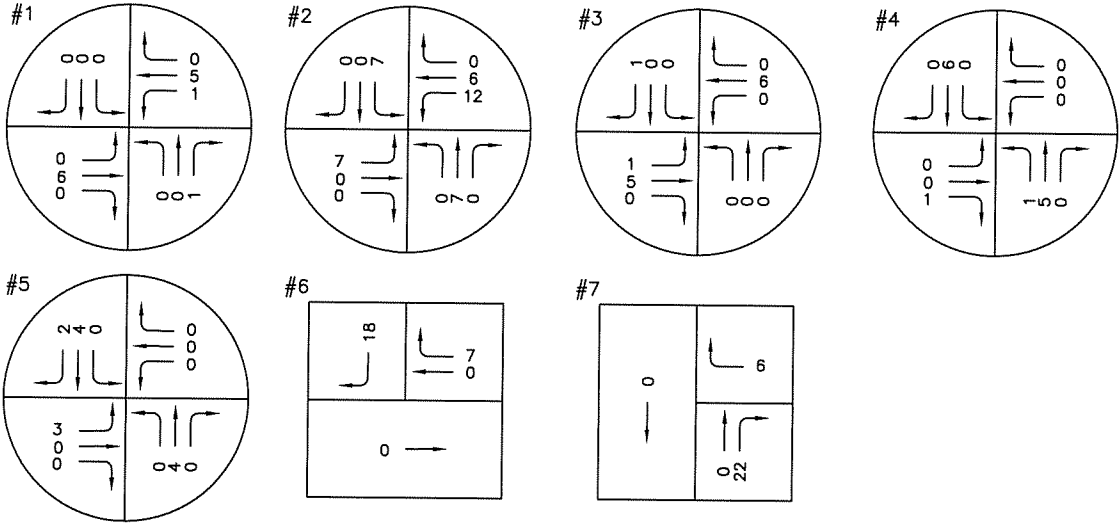
**EXHIBIT 5. TRIP DISTRIBUTION**



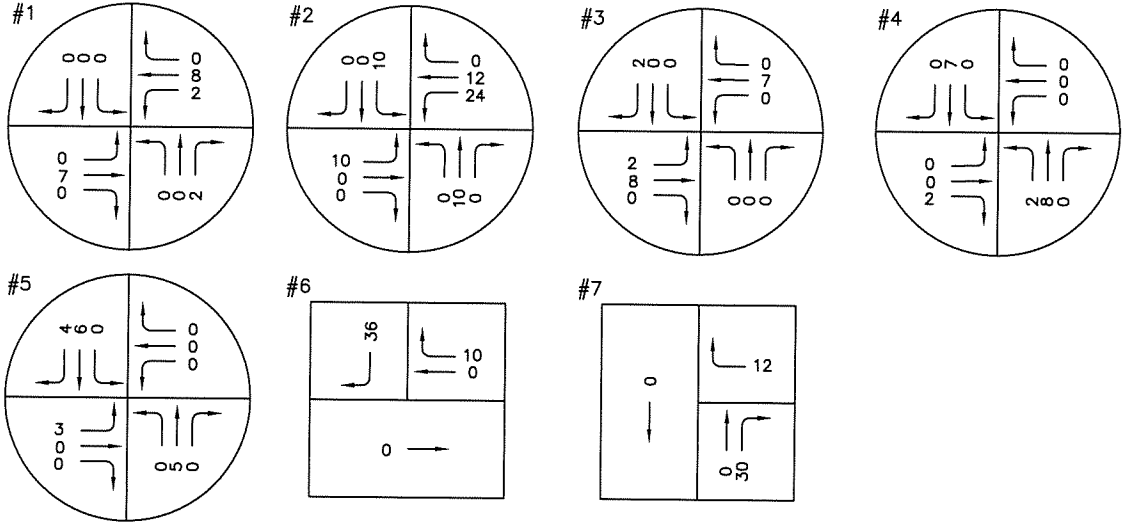


LEGEND:  
 ○ STUDY INTERSECTION  
 ▣ DRIVEWAY

AM PEAK



PM PEAK



TRAFFIC ASSIGNMENT

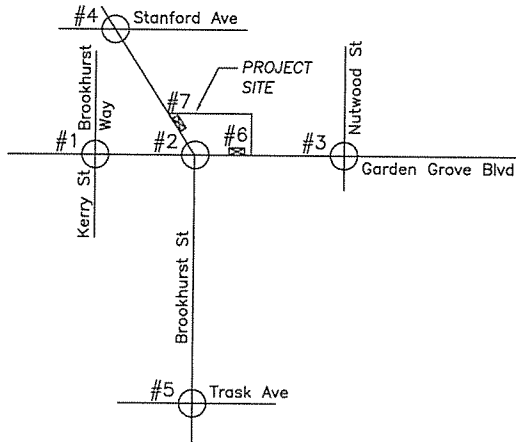


## EXISTING CONDITIONS WITH PROJECT

Traffic volumes at the study intersections based on existing conditions plus project are shown in **Exhibit 7**. The level of service and V/C ratios are shown in **Table 6**. All study intersections will operate at LOS D or better for the AM and PM peak hours.

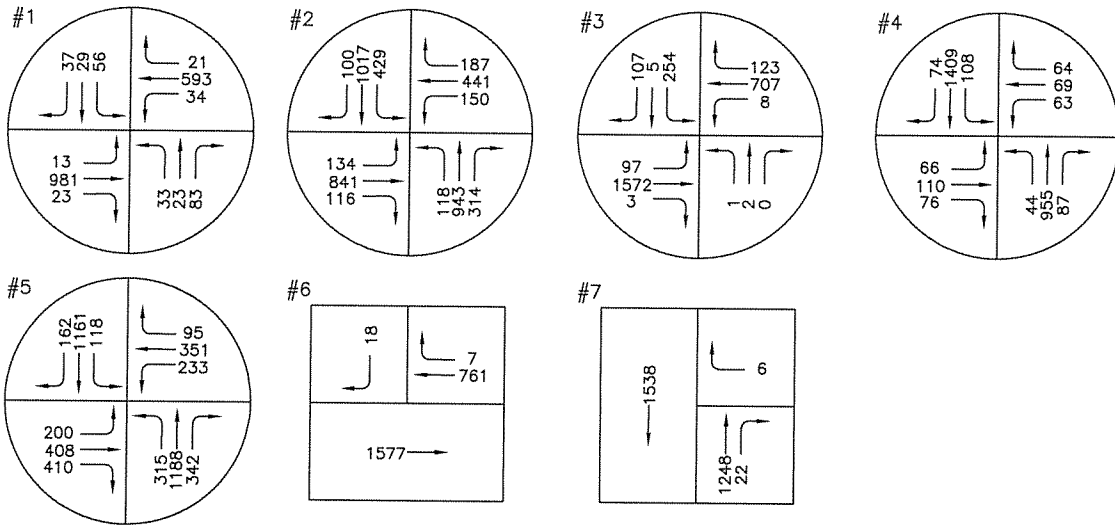
**Table 6. Existing Conditions plus Project**

Intersection	AM Peak Hour		PM Peak Hour	
	LOS	V/C	LOS	V/C
1. Garden Grove Blvd at Brookhurst Way/Kerry St	A	0.363	A	0.356
2. Garden Grove Blvd at Brookhurst St	B	0.650	B	0.699
3. Garden Grove Blvd at Gilbert St	A	0.514	A	0.479
4. Brookhurst St at Stanford Ave	A	0.513	A	0.645
5. Brookhurst St at Trask Ave	C	0.758	D	0.904
6. Driveway "A" on Garden Grove Blvd	A		A	
7. Driveway "B" on Brookhurst St	C		D	

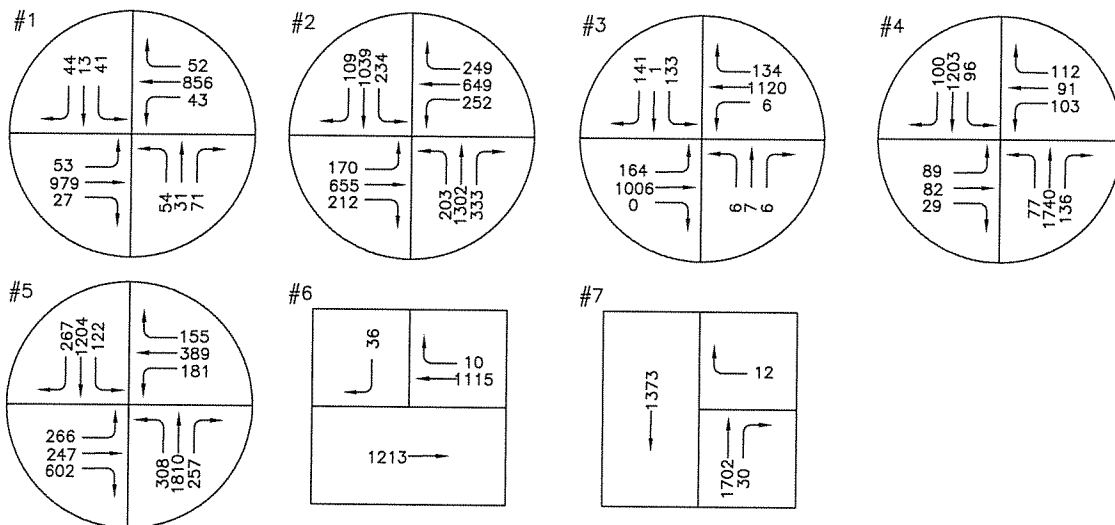


**LEGEND:**  
 ○ STUDY INTERSECTION  
 □ DRIVEWAY

AM PEAK



PM PEAK



EXISTING PLUS PROJECT TRAFFIC VOLUMES

## OPENING YEAR CUMULATIVE CONDITIONS

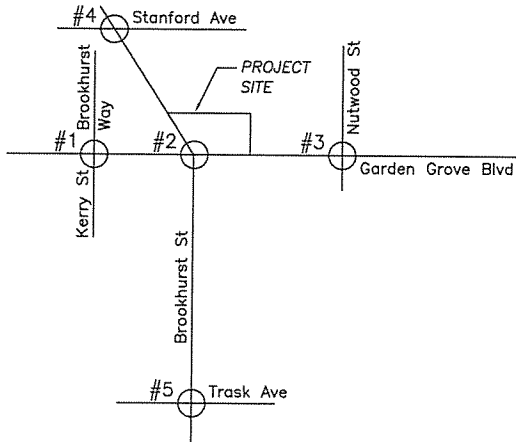
For project opening year 2023, the annual growth rate of one percent (1%) is used to reflect the cumulative growth of traffic in regional developments. Lane configurations and traffic controls for the opening year are assumed consistent with those previously shown in **Exhibit 3**. Traffic volumes for the project opening year with cumulative developments are illustrated in **Exhibit 8**.

The project's level of service under opening year with cumulative developments conditions are shown in **Table 7**. All study intersections operate at acceptable LOS D or better in the AM and PM peak hours except the following:

- #5, Brookhurst Street at Trask Avenue: LOS E in the PM peak hour

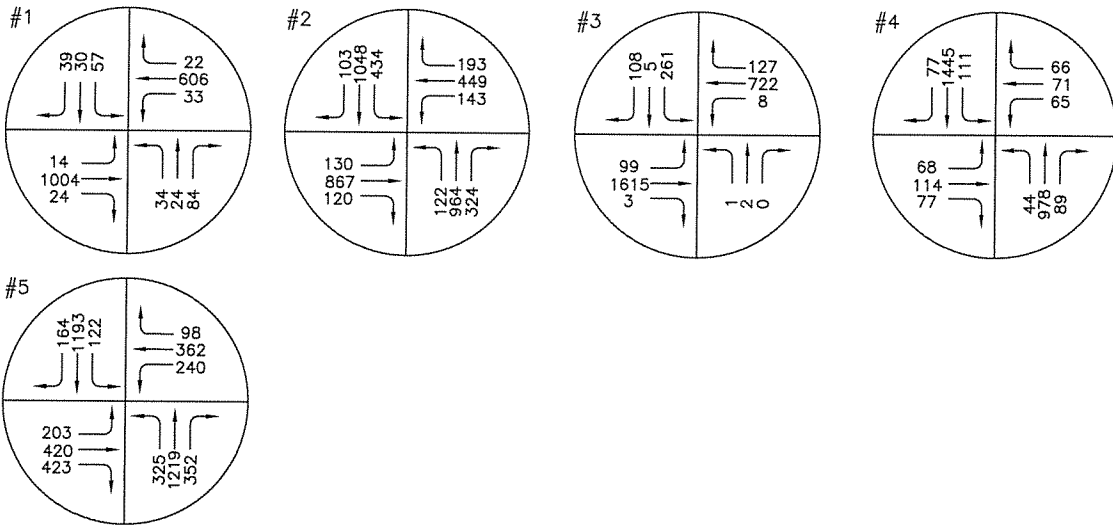
**Table 7. Opening Year (2023) Cumulative Conditions - Without Project**

Intersection	AM Peak Hour		PM Peak Hour	
	LOS	V/C	LOS	V/C
1. Garden Grove Blvd at Brookhurst Way/Kerry St	A	0.369	A	0.362
2. Garden Grove Blvd at Brookhurst St	B	0.658	B	0.699
3. Garden Grove Blvd at Gilbert St	A	0.527	A	0.489
4. Brookhurst St at Stanford Ave	A	0.523	A	0.662
5. Brookhurst St at Trask Ave	C	0.778	E	0.930

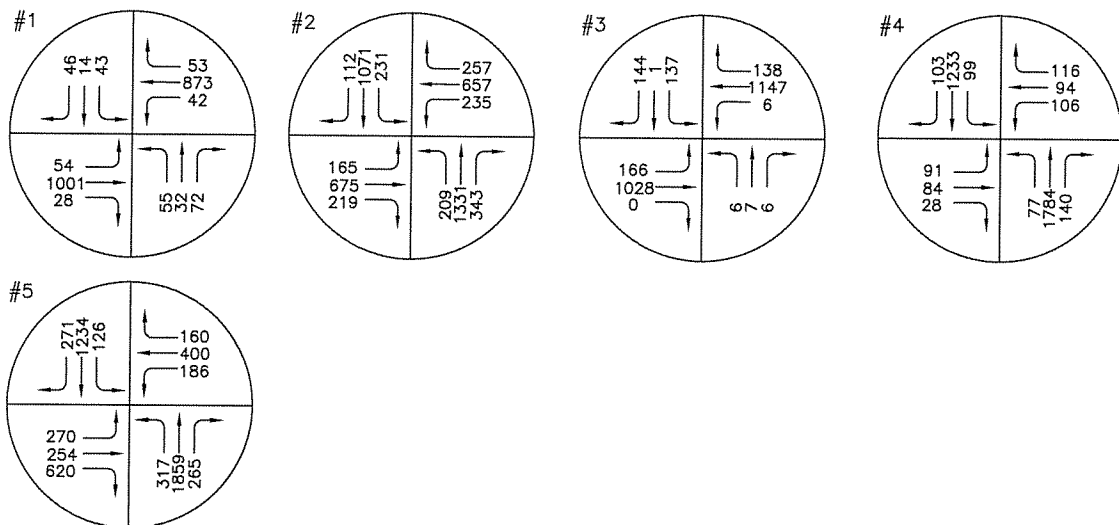


LEGEND:  
 ○ STUDY INTERSECTION

AM PEAK



PM PEAK



OPENING YEAR WITH CUMULATIVE TRAFFIC VOLUMES

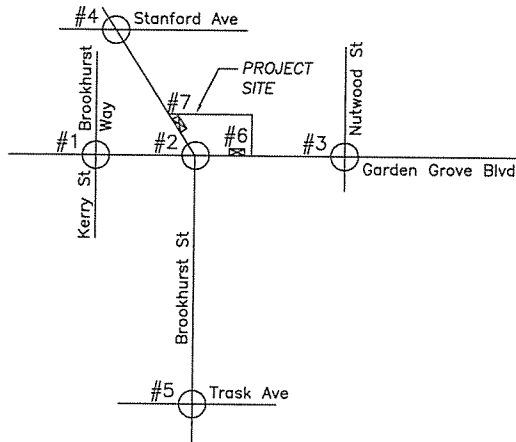
## OPENING YEAR CUMULATIVE CONDITIONS PLUS PROJECT

Traffic volumes for the project opening year with cumulative developments plus project traffic are illustrated in **Exhibit 9**. The level of services and V/C ratios at study intersections under opening year cumulative plus project conditions are shown in **Table 8**. All study intersections operate at acceptable LOS D or better in the AM and PM peak hours except the following:

- #5, Brookhurst Street at Trask Avenue: LOS E in the PM peak hour

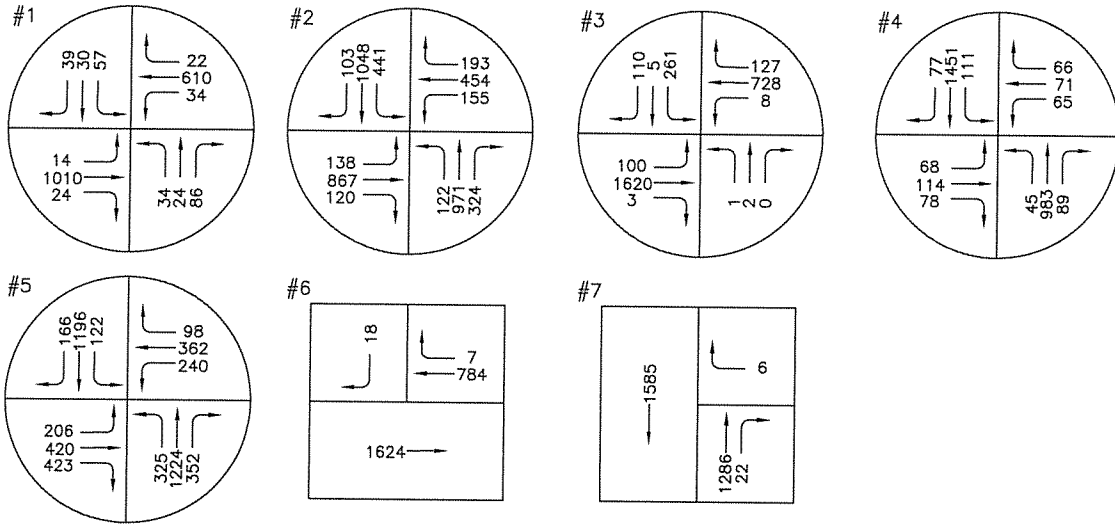
**Table 8. Opening Year Cumulative Conditions plus Project**

Intersection	AM Peak Hour		PM Peak Hour	
	LOS	V/C	LOS	V/C
1. Garden Grove Blvd at Brookhurst Way/Kerry St	A	0.371	A	0.365
2. Garden Grove Blvd at Brookhurst St	B	0.668	C	0.718
3. Garden Grove Blvd at Gilbert St	A	0.528	A	0.491
4. Brookhurst St at Stanford Ave	A	0.527	B	0.664
5. Brookhurst St at Trask Ave	C	0.779	E	0.931
6. Driveway "A" on Garden Grove Blvd	A		A	
7. Driveway "B" on Brookhurst St	C		D	

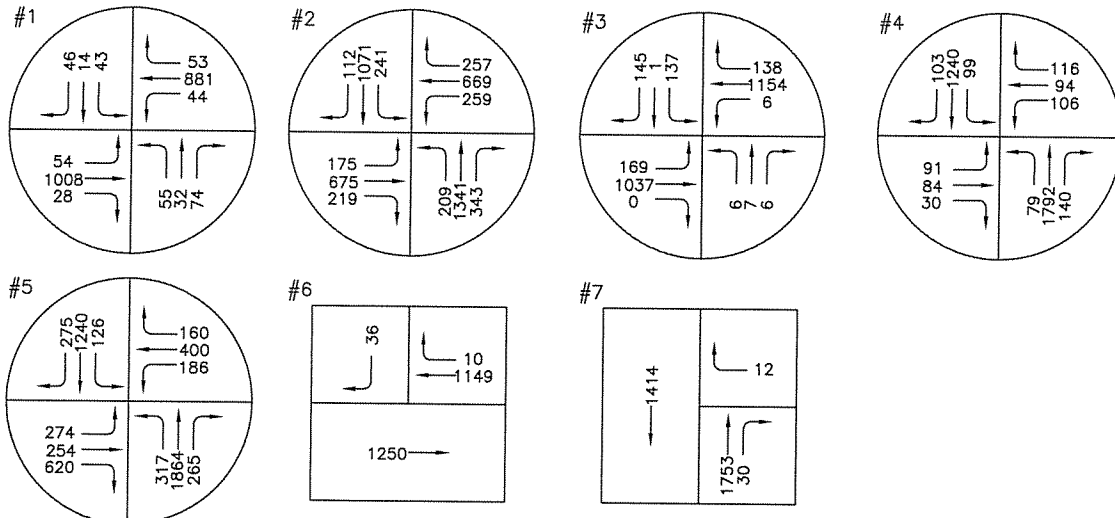


**LEGEND:**  
 ○ STUDY INTERSECTION  
 ▭ DRIVEWAY

AM PEAK



PM PEAK



**OPENING YEAR WITH CUMULATIVE PLUS PROJECT TRAFFIC VOLUMES**

## THRESHOLD OF SIGNIFICANT IMPACT

In accordance with the 2017 Orange County Transportation Authority (OCTA) Congestion Management Plan (CMP), the traffic impact is deemed significant and mitigation is required if (1) the intersection operates at worse than LOS E; and (2) the ICU increases by 0.01 or more.

The traffic impacts of the proposed project based on the opening year conditions are shown in **Table 9**. The project does not have a significant traffic impact and mitigation measure is, therefore, not required.

Table 9. Project Intersection Impact Analysis - Opening Year

No.	Intersection	W/O Project		With Project		ICU Increase	Significant Impact
		LOS	V/C	LOS	V/C		
<b>AM PEAK</b>							
1	Garden Grove Blvd at Brookhurst Way/Kerry St	A	0.369	A	0.371	0.003	No
2	Garden Grove Blvd at Brookhurst St	B	0.658	B	0.668	0.006	No
3	Garden Grove Blvd at Gilbert St	A	0.527	A	0.528	0.000	No
4	Brookhurst St at Stanford Ave	A	0.523	A	0.527	0.008	No
5	Brookhurst St at Trask Ave	C	0.778	C	0.779	0.007	No
<b>PM PEAK</b>							
1	Garden Grove Blvd at Brookhurst Way/Kerry St	A	0.362	A	0.365	0.004	No
2	Garden Grove Blvd at Brookhurst St	B	0.699	C	0.718	0.002	No
3	Garden Grove Blvd at Gilbert St	A	0.489	A	0.491	0.020	No
4	Brookhurst St at Stanford Ave	A	0.662	B	0.664	0.008	No
5	Brookhurst St at Trask Ave	E	0.930	E	0.931	0.001	No

## SITE ACCESS

The project provides two access driveways: one on Garden Grove Boulevard and the other on Brookhurst Street. With the presence of an existing raised median, the driveway on Garden Grove Boulevard is for right-in-right-out only. The driveway on Brookhurst Street currently faces an existing median opening. However, the median opening on Brookhurst Street will be closed off as part of the Brookhurst Triangle Development on the west side of Brookhurst Street. Therefore, the driveway on Brookhurst Street will also become right-in-right-out only. In the event that the Brookhurst Triangle Development has not closed off the southbound left turn at the subject driveway prior to project completion, the project will be required to develop a plan and construct accordingly to restripe and add delineators to prevent left turns in and out from Brookhurst Street.

Both driveways will operate at LOS C or better in the AM and PM peak hour for the project opening year with project, as shown in **Table 11**. The analysis worksheets can be found in **Appendix D**.

**Table 10. Level of Service of Driveway**

Driveway	AM		PM	
	LOS	Delay (s)	LOS	Delay (s)
Driveway on Garden Grove Blvd	B	12.7	C	16.1
Driveway on Brookhurst St	C	16.4	C	22.6

At the driveway entries, it is necessary that the height of shrubs, planting, and other visual obstructions be limited to a maximum height of thirty inches to maintain sufficient corner sight distance.



## **ON-SITE CIRCULATION**

Both driveways provide normal access to the parking structure for retail, office, and residential uses during business hours. All entrance gates will be closed at night prohibiting non-residential access. The only access point for residents at night times will be through the south gate from Garden Grove Boulevard which is controlled by coded cards and intercom and provided with a turn-around area in front of the gate. The rollup gate at Brookhurst Street will allow sensor-activated exit only. An internally illuminated "No Entry" sign will turn on at night and access from this entrance is prohibited after business hours. In addition, a turn-around area is provided for vehicles inadvertently turning into the driveway on Brookhurst Street.

Stacking distances from the gate to the driveway are approximately 180 feet from Garden Grove Boulevard and 60 feet from Brookhurst Street. Estimated stacking capacities are eight (8) cars from Garden Grove Boulevard and three (3) cars from Brookhurst Street. Both access gates provide adequate stacking distance to effectively avoid excessive queuing backing up onto public streets.

On-site circulation appears efficient and safe without unnecessary bottlenecks. The site plan is subject to review and final approval by the Fire Department, Planning Department and Traffic Engineer.

## **ACTIVE TRANSPORTATION AND PUBLIC TRANSIT ANALYSIS**

The proposed development does not plan to make any change to the existing public transit, bicycle, or pedestrian facilities. The project does not result in any conflict with adopted policies, plans, or programs regarding public transit, bicycle, or pedestrian facilities, or otherwise decreases the performance or safety of such facilities. The project is not anticipated to have a significant impact to the active transportation and public transit system.

## **VEHICLE MILES TRAVELED (VMT) SCREENING**

In accordance with Senate Bill (SB) 743 and the new California Environmental Quality Act (CEQA) requirements, “*City of Garden Grove Traffic Impact Analysis Guidelines for Vehicle Miles Traveled and Level of Service Assessment, dated May 2020*” has identified Vehicle Miles Traveled (VMT) as the preferred metric to assess transportation impacts. A VMT analysis should be conducted for land use projects as deemed necessary by the City Traffic Engineer and would apply to projects that have the potential to increase the average VMT per service population (e.g. population plus employment) for the City of Garden Grove. Normalizing VMT per service population provides a transportation efficiency metric that the analysis is based on. All assumptions and methodologies of the VMT analysis are subject to review by the City Traffic Engineer.

In accordance with the project screening methodologies listed in the City of Garden Grove Traffic Impact Analysis Guidelines for Vehicle Miles Traveled and Level of Service Assessment dated May 2020, there are three types of project screening that lead agencies can apply to effectively screen projects from project’s level assessment. The project only needs to fulfill one of the screening types below to qualify for screening. These screening procedures are summarized below:

### **Step 1: Transit Priority Area (TPA) Screening**

TPA is defined as a half-mile area around an existing major transit stop or an existing stop along a High-Quality Transit Corridor (HQTC). Major transit stop means a site containing an existing rail transit station, a ferry terminal served by either a bus or rail transit service, or the intersection of two or more major bus routes with a frequency of service interval of 15 minutes or less during the morning and afternoon peak commute periods. As shown in **Exhibit 10**, the project site is not located within Garden Grove Transit Priority Area (TPA). Therefore, the project does not meet the TPA Screening.

### **Type 2: Low VMT Area Screening**

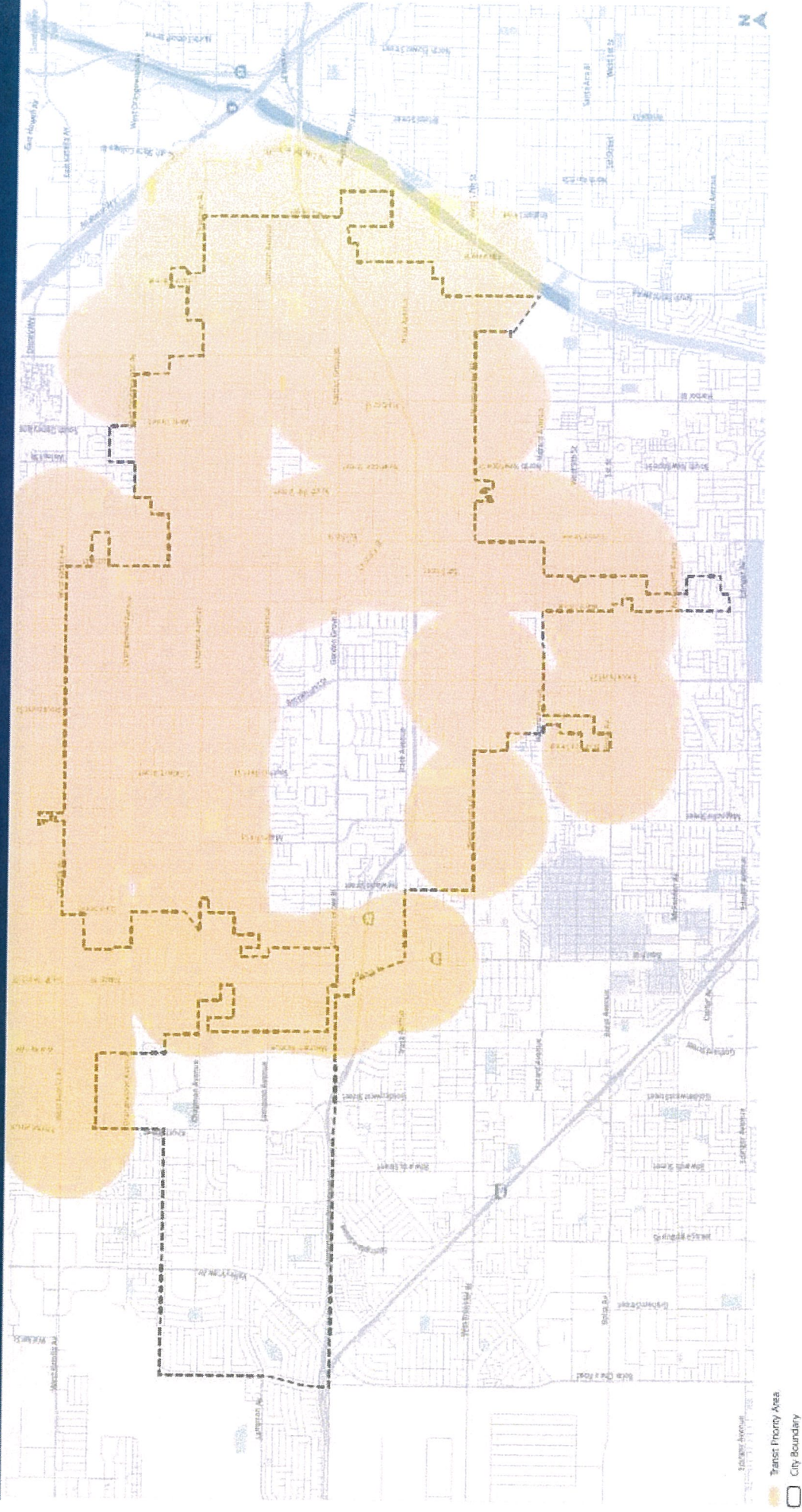
Mixed-use projects may qualify for VMT Area Screening if the project is located within a low-VMT-generating area. As shown in **Exhibit 11**, the project is located within the Garden Grove Low-VMT-generating Areas identified as 15% below the countywide average. In addition, the project can reasonably be expected to generate VMT per service population similar to the existing land uses in the low VMT area. The project land uses would not alter the existing built environment in such a way as to increase the rate or length of vehicle trips. Therefore, the project meets the criteria of Type 2 Low VMT Area Screening and may be presumed to have a less than significant impact.

### **Type 3: Project Type Screening**

The proposed mixed-use development is not listed among the project types that can be screened from project-level assessment. Therefore, Type 3 Screening is not applicable.

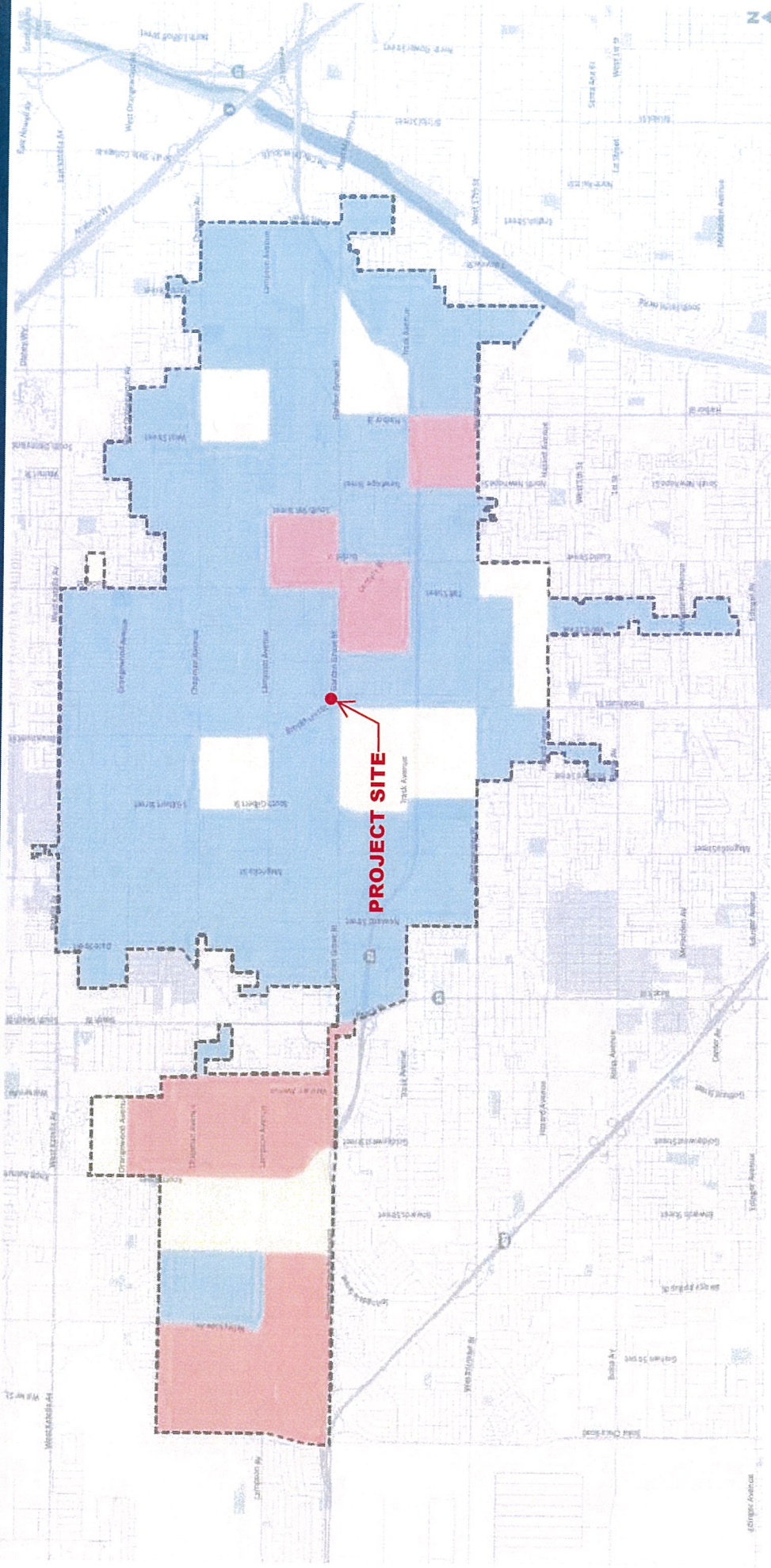
Absent substantial evidence otherwise, it is reasonable to conclude that the project could be considered not to lead to a significant VMT impact. Subject to final determination by City Traffic Engineer, the complete project-level VMT analysis and forecasting through regional model is not required.

# Garden Grove Transit Priority Areas (TPAs)



**Exhibit 10. Garden Grove Transit Priority Areas (TPA)**

# Garden Grove Low VMT Areas 15% Below Countywide Comparison



- City Boundary
- <15% below County Average
- 0 to -15% below County Average
- Higher than County Average

**Exhibit 11. Garden Grove Low-VMT-generating Areas**

OD Method: Daily VMT per Service Population Compared to County Average (2012)



## CONCLUSION

All study intersections are expected to operate at acceptable LOS D or better in the AM and PM peak hours for the project opening year, except for the intersection of Brookhurst Street at Trask Avenue where the intersection operates at LOS E in the PM peak hour. According to the significant impact thresholds outlined by the 2017 Orange County Transportation Authority (OCTA) Congestion Management Plan (CMP), the project does not have a significant impact and mitigation measure is not required.

The median opening on Brookhurst Street near the project driveway is expected to be closed off as part of the Brookhurst Triangle Development. In the event that the median closure has not taken place prior to project completion, the project will be required to develop a plan and construct accordingly to restripe and add delineators to prevent left turns in and out from Brookhurst Street.

In accordance with the "*City of Garden Grove Traffic Impact Analysis Guidelines for Vehicle Miles Traveled and Level of Service Assessment*", the project meets the Low VMT Area Screening and may be presumed to have a less than significant VMT impact. A complete project-level VMT analysis and forecasting through the regional model is not required.

The complete findings and recommendations of this report can be found in the Executive Summary on page 2.

**APPENDIX A**  
**SCOPING AGREEMENT**

**Traffic Impact Study Scope**

Project Names:	Mixed-Use Development	
Project Address:	10201 & 10231 Garden Grove Blvd, Garden Grove	
Project Description:	Mixed-use development of a new 52-unit apartment, medical offices (9,927 sq.ft.) and retail uses (9,004 sq.ft.). See Exhibit 1 for Site Plan.	
	Consultant	Developer
Name	Kay Hsu, PE, TE K2 Traffic Engineering, Inc.	Paul Kim, Architect Cal-City Construction, Inc.
Address	1442 Irvine Blvd, Ste 210 Tustin, CA 92780	16605 Norwalk Blvd Cerritos, CA 90703
Telephone	714-832-2116	714-309-7444
Email	khsu@k2traffic.com	paulkim@cal-city.com

**A. Trip Generation**

Proposed Land Use	See Exhibit 2
Reference	Trip Generation (10th Edition) by ITE

Net Trip Generation	Inbound	Outbound	Total
AM Peak Hour	30	24	54
PM Peak Hour	35	42	77
Daily Trip	959		

**B. Trip Distribution** Predicted distribution as shown on Exhibit 3

**C. Background Traffic**

Project Opening Year	2023	Growth Rate	1% Annual
----------------------	------	-------------	-----------


**D. Study Intersections** (Pre-Covid Counts to be provided by the City)

1. Garden Grove Blvd at Brookhurst Way/Kerry St	5. Brookhurst St at Trask Ave
2. Garden Grove Blvd at Brookhurst St	6. Driveway "A" at Garden Grove St
3. Garden Grove Blvd at Gilbert St	7. Driveway "B" at Brookhurst St
4. Brookhurst St at Stanford Ave	


**E. Specific Issues to be addressed in the Study**

1. Study scenarios: Existing Conditions, Existing Plus Project, Opening Year Cumulative with and without Project, Opening Year Cumulative Plus Project with Mitigation Measures, if applicable.
2. Site access with discussions of parking structure access, gate location, and access for retail components. Right-in-right-out driveway on Brookhurst assuming closure of median opening by Brookhurst Triangle Development.
3. VMT screening is required.
4. Cumulative project info to be furnished by the City and attached hereon.

**Recommended by:**

 7/22/2020  
 Consultant Date  
 Resubmitted on 10/7/2020

**Approved by:**

 10/8/2020  
 City of Garden Grove Date  
 Public Works Dept., Engineering Div.





**EXHIBIT 2. TRIP GENERATION**

**Proposed uses:**

1. 52-unit Apartment
2. Medical Office, 9,927 sq.ft
3. Retail, 9,004 sq.ft

**TABLE 1. TRIP GENERATION RATE (ITE)**

LAND USE	UNIT	Daily	AM Peak Hour			PM Peak Hour		
			Total	In	Out	Total	In	Out
Multifamily Housing (Low-Rise) (220)	Dwelling Unit	7.32	0.46	23%	77%	0.56	63%	37%
Medical-Dental Office Building (720)	1,000 Sq. Ft.	34.80	2.78	78%	22%	3.46	28%	72%
Shopping Center (820)	1,000 Sq. Ft.	37.75	0.94	62%	38%	3.81	48%	52%

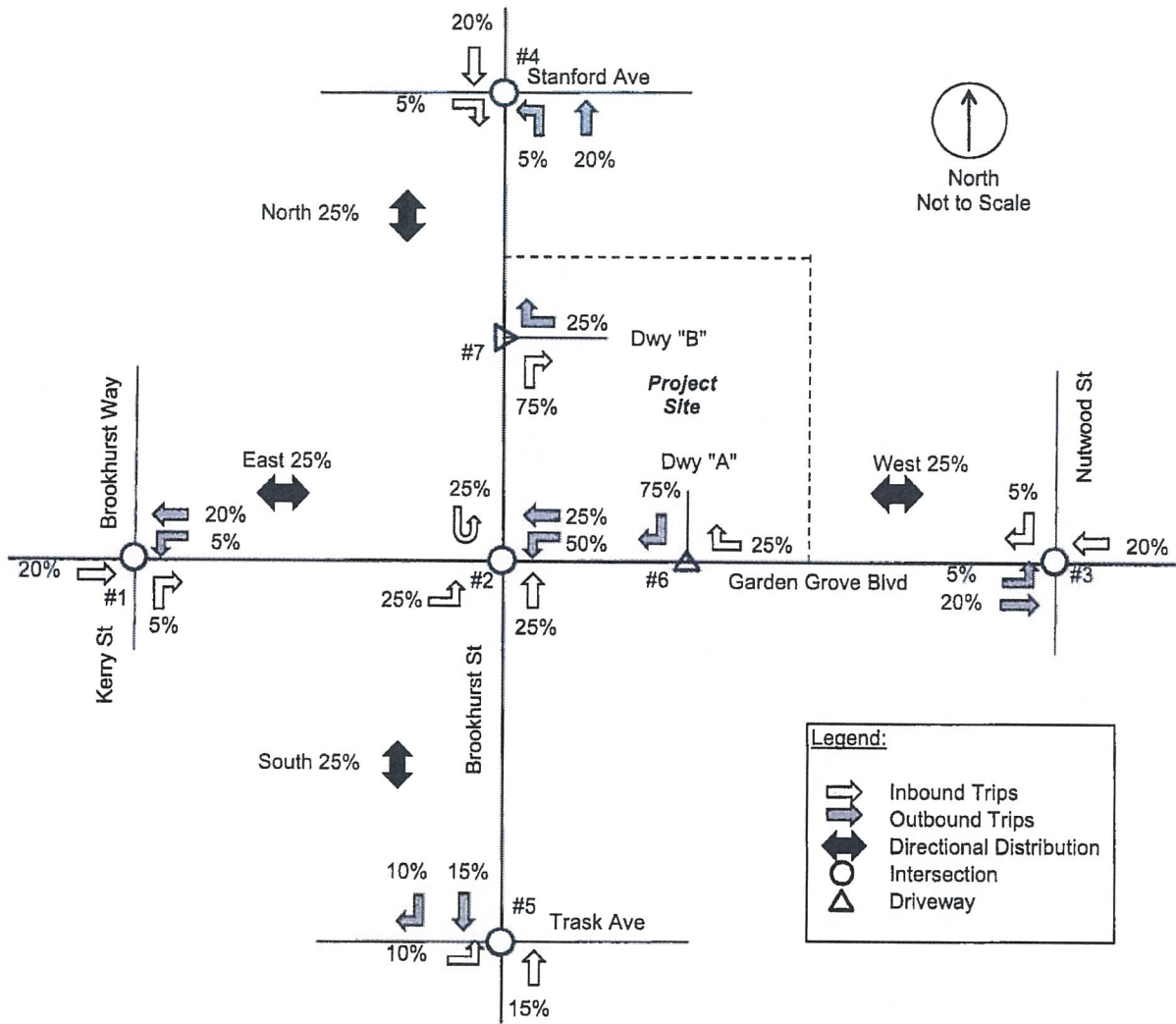
Source: Trip Generation Manual, 10th Edition

**TABLE 2. NET TRIP GENERATION**

LAND USE	UNIT	Quantity	AM Peak Hour			PM Peak Hour			Daily
			Total	In	Out	Total	In	Out	
Multifamily Housing (Low-Rise) (220)	Dwelling Unit	52	24	6	18	29	18	11	381
Medical-Dental Office Building (720)	1,000 Sq. Ft.	9,927	28	22	6	34	10	24	345
Shopping Center (820)	1,000 Sq. Ft.	9,004	8	5	3	34	16	18	340
Subtotal			60	33	27	97	44	53	1066
Internal Trip Credit		10%	-6	-3	-3	-10	-4	-5	-107
<b>Trip Generation less Internal Trip Credit</b>			<b>54</b>	<b>30</b>	<b>24</b>	<b>88</b>	<b>40</b>	<b>48</b>	<b>959</b>
Pass-by Trip Credit (Shopping Center Only)*		PM: 34%	-	-	-	-11	-5	-6	-
<b>NET Trip Generation</b>			<b>54</b>	<b>30</b>	<b>24</b>	<b>77</b>	<b>35</b>	<b>42</b>	<b>959</b>

\*Per Trip Generation Handbook, 3rd Edition

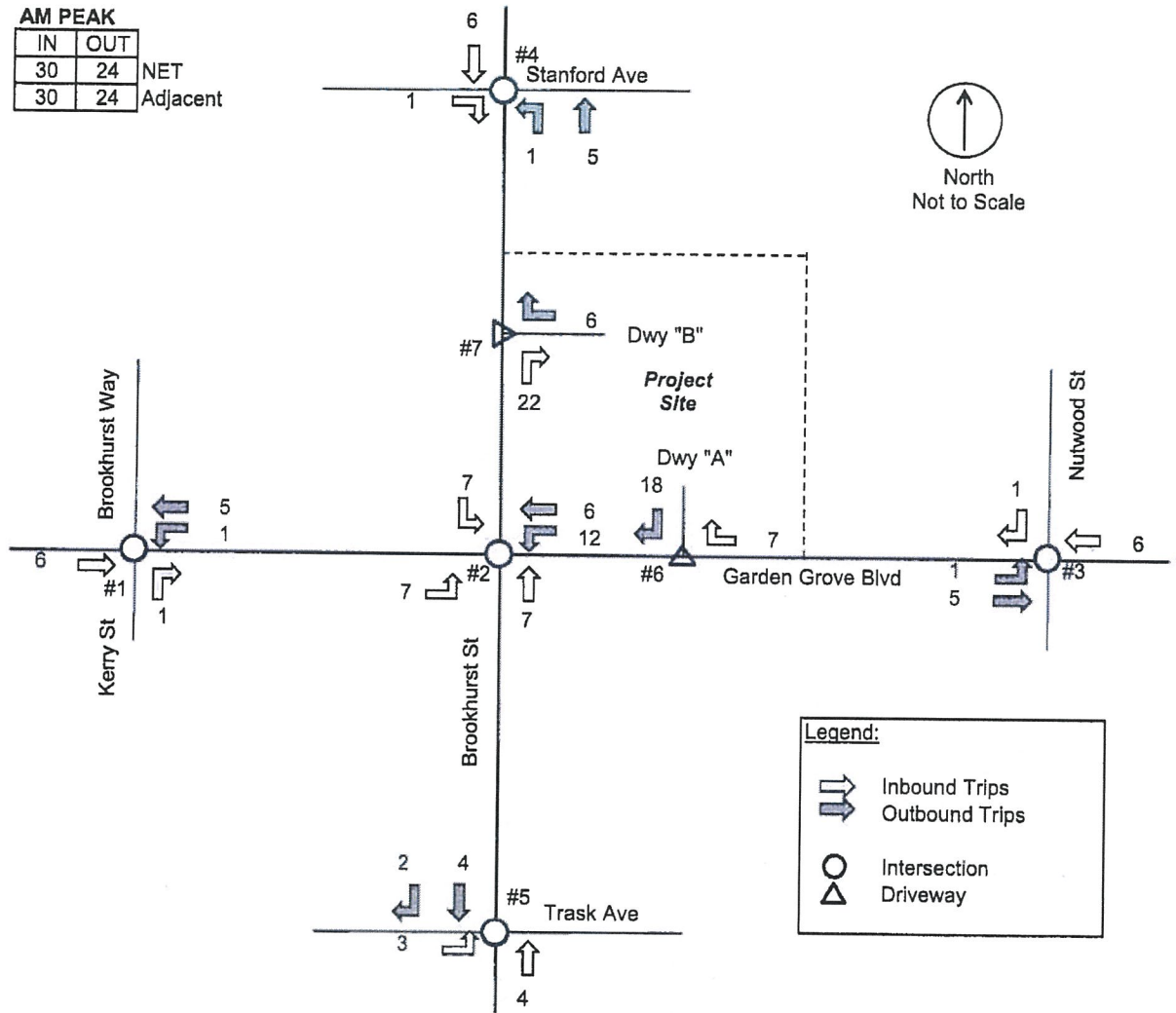
**EXHIBIT 3. TRIP DISTRIBUTION**



**EXHIBIT 4. TRAFFIC ASSIGNMENT - AM PEAK**

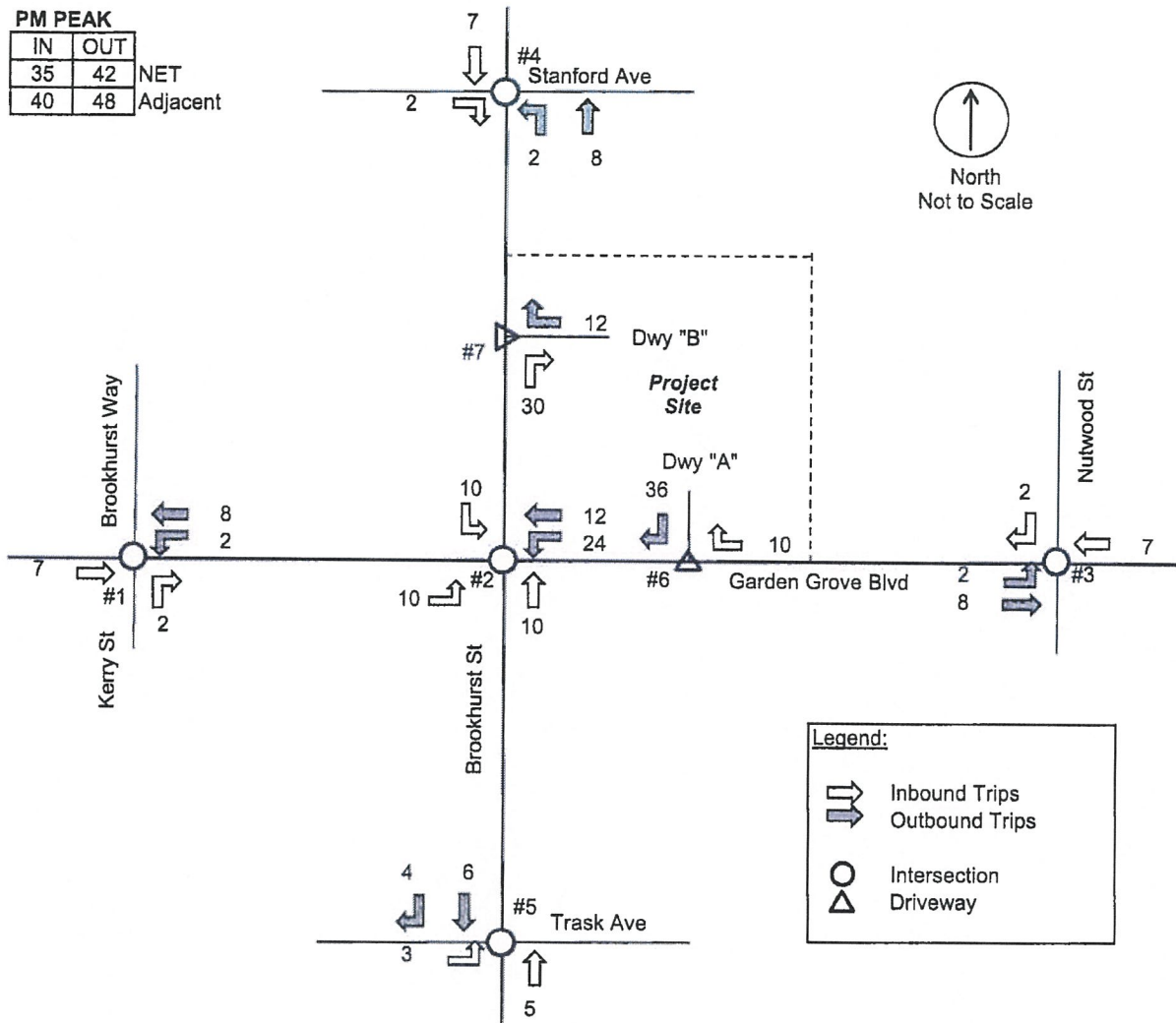
**AM PEAK**

IN	OUT	NET
30	24	NET
30	24	Adjacent



**EXHIBIT 4. TRAFFIC ASSIGNMENT - PM PEAK**

PM PEAK		
IN	OUT	NET
35	42	
40	48	Adjacent



**APPENDIX B**  
**TURNING MOVEMENT COUNT DATA**

### INTERSECTION TURNING MOVEMENT COUNTS

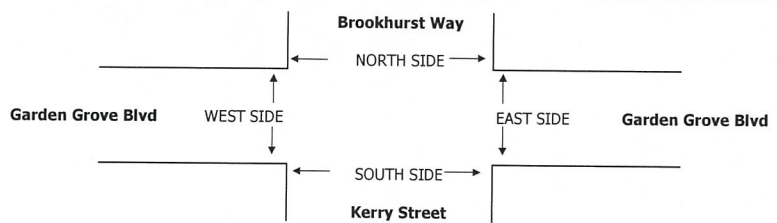
PREPARED BY:

<b>DATE:</b> Tue, Nov 19, 19	<b>LOCATION:</b> Garden Grove NORTH & SOUTH: Brookhurst Way/Kerry St EAST & WEST: Garden Grove Blvd	<b>PROJECT #:</b> SC2085 <b>LOCATION #:</b> <b>CONTROL:</b> SIGNAL
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NOTES:

Add U-Turns to Left Turns

	NORTHBOUND Kerry Street			SOUTHBOUND Brookhurst Way			EASTBOUND Garden Grove Blvd			WESTBOUND Garden Grove Blvd			TOTAL	U-TURNS				
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR		NB	SB	EB	WB	TTL
LANES:	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
AM																		
7:00 AM																		
7:15 AM																		
7:30 AM																		
7:45 AM																		
8:00 AM																		
8:15 AM																		
8:30 AM																		
8:45 AM																		
VOLUMES																		
APPROACH %																		
APP/DEPART																		
BEGIN PEAK HR																		
VOLUMES																		
APPROACH %																		
PEAK HR FACTOR																		
APP/DEPART																		



AM
7:00 AM
7:15 AM
7:30 AM
7:45 AM
8:00 AM
8:15 AM
8:30 AM
8:45 AM
TOTAL

PEDESTRIAN + BIKE CROSSINGS					
	N SIDE	S SIDE	E SIDE	W SIDE	TOTAL
7:00 AM	0	1	0	0	1
7:15 AM	0	1	4	0	5
7:30 AM	0	2	4	0	6
7:45 AM	0	4	2	1	7
8:00 AM	2	3	1	0	6
8:15 AM	1	4	1	3	9
8:30 AM	0	0	1	0	1
8:45 AM	0	1	2	0	3
TOTAL	3	16	15	4	38

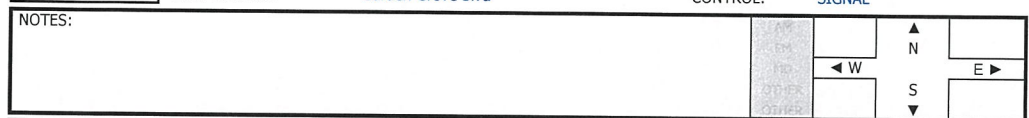
PEDESTRIAN CROSSINGS					
	N SIDE	S SIDE	E SIDE	W SIDE	TOTAL
7:00 AM	0	1	0	0	1
7:15 AM	0	1	4	0	5
7:30 AM	0	2	4	0	6
7:45 AM	0	4	2	1	7
8:00 AM	2	3	1	0	6
8:15 AM	1	4	1	3	9
8:30 AM	0	0	1	0	1
8:45 AM	0	1	2	0	3
TOTAL	3	16	15	4	38

BICYCLE CROSSINGS					
	NS	SS	ES	WS	TOTAL
7:00 AM	0	0	0	0	0
7:15 AM	0	0	0	0	0
7:30 AM	0	0	0	0	0
7:45 AM	0	0	0	0	0
8:00 AM	0	0	0	0	0
8:15 AM	0	0	0	0	0
8:30 AM	0	0	0	0	0
8:45 AM	0	0	0	0	0
TOTAL	0	0	0	0	0

### INTERSECTION TURNING MOVEMENT COUNTS

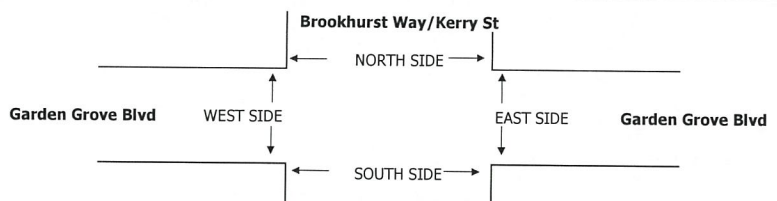
PREPARED BY:

<b>DATE:</b> Tue, Nov 19, 19	<b>LOCATION:</b> NORTH & SOUTH: EAST & WEST:	Garden Grove Brookhurst Way/Kerry St Garden Grove Blvd	<b>PROJECT #:</b> <b>LOCATION #:</b> <b>CONTROL:</b>	SC2085 0 SIGNAL
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	NORTHBOUND			SOUTHBOUND			EASTBOUND			WESTBOUND			TOTAL	
	Brookhurst Way/Kerry St			Brookhurst Way/Kerry St			Garden Grove Blvd			Garden Grove Blvd				
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR		
LANES:	0	0	0	0	0	0	0	0	0	0	0	0		
MIDDAY	12:00 PM	6	3	6	6	3	21	5	186	10	5	197	10	458
	12:15 PM	7	5	8	8	4	13	7	182	5	10	201	7	457
	12:30 PM	11	3	8	9	6	17	4	184	8	1	166	10	427
	12:45 PM	6	5	25	11	7	11	9	196	6	6	179	2	463
	1:00 PM	6	0	11	13	3	14	11	178	5	7	134	7	389
	1:15 PM	4	2	9	9	4	12	7	193	5	4	164	9	422
	1:30 PM	6	3	15	20	6	8	3	155	6	7	140	6	375
	1:45 PM	9	4	14	14	3	15	7	187	4	6	171	6	440
	VOLUMES	55	25	96	90	36	111	53	1,461	49	46	1,352	57	3,431
	APPROACH %	31%	14%	55%	38%	15%	47%	3%	93%	3%	3%	93%	4%	
APP/DEPART	176	/	135	237	/	131	1,563	/	1,647	1,455	/	1,518	0	
BEGIN PEAK HR	12:00 PM													
VOLUMES	30	16	47	34	20	62	25	748	29	22	743	29	1,805	
APPROACH %	32%	17%	51%	29%	17%	53%	3%	93%	4%	3%	94%	4%		
PEAK HR FACTOR	0.646			0.906			0.950			0.911			0.975	
APP/DEPART	93	/	70	116	/	71	802	/	829	794	/	835	0	

U-TURNS				
NB	SB	EB	WB	TTL
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0



MIDDAY	PEDESTRIAN + BIKE CROSSINGS					
	N SIDE	S SIDE	E SIDE	W SIDE	TOTAL	
	12:00 PM	0	3	0	2	5
	12:15 PM	0	1	2	1	4
	12:30 PM	0	3	1	1	5
	12:45 PM	1	1	2	1	5
	1:00 PM	1	3	1	1	6
	1:15 PM	0	1	0	1	2
1:30 PM	0	1	0	0	1	
1:45 PM	2	0	1	1	4	
TOTAL	4	13	7	8	32	

PEDESTRIAN CROSSINGS					
N SIDE	S SIDE	E SIDE	W SIDE	TOTAL	
12:00 PM	0	3	0	2	5
12:15 PM	0	1	2	1	4
12:30 PM	0	3	1	1	5
12:45 PM	1	1	2	1	5
1:00 PM	1	2	1	1	5
1:15 PM	0	1	0	1	2
1:30 PM	0	1	0	0	1
1:45 PM	2	0	1	1	4
TOTAL	4	12	7	8	31

BICYCLE CROSSINGS					
NS	SS	ES	WS	TOTAL	
12:00 PM	0	0	0	0	0
12:15 PM	0	0	0	0	0
12:30 PM	0	0	0	0	0
12:45 PM	0	0	0	0	0
1:00 PM	0	1	0	0	1
1:15 PM	0	0	0	0	0
1:30 PM	0	0	0	0	0
1:45 PM	0	0	0	0	0
TOTAL	0	1	0	0	1



### INTERSECTION TURNING MOVEMENT COUNTS

PREPARED BY:

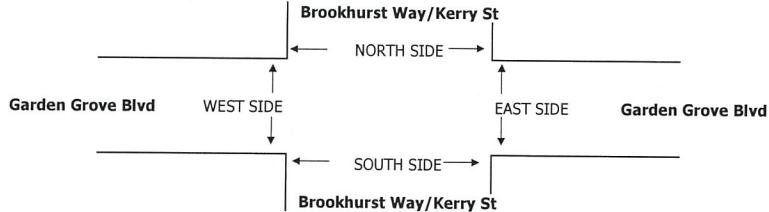
DATE: Tue, Nov 19, 19	LOCATION: NORTH & SOUTH: EAST & WEST:	Garden Grove Brookhurst Way/Kerry St Garden Grove Blvd	PROJECT #: LOCATION #: CONTROL:	SC2085 0 SIGNAL
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NOTES:	
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Add U-Turns to Left Turns

	NORTHBOUND Brookhurst Way/Kerry St			SOUTHBOUND Brookhurst Way/Kerry St			EASTBOUND Garden Grove Blvd			WESTBOUND Garden Grove Blvd			TOTAL
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
LANES:	0	0	0	0	0	0	0	0	0	0	0	0	
4:15 PM	8	2	12	4	4	12	10	245	8	7	203	8	523
4:30 PM	11	4	17	13	8	17	11	228	4	6	217	11	547
4:45 PM	9	6	11	14	7	8	6	242	8	9	236	16	572
5:00 PM	16	8	17	10	14	16	10	210	4	9	207	7	528
5:15 PM	8	11	17	15	1	14	9	247	4	6	200	17	549
5:30 PM	15	3	15	12	5	12	14	223	8	10	210	11	538
5:45 PM	16	6	22	9	4	12	16	279	8	7	195	11	585
6:00 PM	14	11	15	5	3	6	13	213	7	17	234	12	550
VOLUMES	97	51	126	82	46	97	89	1,887	51	71	1,702	93	4,392
APPROACH %	35%	19%	46%	36%	20%	43%	4%	93%	3%	4%	91%	5%	
APP/DEPART	274	/	233	225	/	168	2,027	/	2,095	1,866	/	1,896	0
BEGIN PEAK HR	5:15 PM												
VOLUMES	53	31	69	41	13	44	52	962	27	40	839	51	2,222
APPROACH %	35%	20%	45%	42%	13%	45%	5%	92%	3%	4%	90%	5%	
PEAK HR FACTOR	0.869			0.817			0.859			0.884			0.950
APP/DEPART	153	/	134	98	/	80	1,041	/	1,072	930	/	936	0

U-TURNS				
NB	SB	EB	WB	TTL
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0



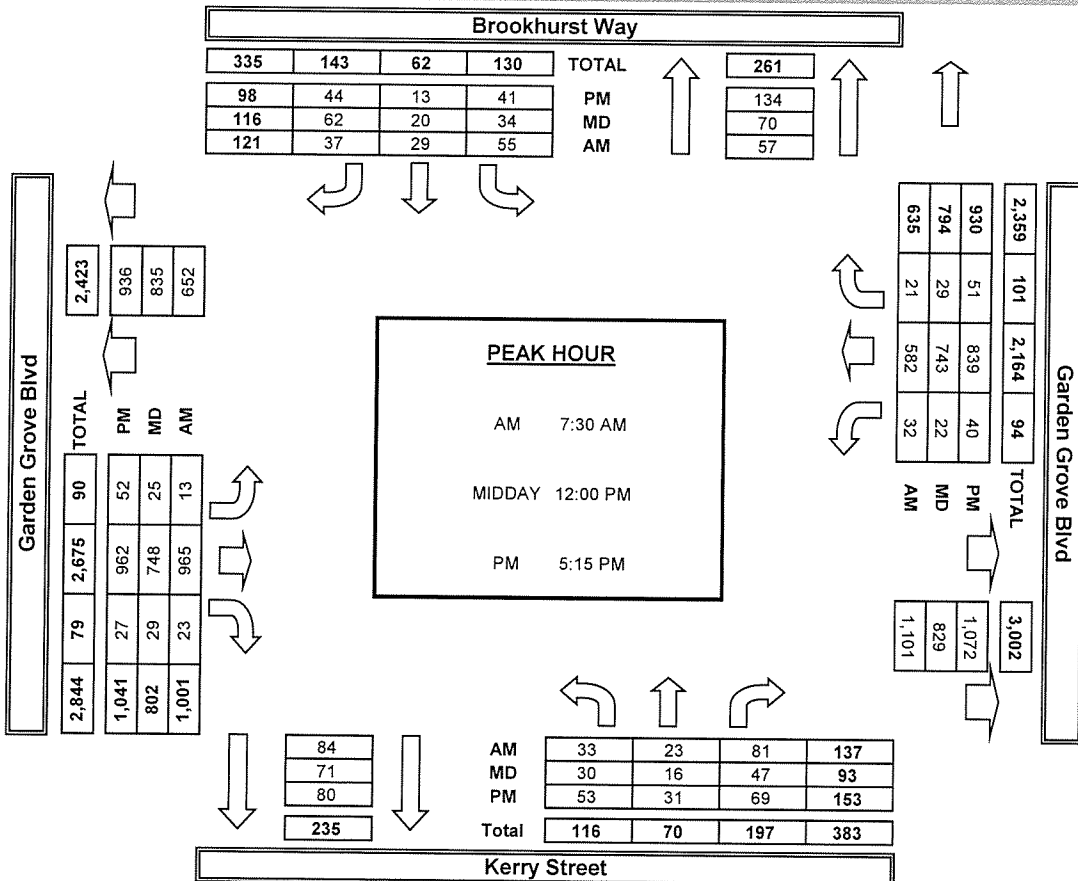
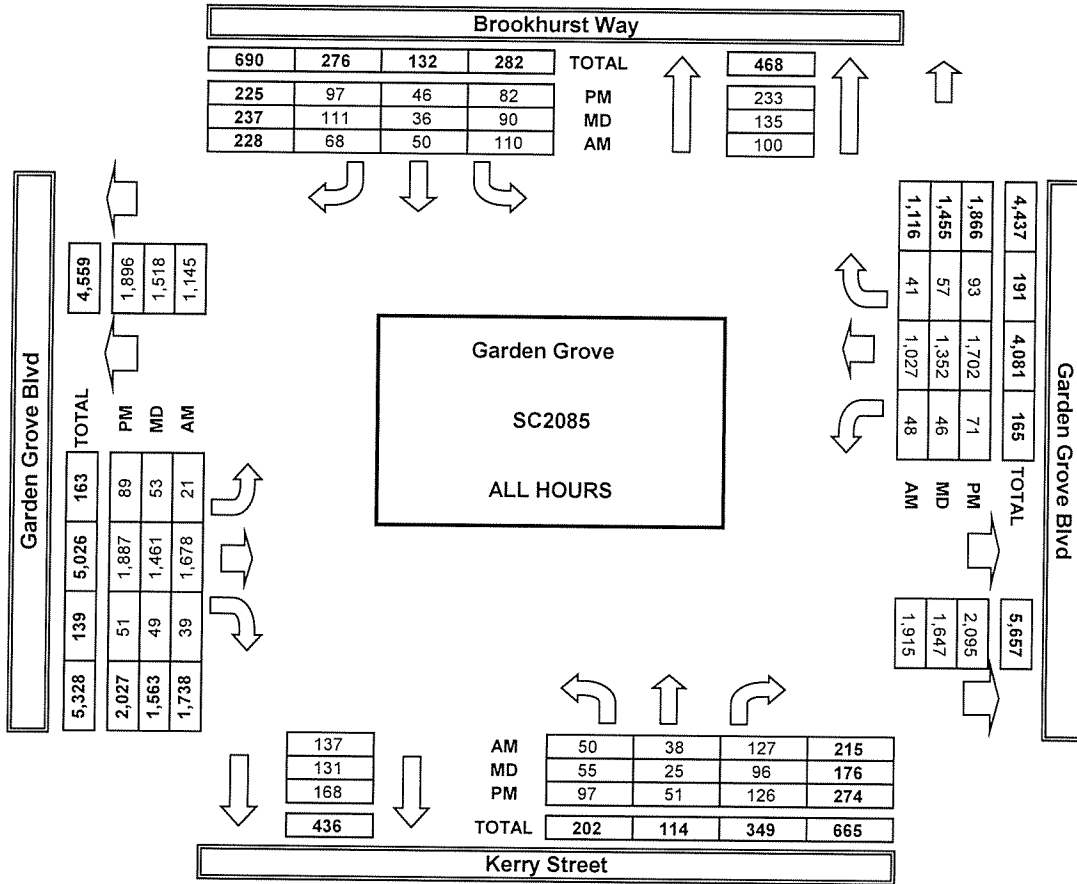
PM	4:15 PM
	4:30 PM
	4:45 PM
	5:00 PM
	5:15 PM
	5:30 PM
	5:45 PM
	6:00 PM
TOTAL	

PEDESTRIAN + BIKE CROSSINGS					
N SIDE	S SIDE	E SIDE	W SIDE	TOTAL	
2	2	1	0	5	
1	1	0	2	4	
2	0	0	0	2	
2	0	0	0	2	
0	0	0	0	0	
0	0	1	1	2	
1	0	0	0	1	
0	0	0	0	0	
TOTAL	8	3	2	3	16

PEDESTRIAN CROSSINGS					
N SIDE	S SIDE	E SIDE	W SIDE	TOTAL	
2	2	1	0	5	
1	0	0	2	3	
2	0	0	0	2	
2	0	0	0	2	
0	0	0	0	0	
0	0	1	1	2	
1	0	0	0	1	
0	0	0	0	0	
TOTAL	8	2	2	3	15

BICYCLE CROSSINGS					
NS	SS	ES	WS	TOTAL	
0	0	0	0	0	
0	1	0	0	1	
0	0	0	0	0	
0	0	0	0	0	
0	0	0	0	0	
0	0	0	0	0	
0	0	0	0	0	
0	0	0	0	0	
TOTAL	0	1	0	0	1

TURNING MOVEMENT COUNTS





### INTERSECTION TURNING MOVEMENT COUNTS

PREPARED BY:

<b>DATE:</b> Tue, Nov 19, 19	<b>LOCATION:</b> NORTH & SOUTH: EAST & WEST:	Garden Grove Brookhurst Street Garden Grove Blvd	<b>PROJECT #:</b> <b>LOCATION #:</b> <b>CONTROL:</b>	SC2085 0 SIGNAL
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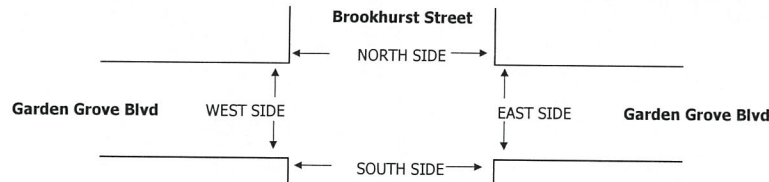
**NOTES:**

OTHER

Add U-Turns to Left Turns

	NORTHBOUND			SOUTHBOUND			EASTBOUND			WESTBOUND			TOTAL
	Brookhurst Street			Brookhurst Street			Garden Grove Blvd			Garden Grove Blvd			
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
<b>LANES:</b>	0	0	0	0	0	0	0	0	0	0	0	0	
<b>MIDDAY</b>													
12:00 PM	56	252	61	62	249	33	32	75	45	53	129	65	1,112
12:15 PM	60	206	72	51	252	24	24	127	59	63	134	54	1,126
12:30 PM	45	239	49	56	241	21	34	98	64	54	112	58	1,071
12:45 PM	34	255	59	48	196	18	42	106	62	61	144	35	1,060
1:00 PM	40	244	70	45	218	18	38	116	66	53	103	48	1,059
1:15 PM	48	235	62	41	227	26	31	110	61	51	102	46	1,040
1:30 PM	45	269	70	49	216	11	29	106	56	47	107	50	1,055
1:45 PM	35	231	71	46	243	27	32	120	57	40	112	68	1,082
VOLUMES	363	1,931	514	398	1,842	178	262	858	470	422	943	424	8,605
APPROACH %	13%	69%	18%	16%	76%	7%	16%	54%	30%	24%	53%	24%	
APP/DEPART	2,808	/	2,617	2,418	/	2,734	1,590	/	1,770	1,789	/	1,484	0
BEGIN PEAK HR	12:00 PM												
VOLUMES	195	952	241	217	938	96	132	406	230	231	519	212	4,369
APPROACH %	14%	69%	17%	17%	75%	8%	17%	53%	30%	24%	54%	22%	
PEAK HR FACTOR	0.940			0.909			0.914			0.958			0.970
APP/DEPART	1,388	/	1,296	1,251	/	1,399	768	/	864	962	/	810	0

U-TURNS				
NB	SB	EB	WB	TTL
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0



MIDDAY	PEDESTRIAN + BIKE CROSSINGS				
	N SIDE	S SIDE	E SIDE	W SIDE	TOTAL
	0	3	1	2	6
	1	1	2	0	4
	0	2	4	2	8
	2	2	3	2	9
	0	2	1	0	3
	1	2	1	2	6
2	1	1	2	6	
3	1	3	5	12	
<b>TOTAL</b>	<b>9</b>	<b>14</b>	<b>16</b>	<b>15</b>	<b>54</b>

MIDDAY	PEDESTRIAN CROSSINGS				
	N SIDE	S SIDE	E SIDE	W SIDE	TOTAL
	0	3	1	2	6
	1	1	2	0	4
	0	2	4	2	8
	2	2	3	2	9
	0	2	1	0	3
	1	2	1	1	5
2	1	1	2	6	
3	1	3	5	12	
<b>TOTAL</b>	<b>9</b>	<b>14</b>	<b>16</b>	<b>14</b>	<b>53</b>

MIDDAY	BICYCLE CROSSINGS				
	NS	SS	ES	WS	TOTAL
	0	0	0	0	0
	0	0	0	0	0
	0	0	0	0	0
	0	0	0	0	0
	0	0	0	0	0
	0	0	0	1	1
0	0	0	0	0	
0	0	0	0	0	
<b>TOTAL</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1</b>	<b>1</b>

### INTERSECTION TURNING MOVEMENT COUNTS

PREPARED BY:

<b>DATE:</b> Tue, Nov 19, 19	<b>LOCATION:</b> NORTH & SOUTH: Garden Grove East & West: Brookhurst Street Garden Grove Blvd	<b>PROJECT #:</b> SC2085 <b>LOCATION #:</b> 0 <b>CONTROL:</b> SIGNAL
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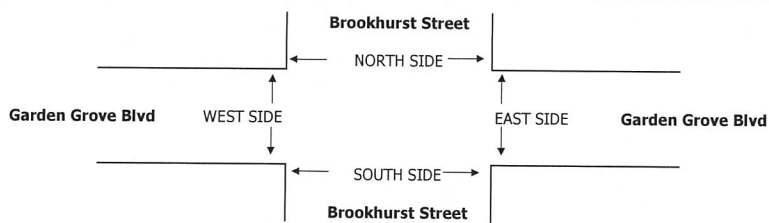
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Add U-Turns to Left Turns

	NORTHBOUND Brookhurst Street			SOUTHBOUND Brookhurst Street			EASTBOUND Garden Grove Blvd			WESTBOUND Garden Grove Blvd			TOTAL
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
LANES:	0	0	0	0	0	0	0	0	0	0	0	0	
4:15 PM	53	318	70	71	255	25	43	151	52	55	145	60	1,298
4:30 PM	40	333	86	56	225	27	51	158	55	52	156	67	1,306
4:45 PM	46	301	75	52	230	25	31	145	57	53	190	66	1,271
5:00 PM	41	301	67	59	257	28	52	140	64	63	161	53	1,286
5:15 PM	45	310	89	63	246	24	36	154	50	63	155	69	1,304
5:30 PM	47	307	86	59	292	28	43	144	64	61	166	62	1,359
5:45 PM	51	335	84	48	244	26	40	202	55	56	147	48	1,336
6:00 PM	58	327	71	52	247	30	40	149	41	46	163	68	1,292
VOLUMES	381	2,532	628	460	1,996	213	336	1,243	438	449	1,283	493	10,452
APPROACH %	11%	72%	18%	17%	75%	8%	17%	62%	22%	20%	58%	22%	
APP/DEPART	3,541	/	3,361	2,669	/	2,883	2,017	/	2,331	2,225	/	1,877	0
BEGIN PEAK HR	5:15 PM												
VOLUMES	201	1,279	330	222	1,029	108	159	649	210	226	631	247	5,291
APPROACH %	11%	71%	18%	16%	76%	8%	16%	64%	21%	20%	57%	22%	
PEAK HR FACTOR	0.963												
APP/DEPART	1,810	/	1,685	1,359	/	1,465	1,018	/	1,201	1,104	/	940	0

U-TURNS				
NB	SB	EB	WB	TTL
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0



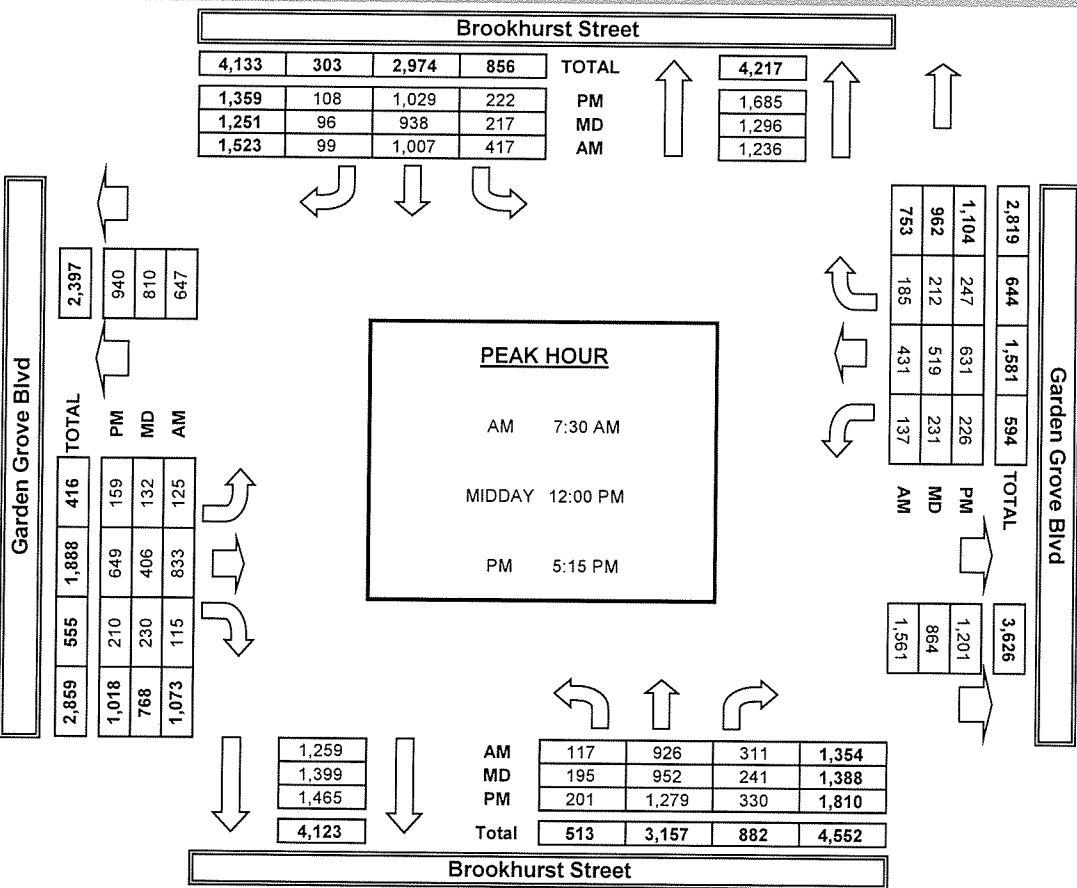
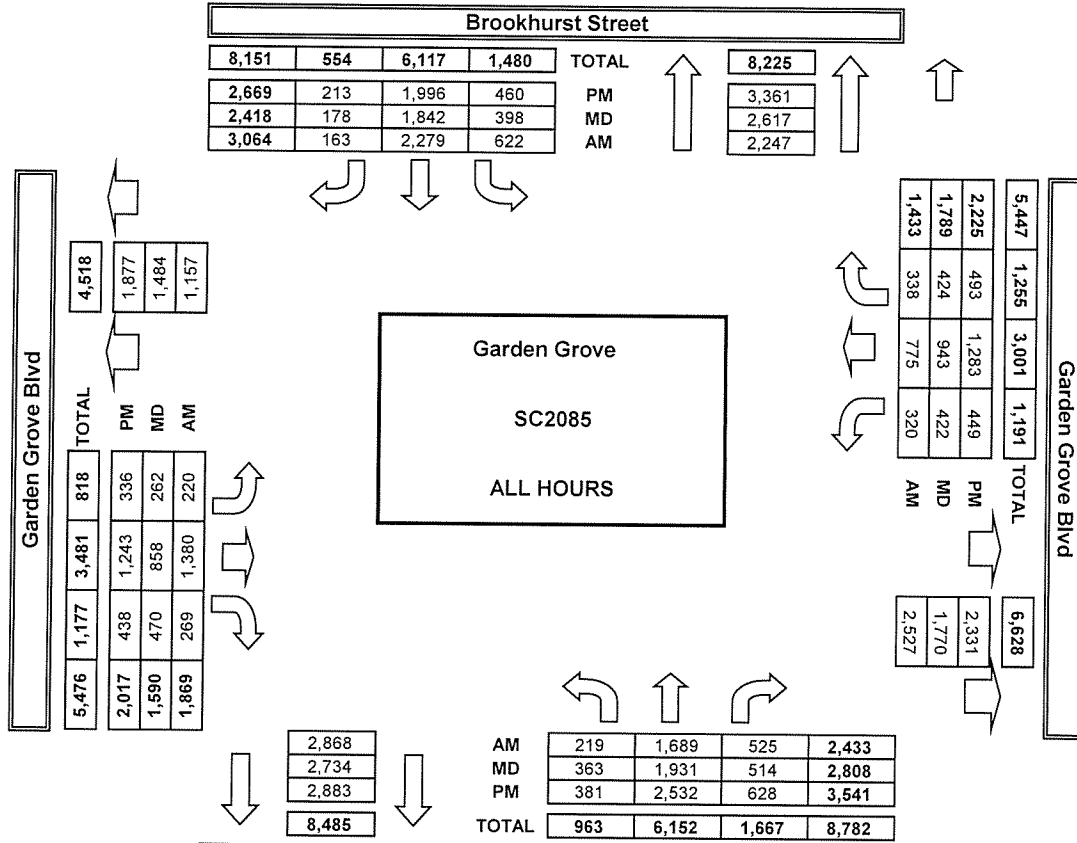
PM	
4:15 PM	
4:30 PM	
4:45 PM	
5:00 PM	
5:15 PM	
5:30 PM	
5:45 PM	
6:00 PM	
TOTAL	

PEDESTRIAN + BIKE CROSSINGS				
N SIDE	S SIDE	E SIDE	W SIDE	TOTAL
3	7	5	3	18
3	3	2	1	9
0	2	5	2	9
1	2	3	2	8
0	3	3	0	6
1	5	5	0	11
1	0	0	1	2
1	1	3	0	5
10	23	26	9	68

PEDESTRIAN CROSSINGS				
N SIDE	S SIDE	E SIDE	W SIDE	TOTAL
3	7	5	3	18
3	3	2	1	9
0	2	5	2	9
1	2	3	1	7
0	3	3	0	6
1	5	5	0	11
1	0	0	1	2
1	1	3	0	5
10	23	26	8	67

BICYCLE CROSSINGS				
NS	SS	ES	WS	TOTAL
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	1	1
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	1	1

TURNING MOVEMENT COUNTS



### INTERSECTION TURNING MOVEMENT COUNTS

PREPARED BY:

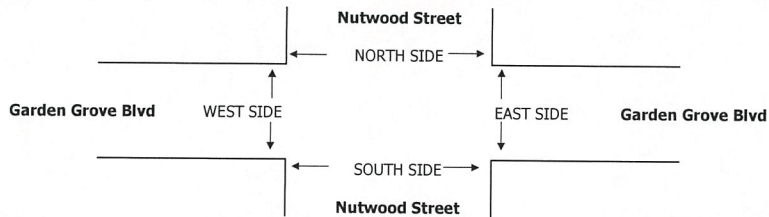
<b>DATE:</b> Tue, Nov 19, 19	<b>LOCATION:</b> NORTH & SOUTH: EAST & WEST:	Garden Grove Nutwood Street Garden Grove Blvd	<b>PROJECT #:</b> SC2085 <b>LOCATION #:</b> <b>CONTROL:</b> SIGNAL
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NOTES:

Add U-Turns to Left Turns

	NORTHBOUND Nutwood Street			SOUTHBOUND Nutwood Street			EASTBOUND Garden Grove Blvd			WESTBOUND Garden Grove Blvd			TOTAL
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
LANES:	0	0	0	0	0	0	0	0	0	0	0	0	
7:00 AM	1	0	0	32	1	28	7	163	0	2	119	6	359
7:15 AM	1	1	1	45	0	30	14	288	1	1	111	21	514
7:30 AM	0	2	0	67	0	30	18	412	0	0	149	16	694
7:45 AM	0	0	0	71	2	17	27	457	2	4	187	35	802
8:00 AM	1	0	0	59	3	32	26	391	0	3	192	37	744
8:15 AM	0	0	0	54	0	25	24	292	1	1	166	34	597
8:30 AM	1	0	2	42	0	23	23	243	0	1	159	25	519
8:45 AM	1	0	0	41	0	38	28	228	0	0	145	26	507
VOLUMES	5	3	3	411	6	223	167	2,474	4	12	1,228	200	4,736
APPROACH %	45%	27%	27%	64%	1%	35%	6%	94%	0%	1%	85%	14%	
APP/DEPART	11	/	370	640	/	22	2,645	/	2,888	1,440	/	1,456	0
BEGIN PEAK HR	7:30 AM												
VOLUMES	1	2	0	251	5	104	95	1,552	3	8	694	122	2,837
APPROACH %	33%	67%	0%	70%	1%	29%	6%	94%	0%	1%	84%	15%	
PEAK HR FACTOR	0.375			0.928			0.849			0.888			0.884
APP/DEPART	3	/	219	360	/	16	1,650	/	1,803	824	/	799	0

U-TURNS				
NB	SB	EB	WB	TTL
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0



AM	7:00 AM	7:15 AM	7:30 AM	7:45 AM	8:00 AM	8:15 AM	8:30 AM	8:45 AM	TOTAL
	1	1	0	0	0	0	0	0	1
	1	1	2	0	1	0	0	0	5
	0	0	2	0	10	7	5	6	31
	0	0	4	0	10	7	5	6	35
	0	0	10	0	2	1	1	2	16
	0	0	7	0	7	0	0	0	14
	0	0	5	0	7	0	0	0	12
	0	0	6	0	6	0	0	0	12
	1	1	11	0	12	7	6	8	46

PEDESTRIAN + BIKE CROSSINGS				
N SIDE	S SIDE	E SIDE	W SIDE	TOTAL
1	5	0	4	10
1	3	1	0	5
2	8	0	5	15
3	4	0	0	7
2	10	0	2	14
0	7	0	1	8
0	5	0	1	6
0	6	0	2	8
9	48	1	15	73

PEDESTRIAN CROSSINGS				
N SIDE	S SIDE	E SIDE	W SIDE	TOTAL
1	5	0	4	10
0	3	1	0	4
2	8	0	5	15
3	4	0	0	7
2	10	0	2	14
0	7	0	1	8
0	5	0	1	6
0	6	0	2	8
8	48	1	15	72

BICYCLE CROSSINGS				
NS	SS	ES	WS	TOTAL
0	0	0	0	0
1	0	0	0	1
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
1	0	0	0	1

### INTERSECTION TURNING MOVEMENT COUNTS

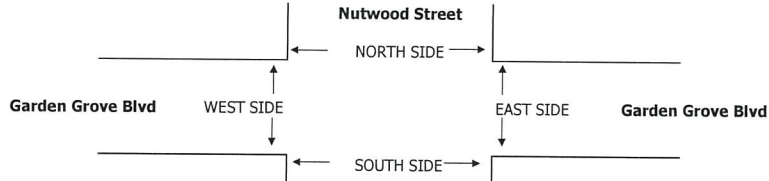
PREPARED BY:

<b>DATE:</b> Tue, Nov 19, 19	<b>LOCATION:</b> Garden Grove NORTH & SOUTH: Nutwood Street EAST & WEST: Garden Grove Blvd	<b>PROJECT #:</b> SC2085 <b>LOCATION #:</b> 0 <b>CONTROL:</b> SIGNAL	
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NOTES:	
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Add U-Turns to Left Turns

	NORTHBOUND Nutwood Street			SOUTHBOUND Nutwood Street			EASTBOUND Garden Grove Blvd			WESTBOUND Garden Grove Blvd			TOTAL	U-TURNS					
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR		NB	SB	EB	WB	TTL	
<b>LANES:</b>	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
<b>MIDDAY</b>	12:00 PM	1	1	2	30	2	34	26	183	0	1	215	27	522	0	0	0	0	0
	12:15 PM	0	1	0	29	0	23	28	228	0	0	240	24	573	0	0	0	0	0
	12:30 PM	0	0	0	38	0	24	26	186	0	2	218	16	510	0	0	0	0	0
	12:45 PM	1	0	3	24	0	30	16	206	0	1	208	25	514	0	0	0	0	0
	1:00 PM	0	2	2	24	0	19	20	205	1	2	196	28	499	0	0	0	0	0
	1:15 PM	1	0	2	26	1	23	23	201	0	3	184	26	490	0	0	0	0	0
	1:30 PM	0	1	0	32	1	29	32	199	0	1	175	21	491	0	0	0	0	0
	1:45 PM	4	0	4	36	0	38	26	193	0	1	188	29	519	0	0	0	0	0
	<b>VOLUMES</b>	7	5	13	239	4	220	197	1,601	1	11	1,624	196	4,118	0	0	0	0	0
	<b>APPROACH %</b>	28%	20%	52%	52%	1%	48%	11%	89%	0%	1%	89%	11%						
<b>APP/DEPART</b>	25	/	398	463	/	16	1,799	/	1,853	1,831	/	1,851	0						
<b>BEGIN PEAK HR</b>	12:00 PM																		
<b>VOLUMES</b>	2	2	5	121	2	111	96	803	0	4	881	92	2,119						
<b>APPROACH %</b>	22%	22%	56%	52%	1%	47%	11%	89%	0%	0%	90%	9%							
<b>PEAK HR FACTOR</b>	0.563			0.886			0.878			0.925			0.925						
<b>APP/DEPART</b>	9	/	190	234	/	6	899	/	929	977	/	994	0						



<b>MIDDAY</b>	12:00 PM	4	2	0	2	8
	12:15 PM	1	1	0	0	2
	12:30 PM	1	3	0	1	5
	12:45 PM	3	6	0	0	9
	1:00 PM	0	0	0	0	0
	1:15 PM	0	2	0	0	2
	1:30 PM	2	4	0	1	7
	1:45 PM	2	5	0	7	14
<b>TOTAL</b>	13	23	0	11	47	

Nutwood Street					
PEDESTRIAN + BIKE CROSSINGS					
N SIDE	S SIDE	E SIDE	W SIDE	TOTAL	
4	2	0	2	8	
1	1	0	0	2	
1	3	0	1	5	
3	6	0	0	9	
0	0	0	0	0	
0	2	0	0	2	
2	4	0	1	7	
2	5	0	7	14	
13	23	0	11	47	

PEDESTRIAN CROSSINGS					
N SIDE	S SIDE	E SIDE	W SIDE	TOTAL	
4	2	0	2	8	
1	1	0	0	2	
1	3	0	1	5	
0	5	0	0	5	
0	0	0	0	0	
0	2	0	0	2	
2	4	0	1	7	
2	5	0	7	14	
10	22	0	11	43	

BICYCLE CROSSINGS					
NS	SS	ES	WS	TOTAL	
0	0	0	0	0	
0	0	0	0	0	
0	0	0	0	0	
3	1	0	0	4	
0	0	0	0	0	
0	0	0	0	0	
0	0	0	0	0	
0	0	0	0	0	
3	1	0	0	4	



### INTERSECTION TURNING MOVEMENT COUNTS

PREPARED BY:

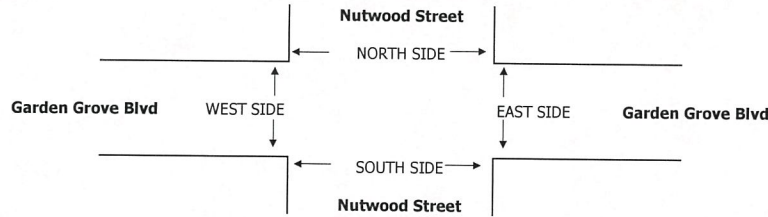
DATE: Tue, Nov 19, 19  
 LOCATION: Garden Grove  
 NORTH & SOUTH: Nutwood Street  
 EAST & WEST: Garden Grove Blvd  
 PROJECT #: SC2085  
 LOCATION #: 0  
 CONTROL: SIGNAL

NOTES:

Add U-Turns to Left Turns

LANES:	NORTHBOUND Nutwood Street			SOUTHBOUND Nutwood Street			EASTBOUND Garden Grove Blvd			WESTBOUND Garden Grove Blvd			TOTAL	
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR		
4:15 PM	0	5	6	36	1	26	32	246	0	4	243	33	632	
4:30 PM	0	0	1	40	1	32	44	262	0	1	241	33	655	
4:45 PM	0	0	2	30	0	35	42	234	0	2	306	43	694	
5:00 PM	1	3	1	42	0	31	29	249	0	3	259	36	654	
5:15 PM	2	2	1	27	0	35	48	253	0	0	263	30	661	
5:30 PM	3	2	2	33	1	37	41	252	0	1	274	24	670	
5:45 PM	0	0	1	24	1	28	48	303	0	0	248	32	685	
6:00 PM	1	0	0	26	0	24	45	252	0	1	264	30	643	
VOLUMES	7	12	14	258	4	248	329	2,051	0	12	2,098	261	5,294	
APPROACH %	21%	36%	42%	51%	1%	49%	14%	86%	0%	1%	88%	11%		
APP/DEPART	33	/	602	510	/	16	2,380	/	2,323	2,371	/	2,353	0	
BEGIN PEAK HR	4:45 PM													
VOLUMES	6	7	6	132	1	138	160	988	0	6	1,102	133	2,679	
APPROACH %	32%	37%	32%	49%	0%	51%	14%	86%	0%	6	89%	11%		
PEAK HR FACTOR	0.679			0.928						0.953			0.884	0.965
APP/DEPART	19	/	300	271	/	7	1,148	/	1,126	1,241	/	1,246	0	

U-TURNS				
NB	SB	EB	WB	TTL
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0



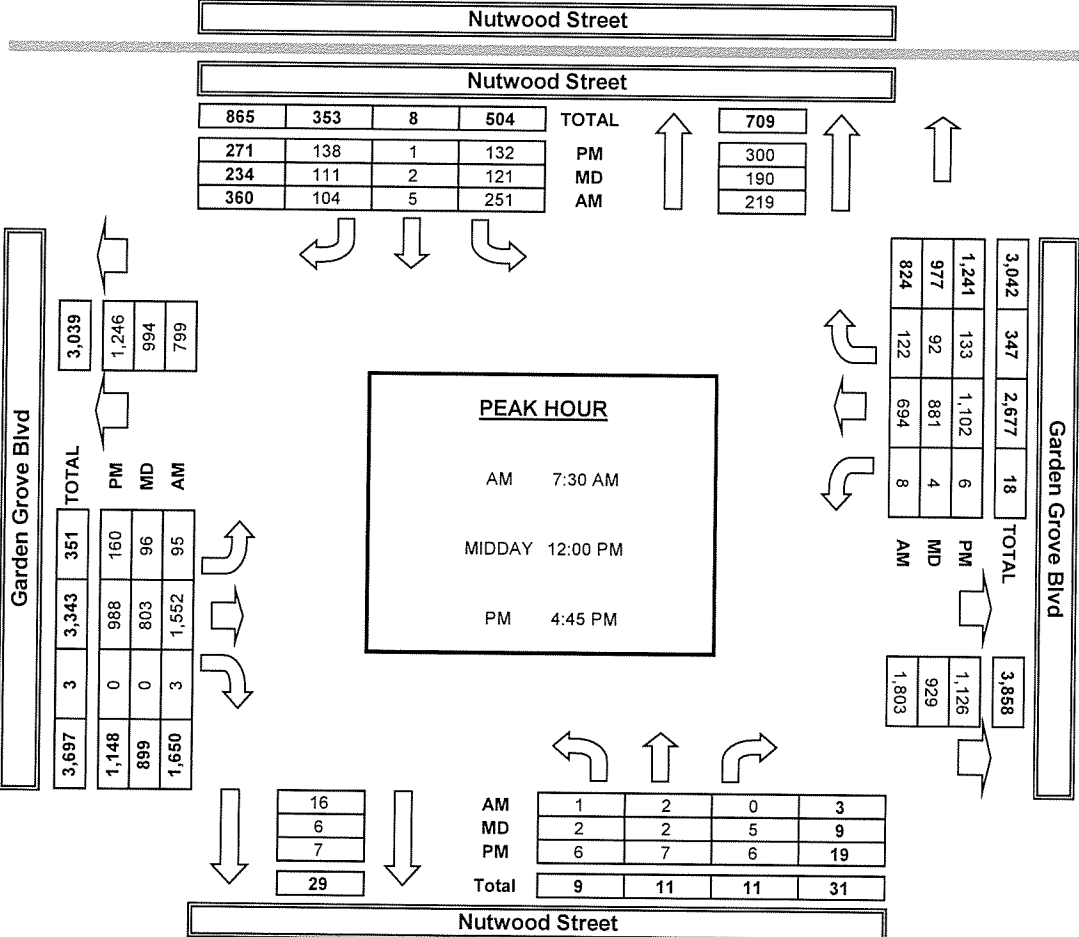
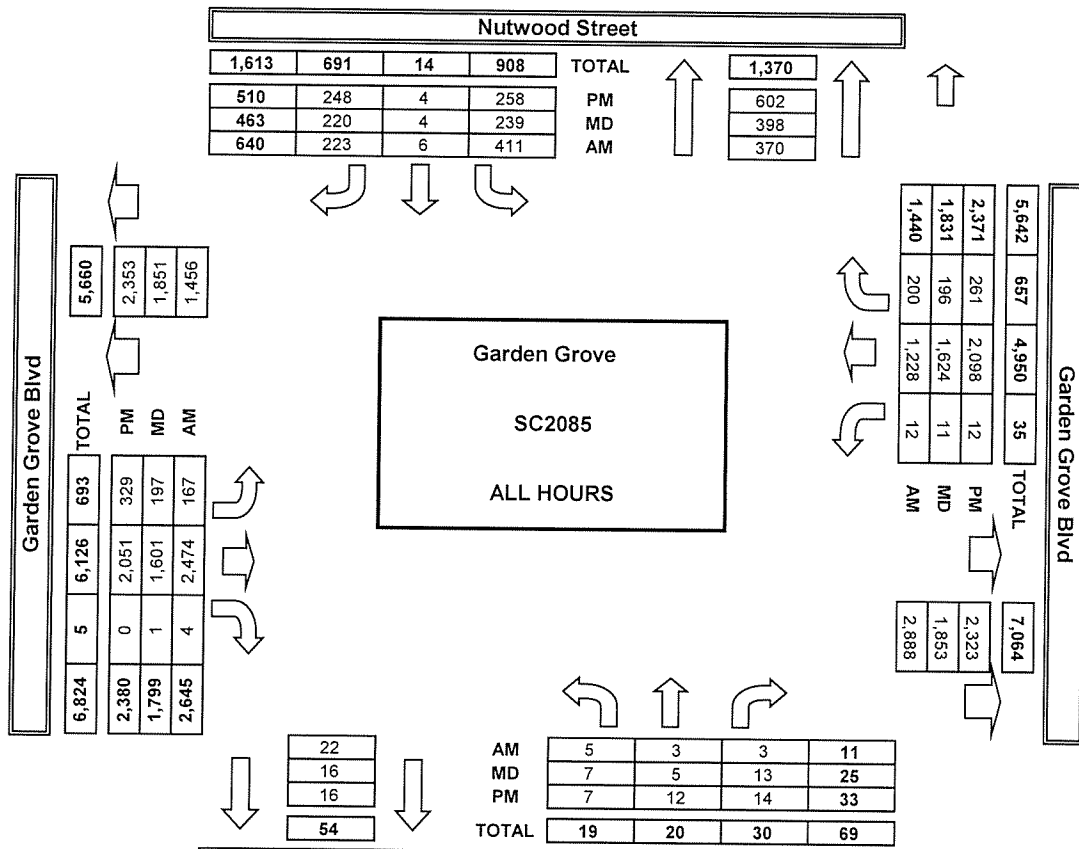
PM	4:15 PM	4:30 PM	4:45 PM	5:00 PM	5:15 PM	5:30 PM	5:45 PM	6:00 PM	TOTAL
	3	2	0	2	0	1	0	1	9
	6	3	3	2	3	5	3	2	27
	0	0	0	0	0	0	0	0	0
	2	0	0	2	0	0	0	0	12
	0	0	0	0	0	0	0	0	0
	0	0	0	0	0	0	0	0	0
	1	0	0	0	0	0	0	0	1
	0	0	0	0	0	0	0	0	0
	1	0	0	0	0	0	0	0	1
	9	27	0	12	0	0	0	0	48

PEDESTRIAN + BIKE CROSSINGS				
N SIDE	S SIDE	E SIDE	W SIDE	TOTAL
3	6	0	2	11
2	3	0	4	9
0	3	0	0	3
2	2	0	2	6
0	3	0	2	5
1	5	0	0	6
0	3	0	0	3
1	2	0	2	5
9	27	0	12	48

PEDESTRIAN CROSSINGS				
N SIDE	S SIDE	E SIDE	W SIDE	TOTAL
3	6	0	2	11
2	3	0	4	9
0	3	0	0	3
2	2	0	2	6
0	2	0	2	4
1	5	0	0	6
0	3	0	0	3
1	2	0	2	5
9	26	0	12	47

BICYCLE CROSSINGS				
NS	SS	ES	WS	TOTAL
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	1	0	0	1
0	0	0	0	0
0	0	0	0	0
0	0	0	0	0
0	1	0	0	1

TURNING MOVEMENT COUNTS



National Data & Surveying Services

Intersection Turning Movement Count

Location: Brookhurst St & Stanford Ave
City: Garden Grove
Control: Signalized

Project ID: 18-01218-4161
Date: 10/30/2018

Total

Table with columns for NS/EW Streets (Brookhurst St, Stanford Ave) and rows for AM, NOON, and PM periods. It includes sub-columns for Northbound, Southbound, Eastbound, and Westbound movements (NL, NT, NR, NU, SL, ST, SR, SU, EL, ET, ER, EU, WL, WT, WR, WU). Rows include hourly counts, total volumes, approach percentages, and peak hour data.

National Data & Surveying Services

Intersection Turning Movement Count

Location: Brookhurst St & Trask Ave  
 City: Garden Grove  
 Control: Signalized

Project ID: 18-01182-4246  
 Date: 9/26/2018

Total

NS/EW Streets:		Brookhurst St				Brookhurst St				Trask Ave				Trask Ave				TOTAL
		NORTHBOUND				SOUTHBOUND				EASTBOUND				WESTBOUND				
AM		2 NL	3 NT	1 NR	0 NU	2 SL	3 ST	1 SR	0 SU	1 EL	2 ET	1 ER	0 EU	1 WL	2 WT	0 WR	0 WU	
6:45 AM		65	144	20	0	16	247	46	6	23	26	70	0	27	43	10	0	
7:00 AM		43	156	39	0	29	357	57	3	32	43	86	0	40	65	11	0	
7:15 AM		54	193	59	0	39	342	47	6	39	47	119	0	51	82	10	0	
7:30 AM		80	256	81	0	25	279	31	3	43	92	113	0	68	105	16	0	
7:45 AM		74	269	80	0	22	285	25	2	55	92	107	0	49	90	28	0	
8:00 AM		85	322	87	0	30	244	40	1	37	104	80	0	61	80	27	0	
8:15 AM		69	313	87	1	28	327	60	5	58	112	102	0	50	69	22	0	
8:30 AM		76	283	86	0	26	295	51	1	40	77	99	0	40	91	21	0	
TOTAL VOLUMES :		NL	NT	NR	NU	SL	ST	SR	SU	EL	ET	ER	EU	WL	WT	WR	WU	
APPROACH %'s :		18.07%	1936	539	1	215	2376	357	27	327	593	776	0	386	625	145	0	
PEAK HR :		07:30 AM - 08:30 AM																
PEAK HR VOL :		308	1160	335	1	105	1135	156	11	193	400	402	0	228	344	93	0	
PEAK HR FACTOR :		0.906	0.901	0.963	0.250	0.875	0.868	0.650	0.550	0.832	0.893	0.889	0.000	0.838	0.819	0.830	0.000	
		0.913				0.838				0.915				0.880				
		TOTAL																
		4871																
		0.935																
NOON		2 NL	3 NT	1 NR	0 NU	2 SL	3 ST	1 SR	0 SU	1 EL	2 ET	1 ER	0 EU	1 WL	2 WT	0 WR	0 WU	
12:30 PM		64	300	44	1	20	289	53	3	48	39	112	0	41	152	23	0	
12:45 PM		88	282	51	0	18	236	45	4	62	49	121	0	48	143	23	0	
1:00 PM		75	290	54	0	18	288	54	3	52	57	130	0	35	154	32	0	
1:15 PM		78	308	37	1	15	273	61	4	68	47	127	0	39	147	21	0	
1:30 PM		76	315	41	0	28	309	56	6	76	46	145	0	48	142	17	0	
1:45 PM		83	307	43	0	23	298	76	3	67	39	152	0	46	136	36	0	
2:00 PM		87	329	36	0	15	281	63	8	58	52	144	0	54	96	18	0	
2:15 PM		74	359	56	3	15	289	61	2	88	69	143	0	46	57	15	0	
TOTAL VOLUMES :		NL	NT	NR	NU	SL	ST	SR	SU	EL	ET	ER	EU	WL	WT	WR	WU	
APPROACH %'s :		625	2490	362	5	152	2263	469	33	519	398	1074	0	357	1027	185	0	
PEAK HR :		01:30 PM - 02:30 PM																
PEAK HR VOL :		320	1310	176	3	81	1177	256	19	289	206	584	0	194	431	86	0	
PEAK HR FACTOR :		0.920	0.912	0.786	0.250	0.723	0.952	0.842	0.594	0.821	0.746	0.961	0.000	0.898	0.759	0.597	0.000	
		0.919				0.958				0.899				0.815				
		TOTAL																
		5132																
		0.980																
PM		2 NL	3 NT	1 NR	0 NU	2 SL	3 ST	1 SR	0 SU	1 EL	2 ET	1 ER	0 EU	1 WL	2 WT	0 WR	0 WU	
4:15 PM		90	407	49	1	26	228	62	4	57	74	137	0	42	79	25	0	
4:30 PM		61	408	59	0	27	294	70	4	54	61	118	0	45	76	27	0	
4:45 PM		51	416	56	0	22	256	59	3	61	58	143	0	46	84	29	0	
5:00 PM		77	426	56	0	25	334	68	1	60	63	115	0	36	83	41	0	
5:15 PM		69	435	59	0	33	285	56	0	56	54	165	0	44	115	55	0	
5:30 PM		91	454	53	0	16	302	67	4	68	66	153	0	53	81	33	0	
5:45 PM		74	447	61	1	29	290	55	5	69	66	137	0	48	97	32	0	
6:00 PM		67	433	79	0	24	297	80	9	64	56	135	0	32	88	32	0	
TOTAL VOLUMES :		NL	NT	NR	NU	SL	ST	SR	SU	EL	ET	ER	EU	WL	WT	WR	WU	
APPROACH %'s :		580	3426	472	2	202	2286	517	30	489	498	1103	0	346	703	274	0	
PEAK HR :		05:15 PM - 06:15 PM																
PEAK HR VOL :		301	1769	252	1	102	1174	258	18	257	242	590	0	177	381	152	0	
PEAK HR FACTOR :		0.827	0.974	0.797	0.250	0.773	0.972	0.806	0.500	0.931	0.917	0.894	0.000	0.835	0.828	0.691	0.000	
		0.971				0.946				0.949				0.829				
		TOTAL																
		5674																
		0.984																

**APPENDIX C**  
**LEVEL OF SERVICE ANALYSIS**

**Intersection Capacity Utilization Analysis (ICU)**

Traffic Scenario: **Existing**  
 Intersection # **1**  
 Project: Garden Grove Mixed-Use  
 North/South St: Brookhurst Way/Kerry St  
 East/West St: Garden Grove Blvd

Date: 11/4/20  
 By: KH

Movement	No. of Lanes	Critical Lane Capacity	A.M. Peak Hour				P.M. Peak Hour			
			Volumes				Volumes			
			Total	Critical Lane	V/C Ratio	Critical V/C	Total	Critical Lane	V/C Ratio	Critical V/C
Northbound :Left	1.0	1700	33	33	0.019		54	54	0.032	
Northbound :Thru	1.0	1700	23	105	0.062	0.062	31	101	0.059	0.059
Northbound Right:		1700	82				70			
Southbound :Left	1.0	1700	56	56	0.033	0.033	41	41	0.024	0.024
Southbound :Thru	1.0	1700	29	66	0.039		13	57	0.034	
Southbound Right:		1700	37				44			
Eastbound :Left	1.0	1700	13	13	0.008		53	53	0.031	
Eastbound :Thru	3.0	1700	975	333	0.196	0.196	972	333	0.196	0.196
Eastbound Right:		1700	23				27			
Westbound :Left	1.0	1700	32	32	0.019	0.019	40	40	0.024	0.024
Westbound :Thru	3.0	1700	588	203	0.119		847	300	0.176	
Westbound Right:		1700	21				52			
Sum of Critical V/C Ratios						0.310	0.303			
Adjustments for Lost Time						0.05	0.05			
<b>Intersection Capacity Utilization (ICU)</b>						<b>0.360</b>	<b>0.353</b>			
<b>Level of Service (LOS)</b>						<b>A</b>	<b>A</b>			

NOTES:

Level of Service (LOS)	
A	0.00 ~ 0.60
B	0.601 ~ 0.70
C	0.701 ~ 0.80
D	0.801 ~ 0.90
E	0.901 ~ 1.00
F	1.00+

Critical Lane Flow Factors		
0.5	Lanes:	2.00
1	Lane:	1.00
1.5	Lanes:	0.67
2	Lanes:	0.50
2.5	Lanes:	0.40
3	Lanes:	0.33

**Intersection Capacity Utilization Analysis (ICU)**

Traffic Scenario: **Existing + Project**

Intersection # 1

Project: Garden Grove Mixed-Use

North/South St: Brookhurst Way/Kerry St

East/West St: Garden Grove Blvd

Date: 11/4/20

By: KH

Movement	No. of Lanes	Critical Lane Capacity	A.M. Peak Hour				P.M. Peak Hour				
			Volumes				Volumes				
			Total	Critical Lane	V/C Ratio	Critical V/C	Total	Critical Lane	V/C Ratio	Critical V/C	
Northbound :Left	1.0	1700	33	33	0.019		54	54	0.032		
Northbound :Thru	1.0	1700	23	106	0.063	0.063	31	103	0.060	0.060	
Northbound Right:		1700	83				72				
Southbound :Left	1.0	1700	56	56	0.033	0.033	41	41	0.024	0.024	
Southbound :Thru	1.0	1700	29	66	0.039		13	57	0.034		
Southbound Right:		1700	37				44				
Eastbound :Left	1.0	1700	13	13	0.008		53	53	0.031		
Eastbound :Thru	3.0	1700	981	335	0.197	0.197	979	335	0.197	0.197	
Eastbound Right:		1700	23				27				
Westbound :Left	1.0	1700	33	33	0.020	0.020	42	42	0.025	0.025	
Westbound :Thru	3.0	1700	593	205	0.120		855	302	0.178		
Westbound Right:		1700	21				52				
Sum of Critical V/C Ratios							0.313				0.306
Adjustments for Lost Time							0.05				0.05
<b>Intersection Capacity Utilization (ICU)</b>							<b>0.363</b>				<b>0.356</b>
<b>Level of Service (LOS)</b>							<b>A</b>				<b>A</b>

NOTES:

Level of Service (LOS)	
A	0.00 ~ 0.60
B	0.601 ~ 0.70
C	0.701 ~ 0.80
D	0.801 ~ 0.90
E	0.901 ~ 1.00
F	1.00+

Critical Lane Flow Factors		
0.5	Lanes:	2.00
1	Lane:	1.00
1.5	Lanes:	0.67
2	Lanes:	0.50
2.5	Lanes:	0.40
3	Lanes:	0.33

**Intersection Capacity Utilization Analysis (ICU)**

Traffic Scenario: **Existing + Growth + Cumulative**

Intersection # **1**

Project: Garden Grove Mixed-Use

North/South St: Brookhurst Way/Kerry St

East/West St: Garden Grove Blvd

Date: 11/4/20

By: KH

Movement	No. of Lanes	Critical Lane Capacity	A.M. Peak Hour				P.M. Peak Hour				
			Volumes				Volumes				
			Total	Critical Lane	V/C Ratio	Critical V/C	Total	Critical Lane	V/C Ratio	Critical V/C	
Northbound :Left	1.0	1700	34	34	0.020		56	56	0.033		
Northbound :Thru	1.0	1700	24	108	0.064	0.064	32	104	0.061	0.061	
Northbound Right:		1700	84				72				
Southbound :Left	1.0	1700	58	58	0.034	0.034	42	42	0.025	0.025	
Southbound :Thru	1.0	1700	30	68	0.040		13	58	0.034		
Southbound Right:		1700	38				45				
Eastbound :Left	1.0	1700	13	13	0.008		55	55	0.032		
Eastbound :Thru	3.0	1700	1005	343	0.202	0.202	1001	343	0.202	0.202	
Eastbound Right:		1700	24				28				
Westbound :Left	1.0	1700	33	33	0.019	0.019	41	41	0.024	0.024	
Westbound :Thru	3.0	1700	606	209	0.123		873	309	0.182		
Westbound Right:		1700	22				54				
Sum of Critical V/C Ratios					0.319		0.312				
Adjustments for Lost Time					0.05		0.05				
<b>Intersection Capacity Utilization (ICU)</b>					<b>0.369</b>		<b>0.362</b>				
<b>Level of Service (LOS)</b>					<b>A</b>		<b>A</b>				

NOTES:

Level of Service (LOS)	
A	0.00 ~ 0.60
B	0.601 ~ 0.70
C	0.701 ~ 0.80
D	0.801 ~ 0.90
E	0.901 ~ 1.00
F	1.00+

Critical Lane Flow Factors		
0.5	Lanes:	2.00
1	Lane:	1.00
1.5	Lanes:	0.67
2	Lanes:	0.50
2.5	Lanes:	0.40
3	Lanes:	0.33



**Intersection Capacity Utilization Analysis (ICU)**

Traffic Scenario: **Existing + Growth + Cumulative + Project**  
 Intersection # **1**  
 Project: Garden Grove Mixed-Use  
 North/South St: Brookhurst Way/Kerry St  
 East/West St: Chapman Ave

Date: 11/4/20  
 By: KH

Movement	No. of Lanes	Critical Lane Capacity	A.M. Peak Hour				P.M. Peak Hour			
			Volumes				Volumes			
			Total	Critical Lane	V/C Ratio	Critical V/C	Total	Critical Lane	V/C Ratio	Critical V/C
Northbound :Left	1.0	1700	34	34	0.020		56	56	0.033	
Northbound :Thru	1.0	1700	24	109	0.064	0.064	32	106	0.062	0.062
Northbound Right:		1700	85				74			
Southbound :Left	1.0	1700	58	58	0.034	0.034	42	42	0.025	0.025
Southbound :Thru	1.0	1700	30	68	0.040		13	58	0.034	
Southbound Right:		1700	38				45			
Eastbound :Left	1.0	1700	13	13	0.008		55	55	0.032	
Eastbound :Thru	3.0	1700	1011	345	0.203	0.203	1008	345	0.203	0.203
Eastbound Right:		1700	24				28			
Westbound :Left	1.0	1700	34	34	0.020	0.020	43	43	0.025	0.025
Westbound :Thru	3.0	1700	611	211	0.124		881	312	0.183	
Westbound Right:		1700	22				54			
Sum of Critical V/C Ratios						0.321	0.315			
Adjustments for Lost Time						0.05	0.05			
<b>Intersection Capacity Utilization (ICU)</b>						<b>0.371</b>	<b>0.365</b>			
<b>Level of Service (LOS)</b>						<b>A</b>	<b>A</b>			

NOTES:

Level of Service (LOS)	
A	0.00 ~ 0.60
B	0.601 ~ 0.70
C	0.701 ~ 0.80
D	0.801 ~ 0.90
E	0.901 ~ 1.00
F	1.00+

Critical Lane Flow Factors		
0.5	Lanes:	2.00
1	Lane:	1.00
1.5	Lanes:	0.67
2	Lanes:	0.50
2.5	Lanes:	0.40
3	Lanes:	0.33

**Intersection Capacity Utilization Analysis (ICU)**

Traffic Scenario: **Existing**  
 Intersection # **2**  
 Project: Garden Grove Mixed-Use  
 North/South St: Brookhurst St  
 East/West St: Garden Grove Blvd

Date: 11/4/20  
 By: KH

Movement	No. of Lanes	Critical Lane Capacity	A.M. Peak Hour				P.M. Peak Hour			
			Volumes				Volumes			
			Total	Critical Lane	V/C Ratio	Critical V/C	Total	Critical Lane	V/C Ratio	Critical V/C
Northbound :Left	2.0	1700	118	65	0.038		203	112	0.066	
Northbound :Thru	3.0	1700	935	312	0.183		1292	431	0.253	0.253
Northbound Right:	1.0	1700	314	314	0.185	0.185	333	333	0.196	
Southbound :Left	2.0	1700	421	232	0.136	0.136	224	123	0.072	0.072
Southbound :Thru	3.0	1700	1017	372	0.219		1039	383	0.225	
Southbound Right:		1700	100				109			
Eastbound :Left	1.0	1700	126	126	0.074		161	161	0.095	
Eastbound :Thru	3.0	1700	841	319	0.188	0.188	655	289	0.170	0.170
Eastbound Right:		1700	116				212			
Westbound :Left	1.0	1700	138	138	0.081	0.081	228	228	0.134	0.134
Westbound :Thru	3.0	1700	435	207	0.122		637	295	0.174	
Westbound Right:		1700	187				249			
Sum of Critical V/C Ratios						0.590	0.629			
Adjustments for Lost Time						0.05	0.05			
<b>Intersection Capacity Utilization (ICU)</b>						<b>0.640</b>	<b>0.679</b>			
<b>Level of Service (LOS)</b>						<b>B</b>	<b>B</b>			

NOTES:

Level of Service (LOS)	
A	0.00 ~ 0.60
B	0.601 ~ 0.70
C	0.701 ~ 0.80
D	0.801 ~ 0.90
E	0.901 ~ 1.00
F	1.00+

Critical Lane Flow Factors		
0.5	Lanes:	2.00
1	Lane:	1.00
1.5	Lanes:	0.67
2	Lanes:	0.50
2.5	Lanes:	0.40
3	Lanes:	0.33

**Intersection Capacity Utilization Analysis (ICU)**

Traffic Scenario: **Existing + Project**

Intersection # **2**

Project: **Garden Grove Mixed-Use**

North/South St: **Brookhurst St**

East/West St: **Garden Grove Blvd**

Date: **11/4/20**

By: **KH**

Movement	No. of Lanes	Critical Lane Capacity	A.M. Peak Hour				P.M. Peak Hour			
			Volumes				Volumes			
			Total	Critical Lane	V/C Ratio	Critical V/C	Total	Critical Lane	V/C Ratio	Critical V/C
Northbound :Left	2.0	1700	118	65	0.038		203	112	0.066	
Northbound :Thru	3.0	1700	942	314	0.185	0.185	1302	434	0.255	0.255
Northbound Right:	1.0	1700	314	314	0.185		333	333	0.196	
Southbound :Left	2.0	1700	428	236	0.139	0.139	234	129	0.076	0.076
Southbound :Thru	3.0	1700	1017	372	0.219		1039	383	0.225	
Southbound Right:		1700	100				109			
Eastbound :Left	1.0	1700	133	133	0.078		171	171	0.101	
Eastbound :Thru	3.0	1700	841	319	0.188	0.188	655	289	0.170	0.170
Eastbound Right:		1700	116				212			
Westbound :Left	1.0	1700	150	150	0.088	0.088	252	252	0.148	0.148
Westbound :Thru	3.0	1700	441	209	0.123		649	299	0.176	
Westbound Right:		1700	187				249			
Sum of Critical V/C Ratios						0.600				0.649
Adjustments for Lost Time						0.05				0.05
<b>Intersection Capacity Utilization (ICU)</b>						<b>0.650</b>				<b>0.699</b>
<b>Level of Service (LOS)</b>						<b>B</b>				<b>B</b>

NOTES:

Level of Service (LOS)	
A	0.00 ~ 0.60
B	0.601 ~ 0.70
C	0.701 ~ 0.80
D	0.801 ~ 0.90
E	0.901 ~ 1.00
F	1.00+

Critical Lane Flow Factors		
0.5	Lanes:	2.00
1	Lane:	1.00
1.5	Lanes:	0.67
2	Lanes:	0.50
2.5	Lanes:	0.40
3	Lanes:	0.33

**Intersection Capacity Utilization Analysis (ICU)**

Traffic Scenario: **Existing + Growth + Cumulative**

Intersection # **2**

Project: **Garden Grove Mixed-Use**

North/South St: **Brookhurst St**

East/West St: **Garden Grove Blvd**

Date: **11/4/20**

By: **KH**

Movement	No. of Lanes	Critical Lane Capacity	A.M. Peak Hour				P.M. Peak Hour			
			Volumes				Volumes			
			Total	Critical Lane	V/C Ratio	Critical V/C	Total	Critical Lane	V/C Ratio	Critical V/C
Northbound :Left	2.0	1700	122	67	0.039		209	115	0.068	
Northbound :Thru	3.0	1700	963	321	0.189		1331	444	0.261	0.261
Northbound Right:	1.0	1700	324	324	0.191	0.191	343	343	0.202	
Southbound :Left	2.0	1700	434	239	0.140	0.140	231	127	0.075	0.075
Southbound :Thru	3.0	1700	1048	384	0.226		1070	394	0.232	
Southbound Right:		1700	103				112			
Eastbound :Left	1.0	1700	130	130	0.076		166	166	0.098	
Eastbound :Thru	3.0	1700	866	329	0.193	0.193	675	298	0.175	0.175
Eastbound Right:		1700	120				218			
Westbound :Left	1.0	1700	142	142	0.084	0.084	235	235	0.138	0.138
Westbound :Thru	3.0	1700	448	214	0.126		656	304	0.179	
Westbound Right:		1700	193				257			
Sum of Critical V/C Ratios						0.608				
Adjustments for Lost Time						0.05				
<b>Intersection Capacity Utilization (ICU)</b>						<b>0.658</b>				
<b>Level of Service (LOS)</b>						<b>B</b>				
							0.649			
							0.05			
							<b>0.699</b>			
							<b>B</b>			

NOTES:

Level of Service (LOS)	
A	0.00 ~ 0.60
B	0.601 ~ 0.70
C	0.701 ~ 0.80
D	0.801 ~ 0.90
E	0.901 ~ 1.00
F	1.00+

Critical Lane Flow Factors		
0.5	Lanes:	2.00
1	Lane:	1.00
1.5	Lanes:	0.67
2	Lanes:	0.50
2.5	Lanes:	0.40
3	Lanes:	0.33

**Intersection Capacity Utilization Analysis (ICU)**

Traffic Scenario: **Existing + Growth + Cumulative + Project**  
 Intersection # **2**  
 Project: Garden Grove Mixed-Use  
 North/South St: Brookhurst St  
 East/West St: Garden Grove Blvd

Date: 11/4/20  
 By: KH

Movement	No. of Lanes	Critical Lane Capacity	A.M. Peak Hour				P.M. Peak Hour			
			Volumes				Volumes			
			Total	Critical Lane	V/C Ratio	Critical V/C	Total	Critical Lane	V/C Ratio	Critical V/C
Northbound :Left	2.0	1700	122	67	0.039		209	115	0.068	
Northbound :Thru	3.0	1700	970	323	0.190		1341	447	0.263	0.263
Northbound Right:	1.0	1700	324	324	0.191	0.191	343	343	0.202	
Southbound :Left	2.0	1700	441	243	0.143	0.143	241	132	0.078	0.078
Southbound :Thru	3.0	1700	1048	384	0.226		1070	394	0.232	
Southbound Right:		1700	103				112			
Eastbound :Left	1.0	1700	137	137	0.081		176	176	0.103	
Eastbound :Thru	3.0	1700	866	329	0.193	0.193	675	298	0.175	0.175
Eastbound Right:		1700	120				218			
Westbound :Left	1.0	1700	154	154	0.091	0.091	259	259	0.152	0.152
Westbound :Thru	3.0	1700	454	216	0.127		668	308	0.181	
Westbound Right:		1700	193				257			
Sum of Critical V/C Ratios						0.618	0.668			
Adjustments for Lost Time						0.05	0.05			
<b>Intersection Capacity Utilization (ICU)</b>						<b>0.668</b>	<b>0.718</b>			
<b>Level of Service (LOS)</b>						<b>B</b>	<b>C</b>			

NOTES:

A	0.00 ~ 0.60
B	0.601 ~ 0.70
C	0.701 ~ 0.80
D	0.801 ~ 0.90
E	0.901 ~ 1.00
F	1.00+

0.5	Lanes:	2.00
1	Lane:	1.00
1.5	Lanes:	0.67
2	Lanes:	0.50
2.5	Lanes:	0.40
3	Lanes:	0.33

**Intersection Capacity Utilization Analysis (ICU)**

Traffic Scenario: **Existing**  
 Intersection # **3**  
 Project: Garden Grove Mixed-Use  
 North/South St: Nutwood St  
 East/West St: Garden Grove Blvd

Date: 11/4/20  
 By: KH

Movement	No, of Lanes	Critical Lane Capacity	A.M. Peak Hour				P.M. Peak Hour			
			Volumes				Volumes			
			Total	Critical Lane	V/C Ratio	Critical V/C	Total	Critical Lane	V/C Ratio	Critical V/C
Northbound :Left		1700	1				6			
Northbound :Thru	1.0	1700	2	2	0.001	0.001	7	13	0.008	0.008
Northbound :Right		1700					6			
Southbound :Left	1.0	1700	254	254	0.149	0.149	133	133	0.078	0.078
Southbound :Thru	1.0	1700	5	110	0.065		1	140	0.082	
Southbound :Right		1700	105				139			
Eastbound :Left	1.0	1700	96	96	0.056		162	162	0.095	0.095
Eastbound :Thru	3.0	1700	1568	524	0.308	0.308	998	333	0.196	
Eastbound :Right		1700	3							
Westbound :Left	1.0	1700	8	8	0.005	0.005	6	6	0.004	
Westbound :Thru	3.0	1700	701	275	0.162		1113	416	0.245	0.245
Westbound :Right		1700	123				134			
Sum of Critical V/C Ratios						0.463	0.426			
Adjustments for Lost Time						0.05	0.05			
<b>Intersection Capacity Utilization (ICU)</b>						<b>0.513</b>	<b>0.476</b>			
<b>Level of Service (LOS)</b>						<b>A</b>	<b>A</b>			

NOTES:

Level of Service (LOS)	
A	0.00 ~ 0.60
B	0.601 ~ 0.70
C	0.701 ~ 0.80
D	0.801 ~ 0.90
E	0.901 ~ 1.00
F	1.00+

Critical Lane Flow Factors		
0.5	Lanes:	2.00
1	Lane:	1.00
1.5	Lanes:	0.67
2	Lanes:	0.50
2.5	Lanes:	0.40
3	Lanes:	0.33

**Intersection Capacity Utilization Analysis (ICU)**

Traffic Scenario: **Existing + Project**

Intersection # **3**

Project: Garden Grove Mixed-Use

North/South St: Nutwood St

East/West St: Garden Grove Blvd

Date: 11/4/20

By: KH

Movement	No, of Lanes	Critical Lane Capacity	A.M. Peak Hour				P.M. Peak Hour			
			Volumes				Volumes			
			Total	Critical Lane	V/C Ratio	Critical V/C	Total	Critical Lane	V/C Ratio	Critical V/C
Northbound :Left		1700	1				6			
Northbound :Thru	1.0	1700	2	2	0.001	0.001	7	13	0.008	0.008
Northbound :Right		1700					6			
Southbound :Left	1.0	1700	254	254	0.149	0.149	133	133	0.078	0.078
Southbound :Thru	1.0	1700	5	111	0.066		1	142	0.083	
Southbound :Right		1700	106				141			
Eastbound :Left	1.0	1700	97	97	0.057		164	164	0.097	0.097
Eastbound :Thru	3.0	1700	1573	525	0.309	0.309	1006	335	0.197	
Eastbound :Right		1700	3							
Westbound :Left	1.0	1700	8	8	0.005	0.005	6	6	0.004	
Westbound :Thru	3.0	1700	707	277	0.163		1120	418	0.246	0.246
Westbound :Right		1700	123				134			
Sum of Critical V/C Ratios						0.464	0.429			
Adjustments for Lost Time						0.05	0.05			
<b>Intersection Capacity Utilization (ICU)</b>						<b>0.514</b>	<b>0.479</b>			
<b>Level of Service (LOS)</b>						<b>A</b>	<b>A</b>			

NOTES:

Level of Service (LOS)	
A	0.00 ~ 0.60
B	0.601 ~ 0.70
C	0.701 ~ 0.80
D	0.801 ~ 0.90
E	0.901 ~ 1.00
F	1.00+

Critical Lane Flow Factors		
0.5	Lanes:	2.00
1	Lane:	1.00
1.5	Lanes:	0.67
2	Lanes:	0.50
2.5	Lanes:	0.40
3	Lanes:	0.33

**Intersection Capacity Utilization Analysis (ICU)**

Traffic Scenario: **Existing + Growth + Cumulative**

Intersection # **3**

Project: Garden Grove Mixed-Use

North/South St: Nutwood St

East/West St: Garden Grove Blvd

Date: 11/4/20

By: KH

Movement	No. of Lanes	Critical Lane Capacity	A.M. Peak Hour				P.M. Peak Hour			
			Volumes				Volumes			
			Total	Critical Lane	V/C Ratio	Critical V/C	Total	Critical Lane	V/C Ratio	Critical V/C
Northbound :Left		1700	1				6			
Northbound :Thru	1.0	1700	2	2	0.001	0.001	7	13	0.008	0.008
Northbound :Right		1700					6			
Southbound :Left	1.0	1700	262	262	0.154	0.154	137	137	0.081	0.081
Southbound :Thru	1.0	1700	5	113	0.066		1	144	0.085	
Southbound :Right		1700	108				143			
Eastbound :Left	1.0	1700	99	99	0.058		167	167	0.098	0.098
Eastbound :Thru	3.0	1700	1616	540	0.317	0.317	1028	343	0.202	
Eastbound :Right		1700	3							
Westbound :Left	1.0	1700	8	8	0.005	0.005	6	6	0.004	
Westbound :Thru	3.0	1700	722	283	0.166		1147	428	0.252	0.252
Westbound :Right		1700	127				138			
Sum of Critical V/C Ratios						0.477	0.439			
Adjustments for Lost Time						0.05	0.05			
<b>Intersection Capacity Utilization (ICU)</b>						<b>0.527</b>	<b>0.489</b>			
<b>Level of Service (LOS)</b>						<b>A</b>	<b>A</b>			

NOTES:

Level of Service (LOS)	
A	0.00 ~ 0.60
B	0.601 ~ 0.70
C	0.701 ~ 0.80
D	0.801 ~ 0.90
E	0.901 ~ 1.00
F	1.00+

Critical Lane Flow Factors		
0.5	Lanes:	2.00
1	Lane:	1.00
1.5	Lanes:	0.67
2	Lanes:	0.50
2.5	Lanes:	0.40
3	Lanes:	0.33



**Intersection Capacity Utilization Analysis (ICU)**

Traffic Scenario: **Existing + Growth + Cumulative + Project**

Intersection # **3**

Project: Garden Grove Mixed-Use

North/South St: Nutwood St

East/West St: Garden Grove Blvd

Date: 11/4/20

By: KH

Movement	No. of Lanes	Critical Lane Capacity	A.M. Peak Hour				P.M. Peak Hour			
			Volumes				Volumes			
			Total	Critical Lane	V/C Ratio	Critical V/C	Total	Critical Lane	V/C Ratio	Critical V/C
Northbound :Left		1700	1				6			
Northbound :Thru	1.0	1700	2	2	0.001	0.001	7	13	0.008	0.008
Northbound :Right		1700					6			
Southbound :Left	1.0	1700	262	262	0.154	0.154	137	137	0.081	0.081
Southbound :Thru	1.0	1700	5	114	0.067		1	146	0.086	
Southbound :Right		1700	109				145			
Eastbound :Left	1.0	1700	100	100	0.059		169	169	0.099	0.099
Eastbound :Thru	3.0	1700	1621	541	0.318	0.318	1036	345	0.203	
Eastbound :Right		1700	3							
Westbound :Left	1.0	1700	8	8	0.005	0.005	6	6	0.004	
Westbound :Thru	3.0	1700	728	285	0.168		1154	431	0.253	0.253
Westbound :Right		1700	127				138			
Sum of Critical V/C Ratios						0.478	0.441			
Adjustments for Lost Time						0.05	0.05			
<b>Intersection Capacity Utilization (ICU)</b>						<b>0.528</b>	<b>0.491</b>			
<b>Level of Service (LOS)</b>						<b>A</b>	<b>A</b>			

**NOTES:**

Level of Service (LOS)	
A	0.00 ~ 0.60
B	0.601 ~ 0.70
C	0.701 ~ 0.80
D	0.801 ~ 0.90
E	0.901 ~ 1.00
F	1.00+

Critical Lane Flow Factors		
0.5	Lanes:	2.00
1	Lane:	1.00
1.5	Lanes:	0.67
2	Lanes:	0.50
2.5	Lanes:	0.40
3	Lanes:	0.33

**Intersection Capacity Utilization Analysis (ICU)**

Traffic Scenario: **Existing**  
 Intersection # **4**  
 Project: Garden Grove Mixed-Use  
 North/South St: Brookhurst St  
 East/West St: Stanford Ave

Date: 11/4/20  
 By: KH

Movement	No. of Lanes	Critical Lane Capacity	A.M. Peak Hour				P.M. Peak Hour			
			Volumes				Volumes			
			Total	Critical Lane	V/C Ratio	Critical V/C	Total	Critical Lane	V/C Ratio	Critical V/C
Northbound :Left	1.0	1700	43	43	0.025	0.025	74	74	0.044	
Northbound :Thru	3.0	1700	950	346	0.203		1731	622	0.366	0.366
Northbound Right:		1700	87				136			
Southbound :Left	1.0	1700	108	108	0.064		96	96	0.056	0.056
Southbound :Thru	3.0	1700	1403	492	0.290	0.290	1197	432	0.254	
Southbound Right:		1700	74				100			
Eastbound :Left	1.0	1700	66	66	0.039		89	89	0.052	0.052
Eastbound :Thru	1.0	1700	110	184	0.108	0.108	82	110	0.065	
Eastbound Right:		1700	74				28			
Westbound :Left	1.0	1700	63	63	0.037	0.037	103	103	0.061	
Westbound :Thru	1.0	1700	69	133	0.078		91	203	0.119	0.119
Westbound Right:		1700	64				112			
Sum of Critical V/C Ratios						0.460	0.593			
Adjustments for Lost Time						0.05	0.05			
<b>Intersection Capacity Utilization (ICU)</b>						<b>0.510</b>	<b>0.643</b>			
<b>Level of Service (LOS)</b>						<b>A</b>	<b>B</b>			

NOTES:

Level of Service (LOS)	
A	0.00 ~ 0.60
B	0.601 ~ 0.70
C	0.701 ~ 0.80
D	0.801 ~ 0.90
E	0.901 ~ 1.00
F	1.00+

Critical Lane Flow Factors		
0.5	Lanes:	2.00
1	Lane:	1.00
1.5	Lanes:	0.67
2	Lanes:	0.50
2.5	Lanes:	0.40
3	Lanes:	0.33

**Intersection Capacity Utilization Analysis (ICU)**

Traffic Scenario: **Existing + Project**

Intersection # **4**

Project: **Garden Grove Mixed-Use**

North/South St: **Brookhurst St**

East/West St: **Stanford Ave**

Date: **11/4/20**

By: **KH**

Movement	No. of Lanes	Critical Lane Capacity	A.M. Peak Hour				P.M. Peak Hour			
			Volumes				Volumes			
			Total	Critical Lane	V/C Ratio	Critical V/C	Total	Critical Lane	V/C Ratio	Critical V/C
Northbound :Left	1.0	1700	44	44	0.026	0.026	76	76	0.045	
Northbound :Thru	3.0	1700	955	347	0.204		1739	625	0.368	0.368
Northbound Right:		1700	87				136			
Southbound :Left	1.0	1700	108	108	0.064		96	96	0.056	0.056
Southbound :Thru	3.0	1700	1409	494	0.291	0.291	1204	435	0.256	
Southbound Right:		1700	74				100			
Eastbound :Left	1.0	1700	66	66	0.039		89	89	0.052	0.052
Eastbound :Thru	1.0	1700	110	185	0.109	0.109	82	112	0.066	
Eastbound Right:		1700	75				30			
Westbound :Left	1.0	1700	63	63	0.037	0.037	103	103	0.061	
Westbound :Thru	1.0	1700	69	133	0.078		91	203	0.119	0.119
Westbound Right:		1700	64				112			
Sum of Critical V/C Ratios						0.463	0.595			
Adjustments for Lost Time						0.05	0.05			
<b>Intersection Capacity Utilization (ICU)</b>						<b>0.513</b>	<b>0.645</b>			
<b>Level of Service (LOS)</b>						<b>A</b>	<b>B</b>			

NOTES:

Level of Service (LOS)	
A	0.00 ~ 0.60
B	0.601 ~ 0.70
C	0.701 ~ 0.80
D	0.801 ~ 0.90
E	0.901 ~ 1.00
F	1.00+

Critical Lane Flow Factors		
0.5	Lanes:	2.00
1	Lane:	1.00
1.5	Lanes:	0.67
2	Lanes:	0.50
2.5	Lanes:	0.40
3	Lanes:	0.33

**Intersection Capacity Utilization Analysis (ICU)**

Traffic Scenario: **Existing + Growth + Cumulative**

Intersection # 4

Project: Garden Grove Mixed-Use

North/South St: Brookhurst St

East/West St: Stanford Ave

Date: 11/4/20

By: KH

Movement	No. of Lanes	Critical Lane Capacity	A.M. Peak Hour				P.M. Peak Hour			
			Volumes				Volumes			
			Total	Critical Lane	V/C Ratio	Critical V/C	Total	Critical Lane	V/C Ratio	Critical V/C
Northbound :Left	1.0	1700	44	44	0.026	0.026	76	76	0.045	
Northbound :Thru	3.0	1700	979	356	0.210		1783	641	0.377	0.377
Northbound Right:		1700	90				140			
Southbound :Left	1.0	1700	111	111	0.065		99	99	0.058	0.058
Southbound :Thru	3.0	1700	1446	507	0.298	0.298	1233	445	0.262	
Southbound Right:		1700	76				103			
Eastbound :Left	1.0	1700	68	68	0.040		92	92	0.054	0.054
Eastbound :Thru	1.0	1700	113	189	0.111	0.111	84	113	0.066	
Eastbound Right:		1700	76				29			
Westbound :Left	1.0	1700	65	65	0.038	0.038	106	106	0.062	
Westbound :Thru	1.0	1700	71	137	0.081		94	209	0.123	0.123
Westbound Right:		1700	66				115			
Sum of Critical V/C Ratios					0.473				0.612	
Adjustments for Lost Time					0.05				0.05	
<b>Intersection Capacity Utilization (ICU)</b>					<b>0.523</b>				<b>0.662</b>	
<b>Level of Service (LOS)</b>					<b>A</b>				<b>B</b>	

NOTES:

Level of Service (LOS)	
A	0.00 ~ 0.60
B	0.601 ~ 0.70
C	0.701 ~ 0.80
D	0.801 ~ 0.90
E	0.901 ~ 1.00
F	1.00+

Critical Lane Flow Factors		
0.5	Lanes:	2.00
1	Lane:	1.00
1.5	Lanes:	0.67
2	Lanes:	0.50
2.5	Lanes:	0.40
3	Lanes:	0.33

**Intersection Capacity Utilization Analysis (ICU)**

Traffic Scenario: **Existing + Growth + Cumulative + Project**  
 Intersection # **4**  
 Project: Garden Grove Mixed-Use  
 North/South St: Brookhurst St  
 East/West St: Stanford Ave

Date: 11/4/20  
 By: KH

Movement	No. of Lanes	Critical Lane Capacity	A.M. Peak Hour				P.M. Peak Hour			
			Volumes				Volumes			
			Total	Critical Lane	V/C Ratio	Critical V/C	Total	Critical Lane	V/C Ratio	Critical V/C
Northbound :Left	1.0	1700	45	45	0.027	0.027	78	78	0.046	
Northbound :Thru	3.0	1700	984	358	0.211		1791	644	0.379	0.379
Northbound Right:		1700	90				140			
Southbound :Left	1.0	1700	111	111	0.065		99	99	0.058	0.058
Southbound :Thru	3.0	1700	1452	509	0.300	0.300	1240	448	0.263	
Southbound Right:		1700	76				103			
Eastbound :Left	1.0	1700	68	68	0.040		92	92	0.054	0.054
Eastbound :Thru	1.0	1700	113	190	0.112	0.112	84	115	0.067	
Eastbound Right:		1700	77				31			
Westbound :Left	1.0	1700	65	65	0.038	0.038	106	106	0.062	
Westbound :Thru	1.0	1700	71	137	0.081		94	209	0.123	0.123
Westbound Right:		1700	66				115			
Sum of Critical V/C Ratios						0.477	0.614			
Adjustments for Lost Time						0.05	0.05			
<b>Intersection Capacity Utilization (ICU)</b>						<b>0.527</b>	<b>0.664</b>			
<b>Level of Service (LOS)</b>						<b>A</b>	<b>B</b>			

NOTES:

Level of Service (LOS)	
A	0.00 ~ 0.60
B	0.601 ~ 0.70
C	0.701 ~ 0.80
D	0.801 ~ 0.90
E	0.901 ~ 1.00
F	1.00+

Critical Lane Flow Factors		
0.5	Lanes:	2.00
1	Lane:	1.00
1.5	Lanes:	0.67
2	Lanes:	0.50
2.5	Lanes:	0.40
3	Lanes:	0.33

**Intersection Capacity Utilization Analysis (ICU)**

Traffic Scenario: **Existing**  
 Intersection # **5**  
 Project: Garden Grove Mixed-Use  
 North/South St: Brookhurst St  
 East/West St: Trask Ave

Date: 11/4/20  
 By: KH

Movement	No. of Lanes	Critical Lane Capacity	A.M. Peak Hour				P.M. Peak Hour			
			Volumes				Volumes			
			Total	Critical Lane	V/C Ratio	Critical V/C	Total	Critical Lane	V/C Ratio	Critical V/C
Northbound :Left	2.0	1700	315	173	0.102	0.102	308	169	0.100	
Northbound :Thru	3.0	1700	1183	394	0.232		1805	602	0.354	0.354
Northbound Right:	1.0	1700	342	342	0.201		257	257	0.151	
Southbound :Left	2.0	1700	118	65	0.038		122	67	0.039	0.039
Southbound :Thru	3.0	1700	1158	386	0.227	0.227	1198	399	0.235	
Southbound Right:	1.0	1700	159	159	0.094		263	263	0.155	
Eastbound :Left	1.0	1700	197	197	0.116		262	262	0.154	
Eastbound :Thru	2.0	1700	408	204	0.120		247	124	0.073	
Eastbound Right:	1.0	1700	410	410	0.241	0.241	602	602	0.354	0.354
Westbound :Left	1.0	1700	233	233	0.137	0.137	181	181	0.106	0.106
Westbound :Thru	2.0	1700	351	223	0.131		389	272	0.160	
Westbound Right:		1700	95				155			
Sum of Critical V/C Ratios						0.707	0.853			
Adjustments for Lost Time						0.05	0.05			
<b>Intersection Capacity Utilization (ICU)</b>						<b>0.757</b>	<b>0.903</b>			
<b>Level of Service (LOS)</b>						<b>C</b>	<b>D</b>			

NOTES:

Level of Service (LOS)	
A	0.00 ~ 0.60
B	0.601 ~ 0.70
C	0.701 ~ 0.80
D	0.801 ~ 0.90
E	0.901 ~ 1.00
F	1.00+

Critical Lane Flow Factors		
0.5	Lanes:	2.00
1	Lane:	1.00
1.5	Lanes:	0.67
2	Lanes:	0.50
2.5	Lanes:	0.40
3	Lanes:	0.33

**Intersection Capacity Utilization Analysis (ICU)**

Traffic Scenario: **Existing + Project**

Intersection # **5**

Project: Garden Grove Mixed-Use

North/South St: Brookhurst St

East/West St: Trask Ave

Date: 11/4/20

By: KH

Movement	No. of Lanes	Critical Lane Capacity	A.M. Peak Hour				P.M. Peak Hour				
			Volumes				Volumes				
			Total	Critical Lane	V/C Ratio	Critical V/C	Total	Critical Lane	V/C Ratio	Critical V/C	
Northbound	:Left	2.0	1700	315	173	0.102	0.102	308	169	0.100	
	:Thru	3.0	1700	1187	396	0.233		1810	603	0.355	0.355
	Right:	1.0	1700	342	342	0.201		257	257	0.151	
Southbound	:Left	2.0	1700	118	65	0.038		122	67	0.039	0.039
	:Thru	3.0	1700	1162	387	0.228	0.228	1204	401	0.236	
	Right:	1.0	1700	161	161	0.095		267	267	0.157	
Eastbound	:Left	1.0	1700	200	200	0.118		265	265	0.156	
	:Thru	2.0	1700	408	204	0.120		247	124	0.073	
	Right:	1.0	1700	410	410	0.241	0.241	602	602	0.354	0.354
Westbound	:Left	1.0	1700	233	233	0.137	0.137	181	181	0.106	0.106
	:Thru	2.0	1700	351	223	0.131		389	272	0.160	
	Right:		1700	95				155			
Sum of Critical V/C Ratios						0.708		0.854			
Adjustments for Lost Time						0.05		0.05			
<b>Intersection Capacity Utilization (ICU)</b>						<b>0.758</b>		<b>0.904</b>			
<b>Level of Service (LOS)</b>						<b>C</b>		<b>E</b>			

NOTES:

Level of Service (LOS)	
A	0.00 ~ 0.60
B	0.601 ~ 0.70
C	0.701 ~ 0.80
D	0.801 ~ 0.90
E	0.901 ~ 1.00
F	1.00+

Critical Lane Flow Factors		
0.5	Lanes:	2.00
1	Lane:	1.00
1.5	Lanes:	0.67
2	Lanes:	0.50
2.5	Lanes:	0.40
3	Lanes:	0.33

**Intersection Capacity Utilization Analysis (ICU)**

Traffic Scenario: **Existing + Growth + Cumulative**

Intersection # **5**

Project: **Garden Grove Mixed-Use**

North/South St: **Brookhurst St**

East/West St: **Trask Ave**

Date: **11/4/20**

By: **KH**

Movement	No. of Lanes	Critical Lane Capacity	A.M. Peak Hour				P.M. Peak Hour			
			Volumes				Volumes			
			Total	Critical Lane	V/C Ratio	Critical V/C	Total	Critical Lane	V/C Ratio	Critical V/C
Northbound :Left	2.0	1700	325	179	0.105	0.105	317	174	0.103	
Northbound :Thru	3.0	1700	1219	406	0.239		1860	620	0.365	0.365
Northbound Right:	1.0	1700	352	352	0.207		265	265	0.156	
Southbound :Left	2.0	1700	122	67	0.039		126	69	0.041	0.041
Southbound :Thru	3.0	1700	1193	398	0.234	0.234	1234	411	0.242	
Southbound Right:	1.0	1700	164	164	0.096		271	271	0.159	
Eastbound :Left	1.0	1700	203	203	0.119		270	270	0.159	
Eastbound :Thru	2.0	1700	420	210	0.124		254	127	0.075	
Eastbound Right:	1.0	1700	422	422	0.248	0.248	620	620	0.365	0.365
Westbound :Left	1.0	1700	240	240	0.141	0.141	186	186	0.109	0.109
Westbound :Thru	2.0	1700	362	230	0.135		401	281	0.165	
Westbound Right:		1700	98				160			
Sum of Critical V/C Ratios						0.728	0.880			
Adjustments for Lost Time						0.05	0.05			
<b>Intersection Capacity Utilization (ICU)</b>						<b>0.778</b>	<b>0.930</b>			
<b>Level of Service (LOS)</b>						<b>C</b>	<b>E</b>			

NOTES:

Level of Service (LOS)	
A	0.00 ~ 0.60
B	0.601 ~ 0.70
C	0.701 ~ 0.80
D	0.801 ~ 0.90
E	0.901 ~ 1.00
F	1.00+

Critical Lane Flow Factors		
0.5	Lanes:	2.00
1	Lane:	1.00
1.5	Lanes:	0.67
2	Lanes:	0.50
2.5	Lanes:	0.40
3	Lanes:	0.33



**Intersection Capacity Utilization Analysis (ICU)**

Traffic Scenario: **Existing + Growth + Cumulative + Project**  
 Intersection # **5**  
 Project: **Garden Grove Mixed-Use**  
 North/South St: **Brookhurst St**  
 East/West St: **Trask Ave**

Date: **11/4/20**  
 By: **KH**

Movement	No. of Lanes	Critical Lane Capacity	A.M. Peak Hour				P.M. Peak Hour			
			Volumes				Volumes			
			Total	Critical Lane	V/C Ratio	Critical V/C	Total	Critical Lane	V/C Ratio	Critical V/C
Northbound :Left	2.0	1700	325	179	0.105	0.105	317	174	0.103	
Northbound :Thru	3.0	1700	1223	408	0.240		1865	622	0.366	0.366
Northbound :Right	1.0	1700	352	352	0.207		265	265	0.156	
Southbound :Left	2.0	1700	122	67	0.039		126	69	0.041	0.041
Southbound :Thru	3.0	1700	1197	399	0.235	0.235	1240	413	0.243	
Southbound :Right	1.0	1700	166	166	0.098		275	275	0.162	
Eastbound :Left	1.0	1700	206	206	0.121		273	273	0.161	
Eastbound :Thru	2.0	1700	420	210	0.124		254	127	0.075	
Eastbound :Right	1.0	1700	422	422	0.248	0.248	620	620	0.365	0.365
Westbound :Left	1.0	1700	240	240	0.141	0.141	186	186	0.109	0.109
Westbound :Thru	2.0	1700	362	230	0.135		401	281	0.165	
Westbound :Right		1700	98				160			
Sum of Critical V/C Ratios						0.729	0.881			
Adjustments for Lost Time						0.05	0.05			
<b>Intersection Capacity Utilization (ICU)</b>						<b>0.779</b>	<b>0.931</b>			
<b>Level of Service (LOS)</b>						<b>C</b>	<b>E</b>			

NOTES:

Level of Service (LOS)	
A	0.00 ~ 0.60
B	0.601 ~ 0.70
C	0.701 ~ 0.80
D	0.801 ~ 0.90
E	0.901 ~ 1.00
F	1.00+

Critical Lane Flow Factors		
0.5	Lanes:	2.00
1	Lane:	1.00
1.5	Lanes:	0.67
2	Lanes:	0.50
2.5	Lanes:	0.40
3	Lanes:	0.33

**APPENDIX D**  
**LEVEL OF SERVICE ANALYSIS OF DRIVEWAY**

**Intersection**

Int Delay, s/veh 0.1

**Movement** EBL EBT WBT WBR SBL SBR

Lane Configurations		↑↑↑	↑↑↑			↗
Traffic Vol, veh/h	0	1624	784	7	0	18
Future Vol, veh/h	0	1624	784	7	0	18
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	-	0
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	1765	852	8	0	20

**Major/Minor** Major1 Major2 Minor2

Conflicting Flow All	-	0	-	0	-	430
Stage 1	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-
Critical Hdwy	-	-	-	-	-	7.14
Critical Hdwy Stg 1	-	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-	-
Follow-up Hdwy	-	-	-	-	-	3.92
Pot Cap-1 Maneuver	0	-	-	-	0	490
Stage 1	0	-	-	-	0	-
Stage 2	0	-	-	-	0	-
Platoon blocked, %	-	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	-	-	-	490
Mov Cap-2 Maneuver	-	-	-	-	-	-
Stage 1	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-

**Approach** EB WB SB

HCM Control Delay, s	0	0	12.7
HCM LOS			B

**Minor Lane/Major Mvmt** EBT WBT WBR SBLn1

Capacity (veh/h)	-	-	-	490
HCM Lane V/C Ratio	-	-	-	0.04
HCM Control Delay (s)	-	-	-	12.7
HCM Lane LOS	-	-	-	B
HCM 95th %tile Q(veh)	-	-	-	0.1

HCM 6th TWSC  
7: Brookhurst St & Driveway B

11/11/2020

Intersection						
Int Delay, s/veh	0					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations		↗ ↑↑↑	↑↑↑			↑↑↑
Traffic Vol, veh/h	0	6	1286	22	0	1585
Future Vol, veh/h	0	6	1286	22	0	1585
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	-	0	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	7	1398	24	0	1723

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	-	711	0	0	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-
Critical Hdwy	-	7.14	-	-	-
Critical Hdwy Stg 1	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-
Follow-up Hdwy	-	3.92	-	-	-
Pot Cap-1 Maneuver	0	322	-	-	0
Stage 1	0	-	-	-	0
Stage 2	0	-	-	-	0
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	-	322	-	-	-
Mov Cap-2 Maneuver	-	-	-	-	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	16.4	0	0
HCM LOS	C		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBT
Capacity (veh/h)	-	-	322
HCM Lane V/C Ratio	-	-	0.02
HCM Control Delay (s)	-	-	16.4
HCM Lane LOS	-	-	C
HCM 95th %tile Q(veh)	-	-	0.1

HCM 6th TWSC  
6: Garden Grove Blvd & Driveway A

11/11/2020

Intersection						
Int Delay, s/veh	0.2					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↑↑↑	↑↑↑			↑
Traffic Vol, veh/h	0	1250	1149	10	0	36
Future Vol, veh/h	0	1250	1149	10	0	36
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	-	0
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	1359	1249	11	0	39

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	-	0	-	0	- 630
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-
Critical Hdwy	-	-	-	-	- 7.14
Critical Hdwy Stg 1	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-
Follow-up Hdwy	-	-	-	-	- 3.92
Pot Cap-1 Maneuver	0	-	-	-	0 364
Stage 1	0	-	-	-	0 -
Stage 2	0	-	-	-	0 -
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	-	-	- 364
Mov Cap-2 Maneuver	-	-	-	-	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-

Approach	EB	WB	SB
HCM Control Delay, s	0	0	16.1
HCM LOS			C

Minor Lane/Major Mvmt	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	-	-	-	364
HCM Lane V/C Ratio	-	-	-	0.108
HCM Control Delay (s)	-	-	-	16.1
HCM Lane LOS	-	-	-	C
HCM 95th %tile Q(veh)	-	-	-	0.4

HCM 6th TWSC  
7: Brookhurst St & Driveway B

11/11/2020

Intersection						
Int Delay, s/veh	0.1					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations		↑	↑↑↑			↑↑↑
Traffic Vol, veh/h	0	12	1753	30	0	1414
Future Vol, veh/h	0	12	1753	30	0	1414
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	-	0	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	13	1905	33	0	1537

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	-	969	0	0	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-
Critical Hdwy	-	7.14	-	-	-
Critical Hdwy Stg 1	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-
Follow-up Hdwy	-	3.92	-	-	-
Pot Cap-1 Maneuver	0	218	-	-	0
Stage 1	0	-	-	-	0
Stage 2	0	-	-	-	0
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	-	218	-	-	-
Mov Cap-2 Maneuver	-	-	-	-	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	22.6	0	0
HCM LOS	C		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBT
Capacity (veh/h)	-	-	218
HCM Lane V/C Ratio	-	-	0.06
HCM Control Delay (s)	-	-	22.6
HCM Lane LOS	-	-	C
HCM 95th %tile Q(veh)	-	-	0.2

**NOISE IMPACT ANALYSIS**  
**MIXED USE GARDEN GROVE PROJECT**  
**GARDEN GROVE, CALIFORNIA**

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Date:

March 21, 2021

Project No.: P20-013 N

## **PROJECT SUMMARY**

The purpose of this study is to evaluate the noise impact of the proposed mixed-use development located at 10201 & 10231 Garden Grove Boulevard in the City of Garden Grove. The project site is currently vacant. The proposed mixed-use development includes a new 52-unit apartment, 9,927-square-foot medical offices and 9,004-squarefoot retail space. Levels 1-3 will house the retail and office uses as well as parking. Residential uses will be above, on floors 4 and 5. The residential units are recessed by at least 15 feet relative to levels 1-3.

The noise impact analysis demonstrates compliance with applicable California Environmental Quality Act (CEQA) exemption criteria for in-fill developments. This study addresses the construction and operational noise impact from the project and examines the noise exposure to the proposed sensitive uses within the mixed-use development. As shown, the project meets applicable noise thresholds without the application of any mitigation measures.

## **NOISE SETTING**

Sound is mechanical energy transmitted by pressure waves in a compressible medium such as air. Noise is generally considered to be unwanted sound. Sound is characterized by various parameters that describe the rate of oscillation of sound waves, the distance between successive troughs or crests, the speed of propagation, and the pressure level or energy content of a given sound. In particular, the sound pressure level has become the most common descriptor used to characterize the loudness of an ambient sound level.

The decibel (dB) scale is used to quantify sound pressure levels. Although decibels are most commonly associated with sound, "dB" is a generic descriptor that is equal to ten times the logarithmic ratio of any physical parameter versus some reference quantity. For sound, the reference level is the faintest sound detectable by a young person with good auditory acuity.

Since the human ear is not equally sensitive to all sound frequencies within the entire auditory spectrum, human response is factored into sound descriptions by weighting sounds within the range of maximum human sensitivity more heavily in a process called "A-weighting," written as dB(A). Any further reference in this discussion to decibels written as "dB" should be understood to be A-weighted.

Time variations in noise exposure are typically expressed in terms of a steady-state energy level equal to the energy content of the time varying period (called LEQ), or alternately, as a statistical description of the sound pressure level that is exceeded over some fraction of a given observation period. Finally, because community receptors are more sensitive to unwanted noise intrusion during the evening and at night, state law requires that, for planning purposes, an artificial dB increment be added to quiet time noise levels in a 24-hour noise descriptor called the Ldn (day-night) or the Community Noise Equivalent Level



(CNEL). The CNEL metric has gradually replaced the Ldn factor, but the two descriptors are essentially identical.

CNEL-based standards are generally applied to transportation-related sources because local jurisdictions are pre-empted from exercising direct noise control over vehicles on public streets, aircraft, trains, etc. The City of Garden Grove therefore regulates the noise exposure of the receiving property through land use controls.

For “stationary” noise sources the City has established noise performance standards designed to not adversely impact adjoining uses. These standards are articulated in the Municipal Code. These standards recognize the varying noise sensitivity of both transmitting and receiving land uses. The property line noise performance standards are normally structured according to land use and time-of-day.

## **CITY OF GARDEN GROVE NOISE STANDARDS**

The Noise Element of the City of Garden Grove General Plan establishes noise quality standards for land use categories based on the State of California Office of Noise Control land use compatibility recommendations. Community noise exposures are recommended as normally acceptable, conditionally acceptable, normally unacceptable, and clearly unacceptable for various classes of land use sensitivity. As shown in Table 1, the City of Garden Grove guidelines recommend an exterior noise exposure of up to 60 dB CNEL as “normally acceptable” for siting residential uses. Noise levels of up to 70 dBA CNEL are considered “conditionally acceptable.” In a “conditionally acceptable” noise category, new construction should be undertaken only after a noise analysis has been made and needed noise insulation features have been incorporated in the project design. These standards apply to exterior recreational noise.

An interior CNEL of 45 dB is mandated by the State of California Noise Insulation Standards (CCR, Title 24, Part 6, Section T25-28) for multiple-family dwellings and hotel and motel rooms. In 1988, the State Building Standards Commission expanded that standard to include all habitable rooms in residential use, including single-family dwelling units. For this project an exterior noise level of 70 dB CNEL in any usable outdoor recreational area and interior noise level of 45 dB in any habitable residential indoor space are considered to be the appropriate compatibility standards for residential use. With required energy-conservation standards in CalGreen, the use of dual-paned windows now can accommodate exterior façade levels of 70-75 dB CNEL and still meet the interior standards as long as windows facing the noise source are closed.

The Noise Ordinance in the City’s municipal code also limits the noise level generated on a property that may cross to a neighboring residential property. The City’s noise ordinance limits are stated in terms of a 30-minute limit with allowable deviations from this 50<sup>th</sup> percentile standard. This noise level describes the noise that is exceeded during a certain percentage of the measurement period. For example, the L<sub>50</sub> is the level exceeded 50% of the measurement period of thirty minutes in an hour. The larger the deviation, the shorter

the allowed duration up to a never-to-exceed 20 dB increase above the 50<sup>th</sup> percentile standard.

Table 1

**City of Garden Grove Land Use Compatibility Guidelines  
for Exterior Community Noise**

Land Use Category	Community Noise Exposure (L <sub>dn</sub> or CNEL, dBA)			
	Normally Acceptable	Conditionally Acceptable	Normally Unacceptable	Clearly Unacceptable
Residential - Low Density, Single-Family, Duplex, Mobile Homes	50 - 60	55 - 70	70-75	75-85
Residential - Multiple Family	50 - 65	60 - 70	70 - 75	70 - 85
Transient Lodging - Motel, Hotels	50 - 65	60 - 70	70 - 80	80 - 85
Schools, Libraries, Churches, Hospitals, Nursing Homes	50 - 70	60 - 70	70 - 80	80 - 85
Auditoriums, Concert Halls, Amphitheaters	NA	50 - 70	NA	65 - 85
Sports Arenas, Outdoor Spectator Sports	NA	50 - 75	NA	70 - 85
Playgrounds, Neighborhood Parks	50 - 70	NA	67.5 - 75	72.5 - 85
Golf Courses, Riding Stables, Water Recreation, Cemeteries	50 - 70	NA	70 - 80	80 - 85
Office Buildings, Business Commercial and Professional	50 - 70	67.5 - 77.5	75 - 85	NA
Industrial, Manufacturing, Utilities, Agriculture	50 - 75	70 - 80	75 - 85	NA
NA: Not Applicable				
Source: Office of Planning and Research, California, <i>General Plan Guidelines</i> , October 2003.				
<p><b>Normally Acceptable</b> – Specified land use is satisfactory, based upon the assumption that any buildings involved are of normal conventional construction, without any special noise insulation requirements.</p> <p><b>Conditionally Acceptable</b> – New construction or development should be undertaken only after a detailed analysis of the noise reduction requirements is made and needed noise insulation features included in the design. Conventional construction, but with closed windows and fresh air supply systems or air conditioning, will normally suffice.</p> <p><b>Normally Unacceptable</b> – New construction or development should be discouraged. If new construction or development does proceed, a detailed analysis of the noise reduction requirements must be made and needed noise insulation features included in the design.</p> <p><b>Clearly Unacceptable</b> – New construction or development should generally not be undertaken.</p>				

Ordinance limits generally apply to “stationary” sources such as mechanical equipment operating on private property. Any stationary equipment at the proposed project must meet the City of Garden Grove Residential Noise Standards at the nearest residential property line, as shown in Table 2. The applicable requirement is a function of the time of day and appropriate zone. The project is required to meet an L<sub>50</sub> of 55 dB day and 50 dB night at the residences east of the project site.

**Table 2  
City of Garden Grove Noise Ordinance Standards**

<b>EXTERIOR NOISE STANDARDS</b>		
<b>Noise Zone</b>	<b>Noise Level</b>	<b>Time Period</b>
Sensitive Uses	55 dB(A)	7:00 a.m.-10:00 p.m.
	50 dB(A)	10:00 p.m.- 7:00 a.m.

The ambient base noise levels contained in this chart shall be utilized as the basis for determining excessive noise levels, unless the actual measured ambient noise level exceeds the ambient base noise level contained in Table 2. When the actual measured ambient noise level exceeds the ambient base noise level, the actual measured ambient noise level shall be utilized as the basis for determining whether the subject noise exceeds the allowed level.

The following adjustments to the noise levels in Table 2 are specified:

1. The noise standard for a cumulative period of more than thirty minutes in any hour; or
2. The noise standard plus 5dB(A) for a cumulative period of more than fifteen minutes in any hour; or
3. The noise standard plus 10dB(A) for a cumulative period of more than five minutes in any hour; or
4. The noise standard plus 15dB(A) for a cumulative period of more than one minute in any hour; or
5. The noise standard plus 20dB(A) for any period of time.
6. In the event the ambient noise level exceeds any of the first four noise limit categories above, the cumulative period applicable to said category shall be increased to reflect the ambient noise level. In the event the ambient noise level exceeds the fifth noise limit category, the maximum allowable noise level under said category shall be increased to reflect the maximum ambient noise level.

Construction noise is regulated in the Municipal Code under Section 8.47.060(D)-Special Noise Sources, which states:

“It shall be unlawful for any person within a residential area, or within a radius of 500 feet therefrom, to operate equipment or perform any outside construction or repair work on buildings, structures, or projects, or to operate any pile driver, power shovel, pneumatic hammer, derrick, power hoist, or any other construction type device between the hours of 10:00 p.m. of one day and 7:00 a.m. of the next day in such a manner that a person of normal sensitiveness, as determined utilizing the criteria established in Section 8.47.050(B), is caused discomfort or annoyance unless such operations are of an emergency nature.”

If construction occurs during permitted daytime hours the noise standards shown in Table 2 are not applicable. However, if construction were to occur during nocturnal hours the noise standards must be met. For this project, only daytime construction is anticipated

The Municipal Code Section 9.18.020.040 requires the notification of residential uses in a mixed residential/commercial mixed-use development:

**A. Resident Notification**

Residents of new residential and residential/commercial mixed use development projects in any Mixed Use zone where residential uses are allowed, whether owners or tenants, shall be notified in writing before taking up residence that they will be living in an urban-type environment and that noise levels may be higher than a strictly residential area.

**B. Acknowledgement of Notification Receipt**

The covenants, conditions, and restrictions of a residential or mixed-use project shall require that prospective residents acknowledge the receipt of the written **noise** notification. Signatures shall confirm receipt and understanding of this information. (2814, 2012)

## **BASELINE NOISE LEVELS**

The project site is on the NE corner of Brookhurst Street and Garden Grove Boulevard. There are seven three story residential structures along the eastern perimeter. These homes are separated from the project site by an approximate 7.5-foot high block wall. The homes are 90-feet to the closest project structure but only 30 feet from the property line. Only paved surfaces will be directly adjacent to the homes.

There are office buildings to the north. Brookhurst Street is to the west. Across Brookhurst are commercial and light industrial uses. Garden Grove Boulevard is to the south. Across Garden Grove Boulevard is a fast-food restaurant and auto-body shop.

Noise measurements were made in order to document existing baseline levels in the area. These help to serve as a basis to determine noise exposure from ambient noise activities upon the proposed project. Short term (15-minute) noise measurements were conducted on Thursday, August 27, 2020 at two on-site locations as shown in Figure 1.

The first measurement location was 75 feet north of the Garden Grove Boulevard centerline and 100 feet east of the Brookhurst Street centerline. The observed Leq was 70 dBA, the Lmax was 86 dBA and the Lmin was 62 dBA.

The second location was along the eastern perimeter and 70 feet north of the Garden Grove Boulevard centerline. This location would be typical of existing noise levels at the adjacent residential uses. The observed Leq was 69 dBA, the Lmax was 80 dBA and the Lmin was 64 dBA.

Monitoring experience shows that 24-hour weighted CNELs can be reasonably well estimated from mid-afternoon noise readings. CNEL's are approximately equal to mid-afternoon Leq plus 2-3 dBA (Caltrans Technical Noise Supplement, 2009). This would equate to an existing CNEL of 72-73 dBA at the intersection of Brookhurst and Garden Grove Boulevard and 71-72 dBA CNEL at the eastern property line.

Figure 1  
Noise Meter Locations



## **NOISE IMPACTS**

### **NOISE SIGNIFICANCE CRITERIA**

According to the current CEQA Appendix G guidelines, noise impacts are considered potentially significant if they result in:

1. Generation of a substantial temporary or permanent increase in ambient noise levels in the vicinity of a project in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?
2. Generation of excessive groundborne vibration or groundborne noise levels?
3. For a project located within the vicinity of a private airstrip or an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?

"Substantially" is not defined in any guidelines. The accuracy of sound level meters and of sound propagation computer models is no better than  $\pm 1$  dB. This is also the human loudness difference discrimination level under ideal laboratory conditions. Most people cannot distinguish a change in the noise environment that differs by less than 3 dB between the pre- and post-project exposure if the change occurs under ambient conditions. For the purposes of this analysis, a traffic noise increase of more than +3 dB that creates or worsens an area of noise/land use incompatibility would be considered a significant degradation of noise quality.

### **SOURCES OF IMPACT**

Several characteristic noise sources are typically identified with mixed use development such as the proposed project. Construction activities, especially heavy equipment, will create short-term noise increases near the project site. Upon completion, vehicular traffic on streets around the proposed project area may create a higher noise exposure. In already-developed areas, the added land use intensity associated with a single project only increases traffic incrementally on existing roadways. These noise impacts are often masked by the baseline, and often preclude perception of any substantial noise level increase. Finally, the analysis examines traffic and ambient noise upon project sensitive uses.

### **SENSITIVE RECEPTORS**

The closest sensitive uses to the project site are the residences to the east. Only paved surfaces will be adjacent to the eastern property line. The closest building façade is 60 feet from the shared residential property line. The homes themselves have a 30 feet setback inside their lots. Therefore, apart from paving and some minimal grading, there will be a 90-foot distance separation between the closest residence and the closest project façade. As discussed, a 7.5-foot CMU separates the two parcels.



## CONSTRUCTION NOISE IMPACTS

Temporary construction noise impacts vary markedly because the noise strength of construction equipment ranges widely as a function of the equipment used and its activity level. Short-term construction noise impacts tend to occur in discrete phases dominated by large, earth-moving equipment sources for demolition and grading. During construction and paving, equipment is generally less noisy.

In 2006, the Federal Highway Administration (FHWA) published the Roadway Construction Noise Model that includes a national database of construction equipment reference noise emissions levels. In addition, the database provides an acoustical usage factor to estimate the fraction of time each piece of construction equipment is operating at full power during a construction phase. The usage factor is a key input variable that is used to calculate the average Leq noise levels.

Table 3 identifies highest (Lmax) noise levels associated with the indicated equipment, then adjusts this noise level for distance to the closest sensitive receptor and the extent of equipment usage (usage factor), which is represented as Leq. The data is first organized by construction activity where noise is shown at a reference 50-foot distance.

Quantitatively, the primary noise prediction equation is expressed as follows for the hourly average noise level (Leq) at distance D between the source and receiver (dBA):

$$Leq = L_{max} @ 50' - 20 \log (D/50') + 10 \log (U.F\%/100) - I.L.(bar)$$

Where:

Lmax @ 50' is the published reference noise level at 50 feet

U.F.% is the usage factor for full power operation per hour

I.L.(bar) is the insertion loss for intervening barriers

**Table 3  
Construction Equipment Noise Levels**

<b>Phase Name and Duration</b>	<b>Equipment</b>	<b>Usage Factor<sup>1</sup></b>	<b>Noise @ 50 feet (dB)<sup>2</sup></b>	<b>Hourly Noise Level @ 50 feet (dB)</b>
Demolition and Fine Grading	Concrete Saw	20%	90	83
	Dozer	40%	85	82
	Loader/Backhoe	37%	78	74
Grading	Grader	40%	85	81
	Dozer	40%	85	82
	Loader/Backhoe	37%	78	74
Construction	Crane	16%	81	73
	Loader/Backhoe	37%	78	74
	Welders	46%	74	71
	Generator Set	50%	81	78
	Forklift	20%	75	69
Paving	Paver	50%	77	74
	Mixer	40%	79	75
	Paving Equipment	40%	76	72
	Loader/Backhoe	37%	78	74
	Roller	20%	80	74

Source: FHWA's Roadway Construction Noise Model, 2006

1. Estimates the fraction of time each piece of equipment is operating at full power during a construction operation
2. The Lmax values presented are the actual measured values summarized in the Roadway Noise Model User Guide (FHWA 2006) unless the actual is unavailable in which case the equipment specifications were used.

The closest sensitive uses to the project site are to the east. As discussed, these residences are 90-feet to the closest project structure but only 30 feet from the property line. Only paved surfaces will be directly adjacent to the homes. Therefore, construction noise is calculated at a 90-foot distance and grading and paving are calculated as occurring at a 30-foot distance. The existing 7.5-foot block wall along the property line would assist in blocking construction noise. A -5 dBA credit was taken for the wall.<sup>1</sup>

With the indicated setback distance and noise wall attenuation the following residual construction noise levels would be observed at the closest off-site use as shown in Table 4.

<sup>1</sup> [https://www.fhwa.dot.gov/Environment/noise/noise\\_barriers/design\\_construction/design/design03.cfm](https://www.fhwa.dot.gov/Environment/noise/noise_barriers/design_construction/design/design03.cfm)  
FHWA: Noise Barrier Design Handbook

**Table 4  
Construction Noise Equipment Levels at Off-Site Sensitive Uses (dBA Leq)**

<b>Phase Name and Duration</b>	<b>Equipment</b>	<b>Noise @ Eastern Perimeter Homes</b>
Demolition	Concrete Saw	82
	Dozer	81
	Loader/Backhoe	73
Grading	Grader	80
	Dozer	81
	Loader/Backhoe	73
Construction	Crane	63
	Loader/Backhoe	64
	Welders	61
	Generator Set	68
	Forklift	58
Paving	Paver	73
	Mixer	74
	Paving Equipment	71
	Loader/Backhoe	73
	Roller	73

As shown, the noisiest activities will occur during grading and demolition. There is only concrete removal required during demolition. Because the site is flat, grading activities will be minimal. The activity with the longest duration (construction) will be much quieter because of increased distance separation.

Interior noise levels would be approximately 25-30 dBA lower than exterior noise levels assuming closed windows. This would mean that homes to the east would experience an interior noise level of 41-52 dBA during the grading and paving activities when equipment could operate directly at the property line. During construction, interior noise levels will decrease to 28-34 dBA.

For indoor noise environments, the highest noise level that permits relaxed conversation with 100 percent intelligibility throughout the room is 45 dBA. Speech interference is considered to be highly intrusive when normal conversation is precluded at 3 feet, which occurs when ambient noise levels substantially exceed 60 dBA. An interior noise level of 52 dBA at indoor locations would maintain a moderately acceptable interior noise environment with closed dual paned windows. In some cases, this noise reduction could be maintained only on a temporary basis, since it requires that windows always remain closed assuming homes have air conditioning.

The potential for construction-related noise to adversely affect nearby residential receptors would depend on the location and proximity of construction activities to these receptors. Most construction equipment will be located at a much greater setback than the worst-case examples provided in Table 4.

Construction activities are exempt from numerical noise regulations if they occur during the hours allowed by the Municipal Code. Equipment noise may be a nuisance even if generated during

allowable hours. Compliance with these hours (7 a.m. to 10 p.m. Monday-Saturday) will maintain construction activity noise impacts at less-than-significant levels with the following recommended noise minimization measures:

- Stockpiling and staging activities must be located as far as practicable from dwellings.
- All mobile equipment shall have properly operating and maintained mufflers.

## CONSTRUCTION ACTIVITY VIBRATION

Ground-borne vibration occurs when heavy equipment travels over unpaved surfaces or when it is engaged in soil movement. The effects of ground-borne vibration include discernable movement of building floors, rattling of windows, shaking of items on shelves or hanging on walls, and rumbling sounds. Vibration related problems generally occur due to resonances in the structural components of a building because structures amplify groundborne vibration. Within the “soft” sedimentary surfaces of much of Southern California, ground vibration is quickly damped out. Groundborne vibration is almost never annoying to people who are outdoors (FTA 2006).

Groundborne vibrations from construction activities rarely reach levels that can damage structures. Because vibration is typically not an issue, very few jurisdictions have adopted vibration significance thresholds. Vibration thresholds have been adopted for major public works construction projects, but these relate mostly to structural protection (cracking foundations or stucco) rather than to human annoyance.

The vibration descriptor commonly used to determine structural damage is the peak particle velocity (ppv) which is defined as the maximum instantaneous positive or negative peak of the vibration signal, usually measured in in/sec. The range of such vibration is shown in Table 5:

**Table 5  
Human Response To Transient Vibration**

Average Human Response	ppv (in/sec)
Severe	2.00
Strongly perceptible	0.90
Distinctly perceptible	0.24
Barely perceptible	0.03

Source: Caltrans Transportation and Construction Vibration Guidance Manual, 2013.

Over the years, numerous vibration criteria and standards have been suggested by researchers, organizations, and governmental agencies. There are no Caltrans or Federal Highway Administration standards for vibration.

According to Caltrans, the threshold for structural vibration damage for modern structures is 0.5 in/sec for intermittent sources, which include impact pile drivers, pogo-stick compactors, crack-

and-seat equipment, vibratory pile drivers, and vibratory compaction equipment. The American Association of State Highway and Transportation Officials (AASHTO) (1990) identifies maximum vibration levels for preventing damage to structures from intermittent construction or maintenance activities for residential buildings in good repair with gypsum board walls to be 0.4–0.5 in/sec. Below this level there is virtually no risk of building damage. Table 6 shows the predicted vibration levels generated by construction equipment at varying distances.

**Table 6**  
**Estimated Vibration Levels During Project Construction**

<b>Equipment</b>	<b>PPV at 25 ft (in/sec)</b>	<b>PPV at 40 ft (in/sec)</b>	<b>PPV at 50 ft (in/sec)</b>	<b>PPV at 100 ft (in/sec)</b>	<b>PPV at 150 ft (in/sec)</b>
Large Bulldozer	0.089	0.044	0.031	0.011	0.006
Loaded trucks	0.076	0.038	0.027	0.010	0.005
Jackhammer	0.035	0.017	0.012	0.004	0.002
Small Bulldozer	0.003	0.001	0.001	<0.001	<0.001

Source: FHWA Transit Noise and Vibration Impact Assessment

The calculation to determine PPV at a given distance is:

$$PPV_{distance} = PPV_{ref} * (25/D)^{1.5}$$

Where:

PPV<sub>distance</sub> = the peak particle velocity in inches/second of the equipment adjusted for distance,

PPV<sub>ref</sub> = the reference vibration level in inches/second at 25 feet, and

D = the distance from the equipment to the receiver.

The closest residential use to the east is 60 feet from the closest on-site structure and the homes are about 30 feet from the property line. As seen on Table 6, at these setbacks the vibration levels are well below levels that could create structural damage (i.e., 0.4-0.5 in/sec).

## OPERATIONAL IMPACTS

Nonresidential project uses are medical/dental offices and small boutique type retail. Neither are considered noise generators. In addition, these facilities would be fully enclosed, and would not operate at night. The vehicular travel aisle at the east of the site (Driveway A accessed via Garden Grove Boulevard), will have a gate which will be closed from 8:00 p.m. to 6:00 a.m. Only residents can open the gates during these hours with a remote control.

There are two parking lot entry points. Driveway B access is directly off Brookhurst and Driveway A is accessed via Garden Grove Boulevard. According to the traffic study 75% of project traffic is expected to access the site via Driveway A and only 25% of traffic would use Driveway B.

For a combination of all project uses, peak hour traffic is 54 a.m. trips and 88 p.m. trips. Only entry via Driveway A could impact adjacent residences. The maximum peak hour trips at Driveway A would be 22 trips (ins and outs) based on the maximal 88 trips (adjusted by 25%). The noise level from 22 trips would be less than 44 dBA without consideration of the 7.5 foot block wall which could reduce noise by about -5 dBA. Entry and egress traffic would be less than ambient noise levels.

Only residential uses will generate nocturnal trips. Peak hour trips for residential uses is 18 trips in the a.m. and 11 trips in the p.m. Applying the 25% use factor for access via Garden Grove Boulevard there would be less than 5 peak hour trips. Because this represents the peak hour, most nocturnal hours would experience considerably less traffic. This would not create a noise impact at adjacent residential uses.

## **VEHICULAR NOISE IMPACTS**

As discussed, the appropriate noise levels for the site are a CNEL of 72-73 dBA at the intersection of Brookhurst and Garden Grove Boulevard and 71-72 dBA CNEL along the eastern perimeter representing noise from Garden Grove Boulevard alone.

The closest project building façade is approximately 67 feet from the Brookhurst centerline and 62 feet from the Garden Grove centerline. However, only commercial uses are located on levels 1-3. Residential units, housed on levels 4 and 5, are at an elevation of 33 feet relative to the ground story. Upper-level residential units are setback to the interior of the structure by at least 15-feet relative to levels 1-3. Therefore, the balconies on residential levels 4 and 5 would benefit from height above the roadway, increased distance to the roadway and noise shielding afforded by the lower levels. In addition, upper-level balconies are partially recessed into the building, which also blocks directional noise. Therefore, the combination of residential units height, being recessed from lower levels and the balconies being recessed into the building, individual balconies would be expected to experience noise levels below 70 dBA CNEL.

Individual balconies represent 3,451 sf of open space. However, the shared roof top gardens provide 14,383 of recreational space. Therefore, 80% of open space is comprised of the roof top gardens. The roof-top starts at 53 feet above grade and will house a perimeter parapet that is 5.5 feet tall.

An interior CNEL of 45 dBA considered the appropriate maximum interior noise exposure for single-family dwelling units. For typical wood-frame construction with stucco and gypsum board wall assemblies, the noise level reduction is as follows:

Partly open windows – 12 dBA

Closed single-paned windows – 20 dBA

Closed dual-paned windows – 30 dBA

Use of dual-paned windows is recommended by the California Building Code for energy conservation in new residential construction. Interior noise standards could readily be met with the

use of closed dual-paned windows. It is noted that where window closure is a requirement for interior noise control, the Building Code requires provision of supplemental ventilation at a specified rate with a specified fraction of fresh make-up air. The provision of supplemental ventilation is a standard construction practice.

## **SUMMARY**

Short-term construction noise intrusion and vibration impacts will be limited by conditions on construction permits requiring compliance with the City of Garden Grove Noise Ordinance. The allowed hours of construction are 7 a.m. and 10 p.m. on weekdays and on Saturdays. Construction is not permitted on any national holiday or on any Sunday. In addition, the following construction practices are recommended:

- Stockpiling and staging activities must be located as far as practicable from dwellings.
- All mobile equipment shall have properly operating and maintained mufflers.

The project is subject to traffic noise emanating from Brookhurst and Garden Grove Boulevard. Traffic noise has the capability of causing an exceedance of the City of Garden Grove noise compatibility guidelines at individual outdoor space for the project residential units facing either roadway. Design and orientation of the balconies is expected to minimize traffic noise. This is due to the height of the residential floors (33 feet above street grade), the recessed design where residential units are recessed at least 15 feet relative to the lower levels and the fact that individual balconies are recessed into the structure. Nevertheless, most open space, comprised of roof top gardens, would be noise protected because of height above ground (53 feet) and a perimeter parapet (5.5 feet).

Interior noise standards can be met with the ability to close dual-paned windows. It is noted that where window closure is a requirement for interior noise control, the provision of supplemental ventilation such as air conditioning is a standard construction practice.

No mitigation is required for compliance to applicable noise requirements.

**AIR QUALITY and GHG IMPACT ANALYSES**  
**MIXED USE GARDEN GROVE PROJECT**  
**GARDEN GROVE, CALIFORNIA**

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Date:

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Project No.: P20-013 AQ



## **PROJECT SUMMARY**

The purpose of this study is to evaluate the air quality and GHG impact of the proposed mixed-use development located at 10201 & 10231 Garden Grove Boulevard in the City of Garden Grove. The project site is currently vacant. The proposed mixed-use development includes a new 52-unit apartment, 9,927-square-foot medical offices and 9,004-squarefoot retail space.

The air quality analysis and GHG analysis demonstrates compliance with applicable CEQA California Environmental Quality Act (CEQA) guidelines set forth by the South Coast Air Quality Management District (SCAQMD). All project related criteria pollutants, both for construction and operations, are below their respective thresholds. This assessment was conducted with the methodology endorsed by the SCAQMD and the CARB (California Air Resources Board). As shown in the analysis, project related air quality and GHG emissions are less-than-significant.

## **ATMOSPHERIC SETTING**

The climate of Garden Grove, technically called a Mediterranean-type climate, is characterized by warm summers, mild winters, infrequent rainfall, moderate afternoon breezes, and generally fair weather. Temperatures near the project area average a very comfortable 63°F year-round. Summer afternoons are typically in the middle 80s and winter mornings drop to the low- to mid-40s. About 45 summer days reach 90 degrees F, and five days per year may drop to 32 degrees, but significant extremes of temperature are rare in the project area. Rainfall in the Los Angeles Basin varies considerably in both time and space. Rainfall amounts vary from an average of 10 to 18 inches as a function of local exposure and topography. Garden Grove averages 14.6 inches of rain during a normal year. Almost all the annual rainfall comes from the fringes of mid-latitude storms from late November to early April with summers often completely dry. Light rain (0.1" in 24 hours) falls on 20 days during a normal year with 10 days in the moderate (0.5" in 24 hours category).

Winds blow primarily from southwest to northeast by day and from northeast to the southwest at night in response to the regional pattern of onshore flow by day and offshore flow at night. Average wind speeds are 5 mph average in the Garden Grove area, reaching 6-8 mph in the afternoon but dropping to near calm conditions (1-3 mph) at night.

The net effect of local airflow in terms of air pollution is that daytime ventilation is good and any locally generated air pollutants will be rapidly dispersed by the strong daytime turbulence. At night, however, pooling of cool air in low elevations combined with light winds does allow for air stagnation in protected areas, especially near area freeways with elevated pollution levels. Because such effects are highly localized, however, the project area is sufficiently far from any major roadways such that it will be little affected by such air stagnation effects.

In addition to winds that control the rate and direction of pollution dispersal, Southern California is notorious for strong temperature inversions that limit the vertical depth through which pollution can be mixed. In summer, coastal areas are characterized by a sharp discontinuity between the

cool marine air at the surface and the warm, sinking air aloft within the high-pressure cell over the ocean to the west. This marine/subsidence inversion allows for good local mixing, but acts like a giant lid over the basin. Air starting onshore at the beach is relatively clean, but becomes progressively more polluted as sources continue to add pollution from below without any dilution from above. Air arriving at Garden Grove during warm season marine flow conditions has undergone limited photochemical reactions, but not to its fullest extent possible. Summer smog levels in Garden Grove are much lower than in inland valleys of the basin such as the San Gabriel or the Pomona-Walnut Valleys. Summer air quality is only moderately degraded compared to the severe degradation found farther inland within the air basin.

A second inversion type forms on clear, winter nights when cold air off the mountains sinks to the surface while the air aloft remains warm. This process forms radiation inversions. These inversions, in conjunction with calm winds, trap pollutants such as automobile exhaust near their source. During the long nocturnal drainage flow from land to sea, the exhaust pollutants continually accumulate within the shallow, cool layer of air near the ground. Central Orange County thus may experience elevated levels of carbon monoxide and nitrogen oxides because of this winter inversion condition. With ongoing vehicular improvements, clean air standards are generally not exceeded during nocturnal stagnation periods as they were 10-20 years ago.

Both types of inversions occur throughout the year to some extent, but the marine inversions are very dominant during the day in summer, and radiation inversions are much stronger on winter nights when nights are long and air is cool. The governing role of these inversions in atmospheric dispersion leads to a substantially different air quality environment in summer near the project area than in winter.

# AIR QUALITY SETTING

## AMBIENT AIR QUALITY STANDARDS (AAQS)

In order to gauge the significance of the air quality impacts of the proposed project, those impacts, together with existing background air quality levels, must be compared to the applicable ambient air quality standards. These standards are the levels of air quality considered safe, with an adequate margin of safety, to protect the public health and welfare. They are designed to protect those people most susceptible to further respiratory distress such as asthmatics, the elderly, very young children, people already weakened by other disease or illness, and persons engaged in strenuous work or exercise, called "sensitive receptors." Healthy adults can tolerate occasional exposure to air pollutant concentrations considerably above these minimum standards before adverse effects are observed. Recent research has shown, however, that chronic exposure to ozone (the primary ingredient in photochemical smog) may lead to adverse respiratory health even at concentrations close to the ambient standard.

National AAQS were established in 1971 for six pollution species with states retaining the option to add other pollutants, require more stringent compliance, or to include different exposure periods. The initial attainment deadline of 1977 was extended several times in air quality problem areas like Southern California. In 2003, the Environmental Protection Agency (EPA) adopted a rule, which extended and established a new attainment deadline for ozone for the year 2021. Because the State of California had established AAQS several years before the federal action and because of unique air quality problems introduced by the restrictive dispersion meteorology, there is considerable difference between state and national clean air standards. Those standards currently in effect in California are shown in Table 1. Sources and health effects of various pollutants are shown in Table 2.

The Federal Clean Air Act Amendments (CAAA) of 1990 required that the U.S. Environmental Protection Agency (EPA) review all national AAQS in light of currently known health effects. EPA was charged with modifying existing standards or promulgating new ones where appropriate. EPA subsequently developed standards for chronic ozone exposure (8+ hours per day) and for very small diameter particulate matter (called "PM-2.5"). New national AAQS were adopted in 1997 for these pollutants.

Planning and enforcement of the federal standards for PM-2.5 and for ozone (8-hour) were challenged by trucking and manufacturing organizations. In a unanimous decision, the U.S. Supreme Court ruled that EPA did not require specific congressional authorization to adopt national clean air standards. The Court also ruled that health-based standards did not require preparation of a cost-benefit analysis. The Court did find, however, that there was some inconsistency between existing and "new" standards in their required attainment schedules. Such attainment-planning schedule inconsistencies centered mainly on the 8-hour ozone standard. EPA subsequently agreed to downgrade the attainment designation for a large number of communities to "non-attainment" for the 8-hour ozone standard.

Table 1

Ambient Air Quality Standards						
Pollutant	Averaging Time	California Standards <sup>1</sup>		National Standards <sup>2</sup>		
		Concentration <sup>3</sup>	Method <sup>4</sup>	Primary <sup>3,5</sup>	Secondary <sup>3,6</sup>	Method <sup>7</sup>
Ozone (O <sub>3</sub> ) <sup>8</sup>	1 Hour	0.09 ppm (180 µg/m <sup>3</sup> )	Ultraviolet Photometry	—	Same as Primary Standard	Ultraviolet Photometry
	8 Hour	0.070 ppm (137 µg/m <sup>3</sup> )		0.070 ppm (137 µg/m <sup>3</sup> )		
Respirable Particulate Matter (PM10) <sup>9</sup>	24 Hour	50 µg/m <sup>3</sup>	Gravimetric or Beta Attenuation	150 µg/m <sup>3</sup>	Same as Primary Standard	Inertial Separation and Gravimetric Analysis
	Annual Arithmetic Mean	20 µg/m <sup>3</sup>		—		
Fine Particulate Matter (PM2.5) <sup>9</sup>	24 Hour	—	—	35 µg/m <sup>3</sup>	Same as Primary Standard	Inertial Separation and Gravimetric Analysis
	Annual Arithmetic Mean	12 µg/m <sup>3</sup>	Gravimetric or Beta Attenuation	12.0 µg/m <sup>3</sup>		
Carbon Monoxide (CO)	1 Hour	20 ppm (23 mg/m <sup>3</sup> )	Non-Dispersive Infrared Photometry (NDIR)	35 ppm (40 mg/m <sup>3</sup> )	—	Non-Dispersive Infrared Photometry (NDIR)
	8 Hour	9.0 ppm (10 mg/m <sup>3</sup> )		9 ppm (10 mg/m <sup>3</sup> )	—	
	8 Hour (Lake Tahoe)	6 ppm (7 mg/m <sup>3</sup> )		—	—	
Nitrogen Dioxide (NO <sub>2</sub> ) <sup>10</sup>	1 Hour	0.18 ppm (339 µg/m <sup>3</sup> )	Gas Phase Chemiluminescence	100 ppb (188 µg/m <sup>3</sup> )	—	Gas Phase Chemiluminescence
	Annual Arithmetic Mean	0.030 ppm (57 µg/m <sup>3</sup> )		0.053 ppm (100 µg/m <sup>3</sup> )	Same as Primary Standard	
Sulfur Dioxide (SO <sub>2</sub> ) <sup>11</sup>	1 Hour	0.25 ppm (655 µg/m <sup>3</sup> )	Ultraviolet Fluorescence	75 ppb (196 µg/m <sup>3</sup> )	—	Ultraviolet Fluorescence; Spectrophotometry (Pararosaniline Method)
	3 Hour	—		—	0.5 ppm (1300 µg/m <sup>3</sup> )	
	24 Hour	0.04 ppm (105 µg/m <sup>3</sup> )		0.14 ppm (for certain areas) <sup>11</sup>	—	
	Annual Arithmetic Mean	—		0.030 ppm (for certain areas) <sup>11</sup>	—	
Lead <sup>12,13</sup>	30 Day Average	1.5 µg/m <sup>3</sup>	Atomic Absorption	—	—	High Volume Sampler and Atomic Absorption
	Calendar Quarter	—		1.5 µg/m <sup>3</sup> (for certain areas) <sup>12</sup>	Same as Primary Standard	
	Rolling 3-Month Average	—		0.15 µg/m <sup>3</sup>		
Visibility Reducing Particles <sup>14</sup>	8 Hour	See footnote 14	Beta Attenuation and Transmittance through Filter Tape	<b>No National Standards</b>		
Sulfates	24 Hour	25 µg/m <sup>3</sup>	Ion Chromatography			
Hydrogen Sulfide	1 Hour	0.03 ppm (42 µg/m <sup>3</sup> )	Ultraviolet Fluorescence			
Vinyl Chloride <sup>12</sup>	24 Hour	0.01 ppm (26 µg/m <sup>3</sup> )	Gas Chromatography			

See footnotes on next page ...

For more information please call ARB-PIO at (916) 322-2990

California Air Resources Board (5/4/16)

## Table 1 (continued)

1. California standards for ozone, carbon monoxide (except 8-hour Lake Tahoe), sulfur dioxide (1 and 24 hour), nitrogen dioxide, and particulate matter (PM10, PM2.5, and visibility reducing particles), are values that are not to be exceeded. All others are not to be equaled or exceeded. California ambient air quality standards are listed in the Table of Standards in Section 70200 of Title 17 of the California Code of Regulations.
2. National standards (other than ozone, particulate matter, and those based on annual arithmetic mean) are not to be exceeded more than once a year. The ozone standard is attained when the fourth highest 8-hour concentration measured at each site in a year, averaged over three years, is equal to or less than the standard. For PM10, the 24 hour standard is attained when the expected number of days per calendar year with a 24-hour average concentration above  $150 \mu\text{g}/\text{m}^3$  is equal to or less than one. For PM2.5, the 24 hour standard is attained when 98 percent of the daily concentrations, averaged over three years, are equal to or less than the standard. Contact the U.S. EPA for further clarification and current national policies.
3. Concentration expressed first in units in which it was promulgated. Equivalent units given in parentheses are based upon a reference temperature of  $25^\circ\text{C}$  and a reference pressure of 760 torr. Most measurements of air quality are to be corrected to a reference temperature of  $25^\circ\text{C}$  and a reference pressure of 760 torr; ppm in this table refers to ppm by volume, or micromoles of pollutant per mole of gas.
4. Any equivalent measurement method which can be shown to the satisfaction of the ARB to give equivalent results at or near the level of the air quality standard may be used.
5. National Primary Standards: The levels of air quality necessary, with an adequate margin of safety to protect the public health.
6. National Secondary Standards: The levels of air quality necessary to protect the public welfare from any known or anticipated adverse effects of a pollutant.
7. Reference method as described by the U.S. EPA. An "equivalent method" of measurement may be used but must have a "consistent relationship to the reference method" and must be approved by the U.S. EPA.
8. On October 1, 2015, the national 8-hour ozone primary and secondary standards were lowered from 0.075 to 0.070 ppm.
9. On December 14, 2012, the national annual PM2.5 primary standard was lowered from  $15 \mu\text{g}/\text{m}^3$  to  $12.0 \mu\text{g}/\text{m}^3$ . The existing national 24-hour PM2.5 standards (primary and secondary) were retained at  $35 \mu\text{g}/\text{m}^3$ , as was the annual secondary standard of  $15 \mu\text{g}/\text{m}^3$ . The existing 24-hour PM10 standards (primary and secondary) of  $150 \mu\text{g}/\text{m}^3$  also were retained. The form of the annual primary and secondary standards is the annual mean, averaged over 3 years.
10. To attain the 1-hour national standard, the 3-year average of the annual 98th percentile of the 1-hour daily maximum concentrations at each site must not exceed 100 ppb. Note that the national 1-hour standard is in units of parts per billion (ppb). California standards are in units of parts per million (ppm). To directly compare the national 1-hour standard to the California standards the units can be converted from ppb to ppm. In this case, the national standard of 100 ppb is identical to 0.100 ppm.
11. On June 2, 2010, a new 1-hour  $\text{SO}_2$  standard was established and the existing 24-hour and annual primary standards were revoked. To attain the 1-hour national standard, the 3-year average of the annual 99th percentile of the 1-hour daily maximum concentrations at each site must not exceed 75 ppb. The 1971  $\text{SO}_2$  national standards (24-hour and annual) remain in effect until one year after an area is designated for the 2010 standard, except that in areas designated nonattainment for the 1971 standards, the 1971 standards remain in effect until implementation plans to attain or maintain the 2010 standards are approved.  
  
Note that the 1-hour national standard is in units of parts per billion (ppb). California standards are in units of parts per million (ppm). To directly compare the 1-hour national standard to the California standard the units can be converted to ppm. In this case, the national standard of 75 ppb is identical to 0.075 ppm.
12. The ARB has identified lead and vinyl chloride as 'toxic air contaminants' with no threshold level of exposure for adverse health effects determined. These actions allow for the implementation of control measures at levels below the ambient concentrations specified for these pollutants.
13. The national standard for lead was revised on October 15, 2008 to a rolling 3-month average. The 1978 lead standard ( $1.5 \mu\text{g}/\text{m}^3$  as a quarterly average) remains in effect until one year after an area is designated for the 2008 standard, except that in areas designated nonattainment for the 1978 standard, the 1978 standard remains in effect until implementation plans to attain or maintain the 2008 standard are approved.
14. In 1989, the ARB converted both the general statewide 10-mile visibility standard and the Lake Tahoe 30-mile visibility standard to instrumental equivalents, which are "extinction of 0.23 per kilometer" and "extinction of 0.07 per kilometer" for the statewide and Lake Tahoe Air Basin standards, respectively.

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California Air Resources Board (5/4/16)

**Table 2**  
**Health Effects of Major Criteria Pollutants**

<b>Pollutants</b>	<b>Sources</b>	<b>Primary Effects</b>
Carbon Monoxide (CO)	<ul style="list-style-type: none"> <li>• Incomplete combustion of fuels and other carbon-containing substances, such as motor exhaust.</li> <li>• Natural events, such as decomposition of organic matter.</li> </ul>	<ul style="list-style-type: none"> <li>• Reduced tolerance for exercise.</li> <li>• Impairment of mental function.</li> <li>• Impairment of fetal development.</li> <li>• Death at high levels of exposure.</li> <li>• Aggravation of some heart diseases (angina).</li> </ul>
Nitrogen Dioxide (NO <sub>2</sub> )	<ul style="list-style-type: none"> <li>• Motor vehicle exhaust.</li> <li>• High temperature stationary combustion.</li> <li>• Atmospheric reactions.</li> </ul>	<ul style="list-style-type: none"> <li>• Aggravation of respiratory illness.</li> <li>• Reduced visibility.</li> <li>• Reduced plant growth.</li> <li>• Formation of acid rain.</li> </ul>
Ozone (O <sub>3</sub> )	<ul style="list-style-type: none"> <li>• Atmospheric reaction of organic gases with nitrogen oxides in sunlight.</li> </ul>	<ul style="list-style-type: none"> <li>• Aggravation of respiratory and cardiovascular diseases.</li> <li>• Irritation of eyes.</li> <li>• Impairment of cardiopulmonary function.</li> <li>• Plant leaf injury.</li> </ul>
Lead (Pb)	<ul style="list-style-type: none"> <li>• Contaminated soil.</li> </ul>	<ul style="list-style-type: none"> <li>• Impairment of blood function and nerve construction.</li> <li>• Behavioral and hearing problems in children.</li> </ul>
Respirable Particulate Matter (PM-10)	<ul style="list-style-type: none"> <li>• Stationary combustion of solid fuels.</li> <li>• Construction activities.</li> <li>• Industrial processes.</li> <li>• Atmospheric chemical reactions.</li> </ul>	<ul style="list-style-type: none"> <li>• Reduced lung function.</li> <li>• Aggravation of the effects of gaseous pollutants.</li> <li>• Aggravation of respiratory and cardio respiratory diseases.</li> <li>• Increased cough and chest discomfort.</li> <li>• Soiling.</li> <li>• Reduced visibility.</li> </ul>
Fine Particulate Matter (PM-2.5)	<ul style="list-style-type: none"> <li>• Fuel combustion in motor vehicles, equipment, and industrial sources.</li> <li>• Residential and agricultural burning.</li> <li>• Industrial processes.</li> <li>• Also, formed from photochemical reactions of other pollutants, including NO<sub>x</sub>, sulfur oxides, and organics.</li> </ul>	<ul style="list-style-type: none"> <li>• Increases respiratory disease.</li> <li>• Lung damage.</li> <li>• Cancer and premature death.</li> <li>• Reduces visibility and results in surface soiling.</li> </ul>
Sulfur Dioxide (SO <sub>2</sub> )	<ul style="list-style-type: none"> <li>• Combustion of sulfur-containing fossil fuels.</li> <li>• Smelting of sulfur-bearing metal ores.</li> <li>• Industrial processes.</li> </ul>	<ul style="list-style-type: none"> <li>• Aggravation of respiratory diseases (asthma, emphysema).</li> <li>• Reduced lung function.</li> <li>• Irritation of eyes.</li> <li>• Reduced visibility.</li> <li>• Plant injury.</li> <li>• Deterioration of metals, textiles, leather, finishes, coatings, etc.</li> </ul>

Source: California Air Resources Board, 2002.

Evaluation of the most current data on the health effects of inhalation of fine particulate matter prompted the California Air Resources Board (ARB) to recommend adoption of the statewide PM-2.5 standard that is more stringent than the federal standard. This standard was adopted in 2002. The State PM-2.5 standard is more of a goal in that it does not have specific attainment planning requirements like a federal clean air standard, but only requires continued progress towards attainment.

Similarly, the ARB extensively evaluated health effects of ozone exposure. A new state standard for an 8-hour ozone exposure was adopted in 2005, which aligned with the exposure period for the federal 8-hour standard. The California 8-hour ozone standard of 0.07 ppm is more stringent than the federal 8-hour standard of 0.075 ppm. The state standard, however, does not have a specific attainment deadline. California air quality jurisdictions are required to make steady progress towards attaining state standards, but there are no hard deadlines or any consequences of non-attainment. During the same re-evaluation process, the ARB adopted an annual state standard for nitrogen dioxide (NO<sub>2</sub>) that is more stringent than the corresponding federal standard, and strengthened the state one-hour NO<sub>2</sub> standard.

As part of EPA's 2002 consent decree on clean air standards, a further review of airborne particulate matter (PM) and human health was initiated. A substantial modification of federal clean air standards for PM was promulgated in 2006. Standards for PM-2.5 were strengthened, a new class of PM in the 2.5 to 10 micron size was created, some PM-10 standards were revoked, and a distinction between rural and urban air quality was adopted. In December, 2012, the federal annual standard for PM-2.5 was reduced from 15 µg/m<sup>3</sup> to 12 µg/m<sup>3</sup> which matches the California AAQS. The severity of the basin's non-attainment status for PM-2.5 may be increased by this action and thus require accelerated planning for future PM-2.5 attainment.

In response to continuing evidence that ozone exposure at levels just meeting federal clean air standards is demonstrably unhealthful, EPA had proposed a further strengthening of the 8-hour standard. A new 8-hour ozone standard was adopted in 2015 after extensive analysis and public input. The adopted national 8-hour ozone standard is 0.07 ppm which matches the current California standard. It will require three years of ambient data collection, then 2 years of non-attainment findings and planning protocol adoption, then several years of plan development and approval. Final air quality plans for the new standard are likely to be adopted around 2022. Ultimate attainment of the new standard in ozone problem areas such as Southern California might be after 2025.

In 2010 a new federal one-hour primary standard for nitrogen dioxide (NO<sub>2</sub>) was adopted. This standard is more stringent than the existing state standard. Based upon air quality monitoring data in the South Coast Air Basin, the California Air Resources Board has requested the EPA to designate the basin as being in attainment for this standard. The federal standard for sulfur dioxide (SO<sub>2</sub>) was also recently revised. However, with minimal combustion of coal and mandatory use of low sulfur fuels in California, SO<sub>2</sub> is typically not a problem pollutant.

## BASELINE AIR QUALITY

Existing and probable future levels of air quality around the project area can best be best inferred from ambient air quality measurements conducted by the SCAQMD at the Costa Mesa monitoring station. This station measures both regional pollution levels such as smog, as well as primary vehicular pollution levels near busy roadways such as carbon monoxide and nitrogen oxides. Pollutants such as particulates (PM-10 and PM-2.5) are not monitored at Costa Mesa but are available from the Anaheim station. Table 3 is a 4-year summary of monitoring data for the major air pollutants compiled from this air monitoring station. From this data the following conclusions regarding air quality trends can be drawn:

Photochemical smog (ozone) levels occasionally exceed standards. All state and federal ozone standards have been exceeded on less than 1 percent of all days in the past four years. While ozone levels are still occasionally elevated, they are much lower than 10 to 20 years ago.

Respirable dust (PM-10) levels exceed the state standard on approximately 3 percent of measured days. The less stringent federal PM-10 standard has not been exceeded in the last four years.

The federal ultra-fine particulate (PM-2.5) standard of  $35 \mu\text{g}/\text{m}^3$  has been exceeded on less than one percent of measurement days in the last four years.

More localized pollutants such as carbon monoxide, nitrogen oxides, etc. are very low near the project site. There is substantial excess dispersive capacity to accommodate localized vehicular air pollutants such as NO<sub>x</sub> or CO without any threat of violating applicable AAQS. Data from a “near roadway” monitoring study directly along the I-5 shoulder (<50 feet) in Anaheim showed noticeably elevated levels of NO<sub>x</sub> and CO, but even at this close distance federal clean air standards were not exceeded.

Although complete attainment of every clean air standard is not yet imminent, extrapolation of the steady improvement trend suggests that such attainment could occur within the reasonably near future.



**Table 3**  
**Air Quality Monitoring Summary (2015-2018)**  
**(Number of Days Standards Were Exceeded, and**  
**Maximum Levels During Such Violations)**  
**(Entries shown as ratios = samples exceeding standard/samples taken)**

Pollutant/Standard	2015	2016	2017	2018
<b>Ozone</b>				
1-Hour > 0.09 ppm (S)	0	0	0	xx
8-Hour > 0.07 ppm (S)	2	0	4	xx
8- Hour > 0.075 ppm (F)	1	0	1	xx
Max. 1-Hour Conc. (ppm)	0.099	0.090	0.080	xx
Max. 8-Hour Conc. (ppm)	0.079	0.069	0.080	xx
<b>Carbon Monoxide</b>				
8- Hour > 9. ppm (S,F)	0	0	0	0
Max 8-hour Conc. (ppm)	2.2	2.1	2.1	1.9
<b>Nitrogen Dioxide</b>				
1-Hour > 0.18 ppm (S)	0	0	0	0
Max. 1-Hour Conc. (ppm)	0.059	0.060	0.045	xx
<b>Inhalable Particulates (PM-10)</b>				
24-hour > 50 µg/m <sup>3</sup> (S)	11/363	3/353	17/332	13/320
24-hour > 150 µg/m <sup>3</sup> (F)	0/363	0/353	0/332	0/320
Max. 24-Hr. Conc. (µg/m <sup>3</sup> )	66.	74.	128.	129.
<b>Ultra-Fine Particulates (PM-2.5)</b>				
24-Hour > 35 µg/m <sup>3</sup> (F)	3/295	1/349	6/305	3/353
Max. 24-Hr. Conc. (µg/m <sup>3</sup> )	45.8	44.4	53.9	54.1

xx = data not available

Costa Mesa Air Quality Monitoring Station (3195): Ozone and Nitrogen Dioxide  
Anaheim Air Quality Monitoring Station (3176): Particulates

## AIR QUALITY PLANNING

The Federal Clean Air Act (1977 Amendments) required that designated agencies in any area of the nation not meeting national clean air standards must prepare a plan demonstrating the steps that would bring the area into compliance with all national standards. The SCAB could not meet the deadlines for ozone, nitrogen dioxide, carbon monoxide, or PM-10. In the SCAB, the agencies designated by the governor to develop regional air quality plans are the SCAQMD and the Southern California Association of Governments (SCAG). The two agencies first adopted an Air Quality Management Plan (AQMP) in 1979 and revised it several times as earlier attainment forecasts were shown to be overly optimistic.

The 1990 Federal Clean Air Act Amendment (CAAA) required that all states with air-sheds with “serious” or worse ozone problems submit a revision to the State Implementation Plan (SIP). Amendments to the SIP have been proposed, revised and approved over the past decade. The most current regional attainment emissions forecast for ozone precursors (ROG and NOx) and for carbon monoxide (CO) and for particulate matter are shown in Table 4. Substantial reductions in emissions of ROG, NOx and CO are forecast to continue throughout the next several decades. Unless new particulate control programs are implemented, PM-10 and PM-2.5 are forecast to slightly increase.

The Air Quality Management District (AQMD) adopted an updated clean air “blueprint” in August 2003. The 2003 Air Quality Management Plan (AQMP) was approved by the EPA in 2004. The AQMP outlined the air pollution measures needed to meet federal health-based standards for ozone by 2010 and for particulates (PM-10) by 2006. The 2003 AQMP was based upon the federal one-hour ozone standard which was revoked late in 2005 and replaced by an 8-hour federal standard. Because of the revocation of the hourly standard, a new air quality planning cycle was initiated.

With re-designation of the air basin as non-attainment for the 8-hour ozone standard, a new attainment plan was developed. This plan shifted most of the one-hour ozone standard attainment strategies to the 8-hour standard. As previously noted, the attainment date was to “slip” from 2010 to 2021. The updated attainment plan also includes strategies for ultimately meeting the federal PM-2.5 standard.

Because projected attainment by 2021 required control technologies that did not exist yet, the SCAQMD requested a voluntary “bump-up” from a “severe non-attainment” area to an “extreme non-attainment” designation for ozone. The extreme designation was to allow a longer time period for these technologies to develop. If attainment cannot be demonstrated within the specified deadline without relying on “black-box” measures, EPA would have been required to impose sanctions on the region had the bump-up request not been approved. In April 2010, the EPA approved the change in the non-attainment designation from “severe-17” to “extreme.” This reclassification set a later attainment deadline (2024), but also required the air basin to adopt even more stringent emissions controls.

**Table 4**  
**South Coast Air Basin Emissions Forecasts (Emissions in tons/day)**

<b>Pollutant</b>	<b>2015<sup>a</sup></b>	<b>2020<sup>b</sup></b>	<b>2025<sup>b</sup></b>	<b>2030<sup>b</sup></b>
<b>NO<sub>x</sub></b>	357	289	266	257
<b>VOC</b>	400	393	393	391
<b>PM-10</b>	161	165	170	172
<b>PM-2.5</b>	67	68	70	71

<sup>a</sup>2015 Base Year.

<sup>b</sup>With current emissions reduction programs and adopted growth forecasts.

Source: California Air Resources Board, 2013 Almanac of Air Quality

In other air quality attainment plan reviews, EPA had disapproved part of the SCAB PM-2.5 attainment plan included in the AQMP. EPA stated that the current attainment plan relied on PM-2.5 control regulations that had not yet been approved or implemented. It was expected that a number of rules that were pending approval would remove the identified deficiencies. If these issues were not resolved within the next several years, federal funding sanctions for transportation projects could result. The 2012 AQMP included in the current California State Implementation Plan (SIP) was expected to remedy identified PM-2.5 planning deficiencies.

The federal Clean Air Act requires that non-attainment air basins have EPA approved attainment plans in place. This requirement includes the federal one-hour ozone standard even though that standard was revoked almost ten years ago. There was no approved attainment plan for the one-hour federal standard at the time of revocation. Through a legal quirk, the SCAQMD is now required to develop an AQMP for the long since revoked one-hour federal ozone standard. Because the current SIP for the basin contains a number of control measures for the 8-hour ozone standard that are equally effective for one-hour levels, the 2012 AQMP was believed to satisfy hourly attainment planning requirements.

AQMPs are required to be updated every three years. The 2012 AQMP was adopted in early 2013. An updated AQMP was required for completion in 2016. The 2016 AQMP was adopted by the SCAQMD Board in March, 2017, and has been submitted the California Air Resources Board for forwarding to the EPA. The 2016 AQMP acknowledges that motor vehicle emissions have been effectively controlled and that reductions in NO<sub>x</sub>, the continuing ozone problem pollutant, may need to come from major stationary sources (power plants, refineries, landfill flares, etc.) . The current attainment deadlines for all federal non-attainment pollutants are now as follows:

8-hour ozone (70 ppb)	2032
Annual PM-2.5 (12 µg/m <sup>3</sup> )	2025
8-hour ozone (75 ppb)	2024 (old standard)
1-hour ozone (120 ppb)	2023 (rescinded standard)

## 24-hour PM-2.5 (35 µg/m<sup>3</sup>) 2019

The key challenge is that NO<sub>x</sub> emission levels, as a critical ozone precursor pollutant, are forecast to continue to exceed the levels that would allow the above deadlines to be met. Unless additional stringent NO<sub>x</sub> control measures are adopted and implemented, ozone attainment goals may not be met.

The proposed project does not directly relate to the AQMP in that there are no specific air quality programs or regulations governing mixed use development projects. Conformity with adopted plans, forecasts and programs relative to population, housing, employment and land use is the primary yardstick by which impact significance of planned growth is determined. The SCAQMD, however, while acknowledging that the AQMP is a growth-accommodating document, does not favor designating regional impacts as less-than-significant just because the proposed development is consistent with regional growth projections. Air quality impact significance for the proposed project has therefore been analyzed on a project-specific basis.

# **AIR QUALITY IMPACT**

## **STANDARDS OF SIGNIFICANCE**

Air quality impacts are considered “significant” if they cause clean air standards to be violated where they are currently met, or if they “substantially” contribute to an existing violation of standards. Any substantial emissions of air contaminants for which there is no safe exposure, or nuisance emissions such as dust or odors, would also be considered a significant impact.

Appendix G of the California CEQA Guidelines offers the following five tests of air quality impact significance. A project would have a potentially significant impact if it:

- a. Conflicts with or obstructs implementation of the applicable air quality plan.
- b. Violates any air quality standard or contributes substantially to an existing or projected air quality violation.
- c. Results in a cumulatively considerable net increase of any criteria pollutants for which the project region is non-attainment under an applicable federal or state ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors).
- d. Exposes sensitive receptors to substantial pollutant concentrations.
- e. Creates objectionable odors affecting a substantial number of people.

### **Primary Pollutants**

Air quality impacts generally occur on two scales of motion. Near an individual source of emissions or a collection of sources such as a crowded intersection or parking lot, levels of those pollutants that are emitted in their already unhealthful form will be highest. Carbon monoxide (CO) is an example of such a pollutant. Primary pollutant impacts can generally be evaluated directly in comparison to appropriate clean air standards. Violations of these standards where they are currently met, or a measurable worsening of an existing or future violation, would be considered a significant impact. Many particulates, especially fugitive dust emissions, are also primary pollutants. Because of the non-attainment status of the South Coast Air Basin (SCAB) for PM-10, an aggressive dust control program is required to control fugitive dust during project construction.

### **Secondary Pollutants**

Many pollutants, however, require time to transform from a more benign form to a more unhealthful contaminant. Their impact occurs regionally far from the source. Their incremental regional impact is minute on an individual basis and cannot be quantified except through complex photochemical computer models. Analysis of significance of such emissions is based upon a

specified amount of emissions (pounds, tons, etc.) even though there is no way to translate those emissions directly into a corresponding ambient air quality impact.

Because of the chemical complexity of primary versus secondary pollutants, the SCAQMD has designated significant emissions levels as surrogates for evaluating regional air quality impact significance independent of chemical transformation processes. Projects with daily emissions that exceed any of the following emission thresholds are recommended by the SCAQMD to be considered significant under CEQA guidelines.

**Table 5**  
**Daily Emissions Thresholds**

<b>Pollutant</b>	<b>Construction</b>	<b>Operations</b>
ROG	75	55
NOx	100	55
CO	550	550
PM-10	150	150
PM-2.5	55	55
SOx	150	150
Lead	3	3

Source: SCAQMD CEQA Air Quality Handbook, November, 1993 Rev.

### **Additional Indicators**

In its CEQA Handbook, the SCAQMD also states that additional indicators should be used as screening criteria to determine the need for further analysis with respect to air quality. The additional indicators are as follows:

- Project could interfere with the attainment of the federal or state ambient air quality standards by either violating or contributing to an existing or projected air quality violation
- Project could result in population increases within the regional statistical area which would be in excess of that projected in the AQMP and in other than planned locations for the project's build-out year.
- Project could generate vehicle trips that cause a CO hot spot.

## CONSTRUCTION ACTIVITY IMPACTS

CalEEMod was developed by the SCAQMD to provide a model by which to calculate both construction emissions and operational emissions from a variety of land use projects. It calculates both the daily maximum and annual average emissions for criteria pollutants as well as total or annual greenhouse gas (GHG) emissions.

The project entails construction of 52 apartments, almost 10,000 sf of medical/dental office space and approximately 9,000 sf of retail space on a 1.86-acre site. Construction was modeled in CalEEMod2016.3.2 using default construction equipment and schedule for a project of this size as shown in Table 6.

**Table 6  
Construction Activity Equipment Fleet**

<b>Phase Name and Duration</b>	<b>Equipment</b>
Demolition/Fine Grading (20 days)	1 Concrete Saw
	1 Dozer
	3 Loader/Backhoes
Grading (4 days) 480 cy export	1 Grader
	1 Dozer
	1 Loader/Backhoe
Construction (200 days)	1 Crane
	1 Loader/Backhoe
	3 Welders
	1 Generator Set
	1 Forklift
Paving (10 days)	1 Paver
	1 Mixer
	1 Paving Equipment
	1 Loader/Backhoe
	1 Roller
Painting (10 days)	1 Air Compressor

Utilizing this indicated equipment fleet and durations shown in Table 6 the following worst-case daily construction emissions are calculated by CalEEMod and are listed in Table 7.

**Table 7**  
**Construction Activity Emissions**  
**Maximum Daily Emissions (pounds/day)**

<b>Maximal Construction Emissions</b>	<b>ROG</b>	<b>NOx</b>	<b>CO</b>	<b>SO<sub>2</sub></b>	<b>PM-10</b>	<b>PM-2.5</b>
<b>2022</b>	52.4	16.6	15.3	0.0	3.0	1.7
SCAQMD Thresholds	75	100	550	150	150	55

Peak daily construction activity emissions are estimated to be below SCAQMD CEQA thresholds without the need for added mitigation. The model reflects required dust suppression during grading as required by the SCAQMD.

Construction equipment exhaust contains carcinogenic compounds within the diesel exhaust particulates. The toxicity of diesel exhaust is evaluated relative to a 24-hour per day, 365 days per year, 70-year lifetime exposure. The SCAQMD does not generally require the analysis of construction-related diesel emissions relative to health risk due to the short period for which the majority of diesel exhaust would occur. Health risk analyses are typically assessed over a 9-, 30-, or 70-year timeframe and not over a relatively brief construction period due to the lack of health risk associated with such a brief exposure.

## **LOCALIZED SIGNIFICANCE THRESHOLDS**

The SCAQMD has developed analysis parameters to evaluate ambient air quality on a local level in addition to the more regional emissions-based thresholds of significance. These analysis elements are called Localized Significance Thresholds (LSTs). LSTs were developed in response to Governing Board's Environmental Justice Enhancement Initiative 1-4 and the LST methodology was provisionally adopted in October 2003 and formally approved by SCAQMD's Mobile Source Committee in February 2005.

Use of an LST analysis for a project is optional. For the proposed project, the primary source of possible LST impact would be during construction. LSTs are applicable for a sensitive receptor where it is possible that an individual could remain for 24 hours such as a residence, hospital or convalescent facility.

LSTs are only applicable to the following criteria pollutants: oxides of nitrogen (NOx), carbon monoxide (CO), and particulate matter (PM-10 and PM-2.5). LSTs represent the maximum emissions from a project that are not expected to cause or contribute to an exceedance of the most stringent applicable federal or state ambient air quality standard, and are developed based on the ambient concentrations of that pollutant for each source receptor area and distance to the nearest sensitive receptor.

LST screening tables are available for 25, 50, 100, 200 and 500 meter source-receptor distances. For this project, there are adjacent residential uses such that the most conservative 25 meter distance was modeled.



The SCAQMD has issued guidance on applying CalEEMod to LSTs. LST pollutant screening level concentration data is currently published for 1, 2 and 5 acre sites for varying distances. For this project, the most stringent thresholds for a 1 acre site were applied.

The following thresholds and emissions in Table 8 are therefore determined (pounds per day):

**Table 8  
LST and Project Emissions (pounds/day)**

<b>1.0 acre/25 meters Central Orange County</b>	<b>CO</b>	<b>NOx</b>	<b>PM-10</b>	<b>PM-2.5</b>
<b>LST Threshold</b>	485	81	4	3
<b>Max On-Site Emissions</b>				
<b>Unmitigated</b>	15	16	6	3
<b>Mitigated</b>	15	16	3	2

CalEEMod Output in Appendix

LSTs were compared to the maximum daily construction activities. As seen in Table 8, with active dust suppression, mitigated emissions meet the LST for construction thresholds. LST impacts are less-than-significant.

Therefore, the following construction mitigation measure is necessary to ensure LST thresholds are maintained below significance thresholds:

- Exposed surfaces will be watered at least two times per day during grading activities

## **OPERATIONAL IMPACTS**

The project would generate 959 daily trips using trip generation numbers provided in the project traffic report. Operational emissions were calculated using CalEEMod2016.3.2 for an assumed full occupancy year of 2023. The operational impacts are shown in Table 9. As shown, operational emissions will not exceed applicable SCAQMD operational emissions CEQA thresholds of significance.

**Table 9  
Proposed Uses Daily Operational Impacts (2023)**

<b>Source</b>	<b>Operational Emissions (lbs/day)</b>					
	<b>ROG</b>	<b>NOx</b>	<b>CO</b>	<b>SO<sub>2</sub></b>	<b>PM-10</b>	<b>PM-2.5</b>
Area*	1.8	0.8	4.6	0.0	0.1	0.1
Energy	0.0	0.2	0.1	0.0	0.0	0.0
Mobile	1.2	4.0	14.9	0.1	5.5	1.5
<b>Total</b>	<b>3.0</b>	<b>5.0</b>	<b>19.6</b>	<b>0.1</b>	<b>5.6</b>	<b>1.6</b>
SCAQMD Threshold	55	55	550	150	150	55
Exceeds Threshold?	No	No	No	No	No	No

\*no wood burning fireplaces-only natural gas  
Source: CalEEMod Output in Appendix

## **CONSTRUCTION EMISSIONS MINIMIZATION**

Construction activities are not anticipated to cause dust emissions to exceed SCAQMD CEQA thresholds. Nevertheless, emissions minimization through enhanced dust control measures is recommended for use because of the non-attainment status of the air and proximity of residential uses. Recommended measures include:

### **Fugitive Dust Control**

- Apply soil stabilizers or moisten inactive areas.
- Water exposed surfaces as needed to avoid visible dust leaving the construction site (typically 2-3 times/day).
- Cover all stock piles with tarps at the end of each day or as needed.
- Provide water spray during loading and unloading of earthen materials.
- Minimize in-out traffic from construction zone
- Cover all trucks hauling dirt, sand, or loose material and require all trucks to maintain at least two feet of freeboard
- Sweep streets daily if visible soil material is carried out from the construction site

Similarly, ozone precursor emissions (ROG and NO<sub>x</sub>) are calculated to be below SCAQMD CEQA thresholds. However, because of the regional non-attainment for photochemical smog, the use of reasonably available control measures for diesel exhaust is recommended. Combustion emissions control options include:

### **Exhaust Emissions Control**

- Utilize well-tuned off-road construction equipment.
- Establish a preference for contractors using Tier 3 or better rated heavy equipment.
- Enforce 5-minute idling limits for both on-road trucks and off-road equipment.

## GREENHOUSE GAS EMISSIONS

“Greenhouse gases” (so called because of their role in trapping heat near the surface of the earth) emitted by human activity are implicated in global climate change, commonly referred to as “global warming.” These greenhouse gases contribute to an increase in the temperature of the earth’s atmosphere by transparency to short wavelength visible sunlight, but near opacity to outgoing terrestrial long wavelength heat radiation in some parts of the infrared spectrum. The principal greenhouse gases (GHGs) are carbon dioxide, methane, nitrous oxide, ozone, and water vapor. For purposes of planning and regulation, Section 15364.5 of the California Code of Regulations defines GHGs to include carbon dioxide, methane, nitrous oxide, hydrofluorocarbons, perfluorocarbons and sulfur hexafluoride. Fossil fuel consumption in the transportation sector (on-road motor vehicles, off-highway mobile sources, and aircraft) is the single largest source of GHG emissions, accounting for approximately half of GHG emissions globally. Industrial and commercial sources are the second largest contributors of GHG emissions with about one-fourth of total emissions.

California has passed several bills and the Governor has signed at least three executive orders regarding greenhouse gases. GHG statues and executive orders (EO) include AB 32, SB 1368, EO S-03-05, EO S-20-06 and EO S-01-07.

AB 32 is one of the most significant pieces of environmental legislation that California has adopted. Among other things, it is designed to maintain California’s reputation as a “national and international leader on energy conservation and environmental stewardship.” It will have wide-ranging effects on California businesses and lifestyles as well as far reaching effects on other states and countries. A unique aspect of AB 32, beyond its broad and wide-ranging mandatory provisions and dramatic GHG reductions are the short time frames within which it must be implemented. Major components of the AB 32 include:

- Require the monitoring and reporting of GHG emissions beginning with sources or categories of sources that contribute the most to statewide emissions.
- Requires immediate “early action” control programs on the most readily controlled GHG sources.
- Mandates that by 2020, California’s GHG emissions be reduced to 1990 levels.
- Forces an overall reduction of GHG gases in California by 25-40%, from business as usual, to be achieved by 2020.
- Must complement efforts to achieve and maintain federal and state ambient air quality standards and to reduce toxic air contaminants.

Statewide, the framework for developing the implementing regulations for AB 32 is under way. Maximum GHG reductions are expected to derive from increased vehicle fuel efficiency, from greater use of renewable energy and from increased structural energy efficiency. Additionally, through the California Climate Action Registry (CCAR now called the Climate Action Reserve), general and industry-specific protocols for assessing and reporting GHG emissions have been

developed. GHG sources are categorized into direct sources (i.e. company owned) and indirect sources (i.e. not company owned). Direct sources include combustion emissions from on-and off-road mobile sources, and fugitive emissions. Indirect sources include off-site electricity generation and non-company owned mobile sources.

## **THRESHOLDS OF SIGNIFICANCE**

In response to the requirements of SB97, the State Resources Agency developed guidelines for the treatment of GHG emissions under CEQA. These new guidelines became state laws as part of Title 14 of the California Code of Regulations in March, 2010. The CEQA Appendix G guidelines were modified to include GHG as a required analysis element. A project would have a potentially significant impact if it:

- Generates GHG emissions, directly or indirectly, that may have a significant impact on the environment, or,
- Conflicts with an applicable plan, policy or regulation adopted to reduce GHG emissions.

Section 15064.4 of the Code specifies how significance of GHG emissions is to be evaluated. The process is broken down into quantification of project-related GHG emissions, making a determination of significance, and specification of any appropriate mitigation if impacts are found to be potentially significant. At each of these steps, the new GHG guidelines afford the lead agency with substantial flexibility.

Emissions identification may be quantitative, qualitative or based on performance standards. CEQA guidelines allow the lead agency to “select the model or methodology it considers most appropriate.” The most common practice for transportation/combustion GHG emissions quantification is to use a computer model such as CalEEMod, as was used in the ensuing analysis.

The significance of those emissions then must be evaluated; the selection of a threshold of significance must take into consideration what level of GHG emissions would be cumulatively considerable. The guidelines are clear that they do not support a zero net emissions threshold. If the lead agency does not have sufficient expertise in evaluating GHG impacts, it may rely on thresholds adopted by an agency with greater expertise.

On December 5, 2008 the SCAQMD Governing Board adopted an Interim quantitative GHG Significance Threshold for industrial projects where the SCAQMD is the lead agency (e.g., stationary source permit projects, rules, plans, etc.) of 10,000 Metric Tons (MT) CO<sub>2</sub> equivalent/year. In September 2010, the SCAQMD CEQA Significance Thresholds GHG Working Group released revisions which recommended a threshold of 3,000 MT CO<sub>2</sub>e for all land use projects. This 3,000 MT/year recommendation has been used as a guideline for this analysis. In the absence of an adopted numerical threshold of significance, project related GHG emissions in excess of the guideline level are presumed to trigger a requirement for enhanced GHG reduction at the project level.

## PROJECT RELATED GHG EMISSIONS GENERATION

### Construction Activity GHG Emissions

The project is assumed to require less than one year for construction. During project construction, the CalEEMod2016.3.2 computer model predicts that the construction activities will generate the annual CO<sub>2</sub>e emissions identified in Table 10.

**Table 10**  
**Construction Emissions (Metric Tons CO<sub>2</sub>e)**

	<b>CO<sub>2</sub>e</b>
Year 2022	333.7
<b>Amortized</b>	<b>11.1</b>

CalEEMod Output provided in appendix

SCAQMD GHG emissions policy from construction activities is to amortize emissions over a 30-year lifetime. The amortized level is also provided. GHG impacts from construction are considered individually less-than-significant.

### Project Operational GHG Emissions

The input assumptions for operational GHG emissions calculations, and the GHG conversion from consumption to annual regional CO<sub>2</sub>e emissions are summarized in the CalEEMod2016.3.2 output files found in the appendix of this report.

The total operational and annualized construction emissions for the proposed project are identified in Table 11. The project GHG emissions are considered less-than-significant.

**Table 11**  
**Operational Emissions**  
**(Metric Tons CO<sub>2</sub>e)**

<b>Consumption Source</b>	
Area Sources	12.2
Energy Utilization	324.7
Mobile Source	873.3
Solid Waste Generation	71.0
Water Consumption	39.2
Construction	11.1
<b>Total</b>	<b>1,331.5</b>
Guideline Threshold	3,000

## **CONSISTENCY WITH GHG PLANS, PROGRAMS AND POLICIES**

The City of Garden Grove has not yet developed a Greenhouse Gas Reduction Plan. The applicable GHG planning document is AB-32. As discussed above, the project is not expected to result in a significant increase in GHG emissions. As a result, the project results in GHG emissions below the recommended SCAQMD 3,000 ton threshold. Therefore, the project would not conflict with any applicable plan, policy, or regulation to reduce GHG emissions.

## **SUMMARY**

Construction activities are not anticipated to cause emissions to exceed their daily SCAQMD CEQA thresholds.

Operational emissions are not anticipated to cause emissions to exceed their daily SCAQMD CEQA thresholds.

With required dust suppression during grading, localized significance thresholds are not anticipated to exceed their SCAQMD significance thresholds.

## **CALEEMOD2016.3.2 COMPUTER MODEL OUTPUT**

- **DAILY EMISISONS**
- **ANNUAL EMISSIONS**



Garden Grove Blvd Mixed Use Proj 2021 - Orange County, Annual

**Garden Grove Blvd Mixed Use Proj 2021**  
Orange County, Annual

**1.0 Project Characteristics**

**1.1 Land Usage**

Land Uses	Size	Metric	Lot Acreage	Floor Surface Area	Population
Medical Office Building	9.93	1000sqft	0.23	9,927.00	0
Enclosed Parking with Elevator	182.00	Space	0.00	72,800.00	0
Apartments Low Rise	52.00	Dwelling Unit	1.42	52,000.00	149
Strip Mall	9.00	1000sqft	0.21	9,004.00	0

**1.2 Other Project Characteristics**

Urbanization	Urban	Wind Speed (m/s)	2.2	Precipitation Freq (Days)	30
Climate Zone	8			Operational Year	2023

Utility Company Southern California Edison

CO2 Intensity (lb/MW/hr)	702.44	CH4 Intensity (lb/MW/hr)	0.029	N2O Intensity (lb/MW/hr)	0.006
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**1.3 User Entered Comments & Non-Default Data**

Garden Grove Blvd Mixed Use Proj 2021 - Orange County, Annual

Project Characteristics -

Land Use - 1.86 acre site

Construction Phase - Demo: 20 days, Grading: 4 days, Construction: 200 days, Paving: 10 days

Trips and VMT - site demoed

Demolition -

Vehicle Trips - trip rates per traffic report: 959 TPD

Construction Off-road Equipment Mitigation -

Area Mitigation -

Grading - 480 CU Export

Table Name	Column Name	Default Value	New Value
tblGrading	MaterialExported	0.00	480.00
tblLandUse	LotAcreage	1.64	0.00
tblLandUse	LotAcreage	3.25	1.42
tblVehicleTrips	ST_TR	7.16	6.63
tblVehicleTrips	ST_TR	42.04	33.87
tblVehicleTrips	SU_TR	6.07	6.63
tblVehicleTrips	SU_TR	20.43	33.87
tblVehicleTrips	WD_TR	6.59	6.63
tblVehicleTrips	WD_TR	36.13	31.13
tblVehicleTrips	WD_TR	44.32	33.87

2.0 Emissions Summary



Garden Grove Blvd Mixed Use Proj 2021 - Orange County, Annual

Quarter	Start Date	End Date	Maximum Unmitigated ROG + NOX (tons/quarter)	Maximum Mitigated ROG + NOX (tons/quarter)
1	1-1-2022	3-31-2022	0.5501	0.5501
2	4-1-2022	6-30-2022	0.5352	0.5352
3	7-1-2022	9-30-2022	0.5411	0.5411
		Highest	0.5501	0.5501

**2.2 Overall Operational**

**Unmitigated Operational**

Category	tons/yr										MT/yr					
	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Area	0.4738	0.0197	0.8695	8.7000e-004		0.0526	0.0526		0.0526	0.0526	5.5234	11.4950	17.0184	0.0173	3.7000e-004	17.5633
Energy	4.1600e-003	0.0359	0.0175	2.3000e-004		2.8800e-003	2.8800e-003		2.8800e-003	2.8800e-003	0.0000	323.1320	323.1320	0.0124	3.1600e-003	324.3855
Mobile	0.1917	0.7082	2.4346	9.4500e-003	0.8988	6.6100e-003	0.9054	0.2407	6.1400e-003	0.2468	0.0000	872.3746	872.3746	0.0354	0.0000	873.2598
Waste						0.0000	0.0000		0.0000	0.0000	28.5426	0.0000	28.5426	1.6868	0.0000	70.7130
Water						0.0000	0.0000		0.0000	0.0000	1.6817	31.8387	33.5204	0.1740	4.3500e-003	39.1677
<b>Total</b>	<b>0.6697</b>	<b>0.7638</b>	<b>3.3216</b>	<b>0.0106</b>	<b>0.8988</b>	<b>0.0621</b>	<b>0.9609</b>	<b>0.2407</b>	<b>0.0617</b>	<b>0.3024</b>	<b>35.7476</b>	<b>1,238,840</b>	<b>1,274,588</b>	<b>1.9260</b>	<b>7.8800e-003</b>	<b>1,325.089</b>

Garden Grove Blvd Mixed Use Proj 2021 - Orange County, Annual

**2.2 Overall Operational  
Mitigated Operational**

Category	tons/yr											MT/yr					
	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio-CO2	NBio-CO2	Total CO2	CH4	N2O	CO2e	
Area	0.3046	0.0159	0.5431	9.0000e-005		3.7600e-003	3.7600e-003		3.7600e-003	3.7600e-003	0.0000	12.1194	12.1194	1.0700e-003	2.1000e-004	12.2075	
Energy	4.1600e-003	0.0359	0.0175	2.3000e-004		2.8800e-003	2.8800e-003		2.8800e-003	2.8800e-003	0.0000	323.1320	323.1320	0.0124	3.1600e-003	324.3855	
Mobile	0.1917	0.7082	2.4346	9.4500e-003	0.8988	6.6100e-003	0.9054	0.2407	6.1400e-003	0.2468	0.0000	872.3746	872.3746	0.0354	0.0000	873.2598	
Waste						0.0000	0.0000		0.0000	0.0000	28.5426	0.0000	28.5426	1.6868	0.0000	70.7130	
Water						0.0000	0.0000		0.0000	0.0000	1.6817	31.8387	33.5204	0.1740	4.3500e-003	39.1677	
<b>Total</b>	<b>0.5005</b>	<b>0.7600</b>	<b>2.9952</b>	<b>9.7700e-003</b>	<b>0.8988</b>	<b>0.0133</b>	<b>0.9120</b>	<b>0.2407</b>	<b>0.0128</b>	<b>0.2535</b>	<b>30.2242</b>	<b>1,239.4648</b>	<b>1,269.6890</b>	<b>1.9098</b>	<b>7.7200e-003</b>	<b>1,319.7336</b>	

Percent Reduction	tons/yr											MT/yr					
	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio-CO2	NBio-CO2	Total CO2	CH4	N2O	CO2e	
25.27	0.49	9.83	7.39	0.00	78.67	5.09	0.00	79.27	16.16	15.45	-0.05	0.38	0.84	2.03	0.40		

**3.0 Construction Detail**

**Construction Phase**

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Phase Number	Phase Name	Phase Type	Start Date	End Date	Num Days Week	Num Days	Phase Description
1	Demolition	Demolition	1/1/2022	1/28/2022	5	20	
2	Grading	Grading	1/29/2022	2/3/2022	5	4	
3	Building Construction	Building Construction	2/4/2022	11/10/2022	5	200	
4	Paving	Paving	11/11/2022	11/24/2022	5	10	
5	Architectural Coating	Architectural Coating	11/25/2022	12/8/2022	5	10	

Acres of Grading (Site Preparation Phase): 0

Acres of Grading (Grading Phase): 1.5

Acres of Paving: 0

Residential Indoor: 105,300; Residential Outdoor: 35,100; Non-Residential Indoor: 28,397; Non-Residential Outdoor: 9,466; Striped Parking Area: 4,368 (Architectural Coating – sqft)

OffRoad Equipment

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Phase Name	Offroad Equipment Type	Amount	Usage Hours	Horse Power	Load Factor
Demolition	Concrete/Industrial Saws	1	8.00	81	0.73
Demolition	Rubber Tired Dozers	1	8.00	247	0.40
Demolition	Tractors/Loaders/Backhoes	3	8.00	97	0.37
Grading	Graders	1	6.00	187	0.41
Grading	Rubber Tired Dozers	1	6.00	247	0.40
Grading	Tractors/Loaders/Backhoes	1	7.00	97	0.37
Building Construction	Cranes	1	6.00	231	0.29
Building Construction	Forklifts	1	6.00	89	0.20
Building Construction	Generator Sets	1	8.00	84	0.74
Building Construction	Tractors/Loaders/Backhoes	1	6.00	97	0.37
Building Construction	Welders	3	8.00	46	0.45
Paving	Cement and Mortar Mixers	1	6.00	9	0.56
Paving	Pavers	1	6.00	130	0.42
Paving	Paving Equipment	1	8.00	132	0.36
Paving	Rollers	1	7.00	80	0.38
Paving	Tractors/Loaders/Backhoes	1	8.00	97	0.37
Architectural Coating	Air Compressors	1	6.00	78	0.48

Trips and VMT

Phase Name	Offroad Equipment Count	Worker Trip Number	Vendor Trip Number	Hauling Trip Number	Worker Trip Length	Vendor Trip Length	Hauling Trip Length	Worker Vehicle Class	Vendor Vehicle Class	Hauling Vehicle Class
Demolition	5	13.00	0.00	0.00	14.70	6.90	20.00	LD_Mix	HDT_Mix	HHDT
Grading	3	8.00	0.00	60.00	14.70	6.90	20.00	LD_Mix	HDT_Mix	HHDT
Building Construction	7	74.00	21.00	0.00	14.70	6.90	20.00	LD_Mix	HDT_Mix	HHDT
Paving	5	13.00	0.00	0.00	14.70	6.90	20.00	LD_Mix	HDT_Mix	HHDT
Architectural Coating	1	15.00	0.00	0.00	14.70	6.90	20.00	LD_Mix	HDT_Mix	HHDT

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**3.1 Mitigation Measures Construction**

Water Exposed Area

**3.2 Demolition - 2022**

**Unmitigated Construction On-Site**

Category	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
tons/yr											MT/yr					
Fugitive Dust					0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Off-Road	0.0169	0.1662	0.1396	2.4000e-004	8.3800e-003	8.3800e-003	8.3800e-003	7.8300e-003	7.8300e-003	7.8300e-003	0.0000	21.0777	21.0777	5.3700e-003	0.0000	21.2120
<b>Total</b>	<b>0.0169</b>	<b>0.1662</b>	<b>0.1396</b>	<b>2.4000e-004</b>	<b>0.0000</b>	<b>8.3800e-003</b>	<b>8.3800e-003</b>	<b>0.0000</b>	<b>7.8300e-003</b>	<b>7.8300e-003</b>	<b>0.0000</b>	<b>21.0777</b>	<b>21.0777</b>	<b>5.3700e-003</b>	<b>0.0000</b>	<b>21.2120</b>



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**3.2 Demolition - 2022**

**Unmitigated Construction Off-Site**

Category	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
tons/yr																
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	4.5000e-004	2.9000e-004	3.4800e-003	1.0000e-005	1.4300e-003	1.0000e-005	1.4400e-003	3.8000e-004	1.0000e-005	3.9000e-004	0.0000	1.1482	1.1482	2.0000e-005	0.0000	1.1488
<b>Total</b>	<b>4.5000e-004</b>	<b>2.9000e-004</b>	<b>3.4800e-003</b>	<b>1.0000e-005</b>	<b>1.4300e-003</b>	<b>1.0000e-005</b>	<b>1.4400e-003</b>	<b>3.8000e-004</b>	<b>1.0000e-005</b>	<b>3.9000e-004</b>	<b>0.0000</b>	<b>1.1482</b>	<b>1.1482</b>	<b>2.0000e-005</b>	<b>0.0000</b>	<b>1.1488</b>
MT/yr																

**Mitigated Construction On-Site**

Category	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
tons/yr																
Fugitive Dust					0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Off-Road	0.0169	0.1662	0.1396	2.4000e-004	8.3800e-003	8.3800e-003	8.3800e-003	7.8300e-003	7.8300e-003	7.8300e-003	0.0000	21.0777	21.0777	5.3700e-003	0.0000	21.2119
<b>Total</b>	<b>0.0169</b>	<b>0.1662</b>	<b>0.1396</b>	<b>2.4000e-004</b>	<b>0.0000</b>	<b>8.3800e-003</b>	<b>8.3800e-003</b>	<b>7.8300e-003</b>	<b>7.8300e-003</b>	<b>7.8300e-003</b>	<b>0.0000</b>	<b>21.0777</b>	<b>21.0777</b>	<b>5.3700e-003</b>	<b>0.0000</b>	<b>21.2119</b>
MT/yr																

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**3.2 Demolition - 2022**

**Mitigated Construction Off-Site**

Category	tons/yr											MT/yr				
	ROG	NOX	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	4.5000e-004	2.9000e-004	3.4800e-003	1.0000e-005	1.4300e-003	1.0000e-005	1.4400e-003	3.8000e-004	1.0000e-005	3.9000e-004	0.0000	1.1482	1.1482	2.0000e-005	0.0000	1.1488
<b>Total</b>	<b>4.5000e-004</b>	<b>2.9000e-004</b>	<b>3.4800e-003</b>	<b>1.0000e-005</b>	<b>1.4300e-003</b>	<b>1.0000e-005</b>	<b>1.4400e-003</b>	<b>3.8000e-004</b>	<b>1.0000e-005</b>	<b>3.9000e-004</b>	<b>0.0000</b>	<b>1.1482</b>	<b>1.1482</b>	<b>2.0000e-005</b>	<b>0.0000</b>	<b>1.1488</b>

**3.3 Grading - 2022**

**Unmitigated Construction On-Site**

Category	tons/yr											MT/yr				
	ROG	NOX	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Fugitive Dust					9.8600e-003	0.0000	9.8600e-003	5.0600e-003	0.0000	5.0600e-003	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Off-Road	2.1700e-003	0.0240	0.0119	3.0000e-005	1.0300e-003	1.0300e-003	1.0300e-003	9.5000e-004	9.5000e-004	9.5000e-004	0.0000	2.4763	2.4763	8.0000e-004	0.0000	2.4963
<b>Total</b>	<b>2.1700e-003</b>	<b>0.0240</b>	<b>0.0119</b>	<b>3.0000e-005</b>	<b>9.8600e-003</b>	<b>1.0300e-003</b>	<b>0.0109</b>	<b>5.0600e-003</b>	<b>9.5000e-004</b>	<b>6.0100e-003</b>	<b>0.0000</b>	<b>2.4763</b>	<b>2.4763</b>	<b>8.0000e-004</b>	<b>0.0000</b>	<b>2.4963</b>

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**3.3 Grading - 2022**

**Unmitigated Construction Off-Site**

Category	tons/yr											MT/yr				
	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Hauling	2.1000e-004	7.2200e-003	2.1500e-003	2.0000e-005	5.1000e-004	2.0000e-005	5.4000e-004	1.4000e-004	2.0000e-005	1.6000e-004	0.0000	2.2485	2.2485	2.4000e-004	0.0000	2.2545
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	6.0000e-005	4.0000e-005	4.3000e-004	0.0000	1.8000e-004	0.0000	1.8000e-004	5.0000e-005	0.0000	5.0000e-005	0.0000	0.1413	0.1413	0.0000	0.0000	0.1414
<b>Total</b>	<b>2.7000e-004</b>	<b>7.2600e-003</b>	<b>2.5800e-003</b>	<b>2.0000e-005</b>	<b>6.9000e-004</b>	<b>2.0000e-005</b>	<b>7.2000e-004</b>	<b>1.9000e-004</b>	<b>2.0000e-005</b>	<b>2.1000e-004</b>	<b>0.0000</b>	<b>2.3899</b>	<b>2.3899</b>	<b>2.4000e-004</b>	<b>0.0000</b>	<b>2.3958</b>

**Mitigated Construction On-Site**

Category	tons/yr											MT/yr				
	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Fugitive Dust					4.4400e-003	0.0000	4.4400e-003	2.2700e-003	0.0000	2.2700e-003	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Off-Road	2.1700e-003	0.0240	0.0119	3.0000e-005	1.0300e-003	1.0300e-003	1.0300e-003	9.5000e-004	9.5000e-004	9.5000e-004	0.0000	2.4763	2.4763	8.0000e-004	0.0000	2.4963
<b>Total</b>	<b>2.1700e-003</b>	<b>0.0240</b>	<b>0.0119</b>	<b>3.0000e-005</b>	<b>4.4400e-003</b>	<b>1.0300e-003</b>	<b>5.4700e-003</b>	<b>2.2700e-003</b>	<b>9.5000e-004</b>	<b>3.2200e-003</b>	<b>0.0000</b>	<b>2.4763</b>	<b>2.4763</b>	<b>8.0000e-004</b>	<b>0.0000</b>	<b>2.4963</b>

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**3.3 Grading - 2022**

**Mitigated Construction Off-Site**

Category	tons/yr											MT/yr				
	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Hauling	2.1000e-004	7.2200e-003	2.1500e-003	2.0000e-005	5.1000e-004	2.0000e-005	5.4000e-004	1.4000e-004	2.0000e-005	1.6000e-004	0.0000	2.2485	2.2485	2.4000e-004	0.0000	2.2545
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	6.0000e-005	4.0000e-005	4.3000e-004	0.0000	1.8000e-004	0.0000	1.8000e-004	5.0000e-005	0.0000	5.0000e-005	0.0000	0.1413	0.1413	0.0000	0.0000	0.1414
<b>Total</b>	<b>2.7000e-004</b>	<b>7.2600e-003</b>	<b>2.5800e-003</b>	<b>2.0000e-005</b>	<b>6.9000e-004</b>	<b>2.0000e-005</b>	<b>7.2000e-004</b>	<b>1.9000e-004</b>	<b>2.0000e-005</b>	<b>2.1000e-004</b>	<b>0.0000</b>	<b>2.3899</b>	<b>2.3899</b>	<b>2.4000e-004</b>	<b>0.0000</b>	<b>2.3958</b>

**3.4 Building Construction - 2022**

**Unmitigated Construction On-Site**

Category	tons/yr											MT/yr				
	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Off-Road	0.1649	1.2503	1.2726	2.2100e-003	0.0589	0.0589	0.0589	0.0569	0.0569	0.0569	0.0000	181.5769	181.5769	0.0316	0.0000	182.3675
<b>Total</b>	<b>0.1649</b>	<b>1.2503</b>	<b>1.2726</b>	<b>2.2100e-003</b>	<b>0.0589</b>	<b>0.0589</b>	<b>0.0589</b>	<b>0.0569</b>	<b>0.0569</b>	<b>0.0569</b>	<b>0.0000</b>	<b>181.5769</b>	<b>181.5769</b>	<b>0.0316</b>	<b>0.0000</b>	<b>182.3675</b>

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**3.4 Building Construction - 2022**

**Unmitigated Construction Off-Site**

Category	tons/yr											MT/yr					
	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e	
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Vendor	5.3800e-003	0.1891	0.0541	5.1000e-004	0.0132	3.6000e-004	0.0136	3.8100e-003	3.5000e-004	4.1600e-003	0.0000	50.1793	50.1793	3.9700e-003	0.0000	0.0000	50.2785
Worker	0.0257	0.0165	0.1982	7.2000e-004	0.0812	5.3000e-004	0.0818	0.0216	4.8000e-004	0.0221	0.0000	65.3593	65.3593	1.3200e-003	0.0000	0.0000	65.3923
<b>Total</b>	<b>0.0310</b>	<b>0.2056</b>	<b>0.2523</b>	<b>1.2300e-003</b>	<b>0.0945</b>	<b>8.9000e-004</b>	<b>0.0953</b>	<b>0.0254</b>	<b>8.3000e-004</b>	<b>0.0262</b>	<b>0.0000</b>	<b>115.5385</b>	<b>115.5385</b>	<b>5.2900e-003</b>	<b>0.0000</b>	<b>0.0000</b>	<b>115.6708</b>

**Mitigated Construction On-Site**

Category	tons/yr											MT/yr				
	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Off-Road	0.1649	1.2503	1.2726	2.2100e-003		0.0589	0.0589		0.0569	0.0569	0.0000	181.5767	181.5767	0.0316	0.0000	182.3673
<b>Total</b>	<b>0.1649</b>	<b>1.2503</b>	<b>1.2726</b>	<b>2.2100e-003</b>		<b>0.0589</b>	<b>0.0589</b>		<b>0.0569</b>	<b>0.0569</b>	<b>0.0000</b>	<b>181.5767</b>	<b>181.5767</b>	<b>0.0316</b>	<b>0.0000</b>	<b>182.3673</b>

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**3.4 Building Construction - 2022**

**Mitigated Construction Off-Site**

Category	tons/yr											MT/yr				
	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Vendor	5.3800e-003	0.1891	0.0541	5.1000e-004	0.0132	3.6000e-004	0.0136	3.8100e-003	3.5000e-004	4.1600e-003	0.0000	50.1793	50.1793	3.9700e-003	0.0000	50.2785
Worker	0.0257	0.0165	0.1982	7.2000e-004	0.0812	5.3000e-004	0.0818	0.0216	4.8000e-004	0.0221	0.0000	65.3593	65.3593	1.3200e-003	0.0000	65.3923
<b>Total</b>	<b>0.0310</b>	<b>0.2056</b>	<b>0.2523</b>	<b>1.2300e-003</b>	<b>0.0945</b>	<b>8.9000e-004</b>	<b>0.0953</b>	<b>0.0254</b>	<b>8.3000e-004</b>	<b>0.0262</b>	<b>0.0000</b>	<b>115.5385</b>	<b>115.5385</b>	<b>5.2900e-003</b>	<b>0.0000</b>	<b>115.6708</b>

**3.5 Paving - 2022**

**Unmitigated Construction On-Site**

Category	tons/yr											MT/yr				
	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Off-Road	3.4400e-003	0.0339	0.0440	7.0000e-005	1.7400e-003	1.7400e-003	1.7400e-003	1.6000e-003	1.6000e-003	1.6000e-003	0.0000	5.8848	5.8848	1.8700e-003	0.0000	5.9315
Paving	0.0000				0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
<b>Total</b>	<b>3.4400e-003</b>	<b>0.0339</b>	<b>0.0440</b>	<b>7.0000e-005</b>	<b>1.7400e-003</b>	<b>1.7400e-003</b>	<b>1.7400e-003</b>	<b>1.6000e-003</b>	<b>1.6000e-003</b>	<b>1.6000e-003</b>	<b>0.0000</b>	<b>5.8848</b>	<b>5.8848</b>	<b>1.8700e-003</b>	<b>0.0000</b>	<b>5.9315</b>

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**3.5 Paving - 2022**

**Unmitigated Construction Off-Site**

Category	tons/yr											MT/yr				
	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	2.3000e-004	1.5000e-004	1.7400e-003	1.0000e-005	7.1000e-004	0.0000	7.2000e-004	1.9000e-004	0.0000	1.9000e-004	0.0000	0.5741	0.5741	1.0000e-005	0.0000	0.5744
<b>Total</b>	<b>2.3000e-004</b>	<b>1.5000e-004</b>	<b>1.7400e-003</b>	<b>1.0000e-005</b>	<b>7.1000e-004</b>	<b>0.0000</b>	<b>7.2000e-004</b>	<b>1.9000e-004</b>	<b>0.0000</b>	<b>1.9000e-004</b>	<b>0.0000</b>	<b>0.5741</b>	<b>0.5741</b>	<b>1.0000e-005</b>	<b>0.0000</b>	<b>0.5744</b>

**Mitigated Construction On-Site**

Category	tons/yr											MT/yr				
	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Off-Road	3.4400e-003	0.0339	0.0440	7.0000e-005	1.7400e-003	1.7400e-003	1.7400e-003	1.6000e-003	1.6000e-003	1.6000e-003	0.0000	5.8848	5.8848	1.8700e-003	0.0000	5.9314
Paving	0.0000				0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
<b>Total</b>	<b>3.4400e-003</b>	<b>0.0339</b>	<b>0.0440</b>	<b>7.0000e-005</b>	<b>1.7400e-003</b>	<b>1.7400e-003</b>	<b>1.7400e-003</b>	<b>1.6000e-003</b>	<b>1.6000e-003</b>	<b>1.6000e-003</b>	<b>0.0000</b>	<b>5.8848</b>	<b>5.8848</b>	<b>1.8700e-003</b>	<b>0.0000</b>	<b>5.9314</b>

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**3.5 Paving - 2022**

**Mitigated Construction Off-Site**

Category	tons/yr											MT/yr				
	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	2.3000e-004	1.5000e-004	1.7400e-003	1.0000e-005	7.1000e-004	0.0000	7.2000e-004	1.9000e-004	0.0000	1.9000e-004	0.0000	0.5741	0.5741	1.0000e-005	0.0000	0.5744
<b>Total</b>	<b>2.3000e-004</b>	<b>1.5000e-004</b>	<b>1.7400e-003</b>	<b>1.0000e-005</b>	<b>7.1000e-004</b>	<b>0.0000</b>	<b>7.2000e-004</b>	<b>1.9000e-004</b>	<b>0.0000</b>	<b>1.9000e-004</b>	<b>0.0000</b>	<b>0.5741</b>	<b>0.5741</b>	<b>1.0000e-005</b>	<b>0.0000</b>	<b>0.5744</b>

**3.6 Architectural Coating - 2022**

**Unmitigated Construction On-Site**

Category	tons/yr											MT/yr				
	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Archit. Coating	0.2606					0.0000	0.0000		0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Off-Road	1.0200e-003	7.0400e-003	9.0700e-003	1.0000e-005	4.1000e-004	4.1000e-004	4.1000e-004	4.1000e-004	4.1000e-004	4.1000e-004	0.0000	1.2766	1.2766	8.0000e-005	0.0000	1.2787
<b>Total</b>	<b>0.2616</b>	<b>7.0400e-003</b>	<b>9.0700e-003</b>	<b>1.0000e-005</b>	<b>4.1000e-004</b>	<b>4.1000e-004</b>	<b>4.1000e-004</b>	<b>4.1000e-004</b>	<b>4.1000e-004</b>	<b>4.1000e-004</b>	<b>0.0000</b>	<b>1.2766</b>	<b>1.2766</b>	<b>8.0000e-005</b>	<b>0.0000</b>	<b>1.2787</b>



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**3.6 Architectural Coating - 2022**  
**Unmitigated Construction Off-Site**

Category	tons/yr											MT/yr				
	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	2.6000e-004	1.7000e-004	2.0100e-003	1.0000e-005	8.2000e-004	1.0000e-005	8.3000e-004	2.2000e-004	0.0000	2.2000e-004	0.0000	0.6624	1.0000e-005	0.0000	0.0000	0.6628
<b>Total</b>	<b>2.6000e-004</b>	<b>1.7000e-004</b>	<b>2.0100e-003</b>	<b>1.0000e-005</b>	<b>8.2000e-004</b>	<b>1.0000e-005</b>	<b>8.3000e-004</b>	<b>2.2000e-004</b>	<b>0.0000</b>	<b>2.2000e-004</b>	<b>0.0000</b>	<b>0.6624</b>	<b>1.0000e-005</b>	<b>0.0000</b>	<b>0.0000</b>	<b>0.6628</b>

**Mitigated Construction On-Site**

Category	tons/yr											MT/yr				
	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Archit. Coating	0.2606					0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Off-Road	1.0200e-003	7.0400e-003	9.0700e-003	1.0000e-005	4.1000e-004	4.1000e-004	4.1000e-004	4.1000e-004	4.1000e-004	4.1000e-004	0.0000	1.2766	8.0000e-005	0.0000	0.0000	1.2787
<b>Total</b>	<b>0.2616</b>	<b>7.0400e-003</b>	<b>9.0700e-003</b>	<b>1.0000e-005</b>	<b>4.1000e-004</b>	<b>4.1000e-004</b>	<b>4.1000e-004</b>	<b>4.1000e-004</b>	<b>4.1000e-004</b>	<b>4.1000e-004</b>	<b>0.0000</b>	<b>1.2766</b>	<b>8.0000e-005</b>	<b>0.0000</b>	<b>0.0000</b>	<b>1.2787</b>

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**3.6 Architectural Coating - 2022**

**Mitigated Construction Off-Site**

Category	tons/yr										MT/yr					
	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	2.6000e-004	1.7000e-004	2.0100e-003	1.0000e-005	8.2000e-004	1.0000e-005	8.3000e-004	2.2000e-004	0.0000	2.2000e-004	0.0000	0.6624	0.6624	1.0000e-005	0.0000	0.6628
<b>Total</b>	<b>2.6000e-004</b>	<b>1.7000e-004</b>	<b>2.0100e-003</b>	<b>1.0000e-005</b>	<b>8.2000e-004</b>	<b>1.0000e-005</b>	<b>8.3000e-004</b>	<b>2.2000e-004</b>	<b>0.0000</b>	<b>2.2000e-004</b>	<b>0.0000</b>	<b>0.6624</b>	<b>0.6624</b>	<b>1.0000e-005</b>	<b>0.0000</b>	<b>0.6628</b>

**4.0 Operational Detail - Mobile**

**4.1 Mitigation Measures Mobile**

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Category	tons/yr											MT/yr				
	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Mitigated	0.1917	0.7082	2.4346	9.4500e-003	0.8988	6.6100e-003	0.9054	0.2407	6.1400e-003	0.2468	0.0000	872.3746	872.3746	0.0354	0.0000	873.2598
Unmitigated	0.1917	0.7082	2.4346	9.4500e-003	0.8988	6.6100e-003	0.9054	0.2407	6.1400e-003	0.2468	0.0000	872.3746	872.3746	0.0354	0.0000	873.2598

4.2 Trip Summary Information

Land Use	Average Daily Trip Rate			Unmitigated Annual VMT	Mitigated Annual VMT
	Weekday	Saturday	Sunday		
Apartment Low Rise	344.76	344.76	344.76	1,178,097	1,178,097
Enclosed Parking with Elevator	0.00	0.00	0.00		
Medical Office Building	309.03	88.95	15.39	611,213	611,213
Strip Mall	304.97	304.97	304.97	580,226	580,226
Total	958.75	738.67	665.11	2,369,536	2,369,536

4.3 Trip Type Information

Land Use	Miles			Trip %			Trip Purpose %		
	H-W or C-W	H-S or C-C	H-O or C-NW	H-W or C-W	H-S or C-C	H-O or C-NW	Primary	Diversified	Pass-by
Apartment Low Rise	14.70	5.90	8.70	40.20	19.20	40.60	86	11	3
Enclosed Parking with Elevator	16.60	8.40	6.90	0.00	0.00	0.00	0	0	0
Medical Office Building	16.60	8.40	6.90	29.60	51.40	19.00	60	30	10
Strip Mall	16.60	8.40	6.90	16.60	64.40	19.00	45	40	15

4.4 Fleet Mix

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Land Use	LDA	LDT1	LDT2	MDV	LHD1	LHD2	MHD	HHD	OBUS	UBUS	MCY	SBUS	MH
Apartments Low Rise	0.563406	0.043070	0.209298	0.109958	0.015015	0.005784	0.026182	0.017546	0.001775	0.001524	0.004941	0.000598	0.000904
Enclosed Parking with Elevator	0.563406	0.043070	0.209298	0.109958	0.015015	0.005784	0.026182	0.017546	0.001775	0.001524	0.004941	0.000598	0.000904
Medical Office Building	0.563406	0.043070	0.209298	0.109958	0.015015	0.005784	0.026182	0.017546	0.001775	0.001524	0.004941	0.000598	0.000904
Strip Mall	0.563406	0.043070	0.209298	0.109958	0.015015	0.005784	0.026182	0.017546	0.001775	0.001524	0.004941	0.000598	0.000904

5.0 Energy Detail

Historical Energy Use: N

5.1 Mitigation Measures Energy

Category	tons/yr										MT/yr					
	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Electricity Mitigated						0.0000	0.0000		0.0000	0.0000	0.0000	281.9224	281.9224	0.0116	2.4100e-003	282.9310
Electricity Unmitigated						0.0000	0.0000		0.0000	0.0000	0.0000	281.9224	281.9224	0.0116	2.4100e-003	282.9310
Natural Gas Mitigated	4.1600e-003	0.0359	0.0175	2.3000e-004	2.8800e-003	2.8800e-003	2.8800e-003	2.8800e-003	2.8800e-003	2.8800e-003	0.0000	41.2096	41.2096	7.9000e-004	7.6000e-004	41.4545
Natural Gas Unmitigated	4.1600e-003	0.0359	0.0175	2.3000e-004	2.8800e-003	2.8800e-003	2.8800e-003	2.8800e-003	2.8800e-003	2.8800e-003	0.0000	41.2096	41.2096	7.9000e-004	7.6000e-004	41.4545

Garden Grove Blvd Mixed Use Proj 2021 - Orange County, Annual

**5.2 Energy by Land Use - NaturalGas**

**Unmitigated**

Land Use	NaturalGas Use kBTU/yr	tons/yr										MT/yr					
		ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Apartments Low Rise	663499	3.5800e-003	0.0306	0.0130	2.0000e-004	2.4700e-003	2.4700e-003	2.4700e-003	2.4700e-003	2.4700e-003	2.4700e-003	0.0000	35.4068	35.4068	6.8000e-004	6.5000e-004	35.6172
Enclosed Parking with Elevator	0	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Medical Office Building	90732.8	4.9000e-004	4.4500e-003	3.7400e-003	3.0000e-005	3.4000e-004	3.4000e-004	3.4000e-004	3.4000e-004	3.4000e-004	3.4000e-004	0.0000	4.8419	4.8419	9.0000e-005	9.0000e-005	4.8706
Strip Mall	18008	1.0000e-004	8.8000e-004	7.4000e-004	1.0000e-005	7.0000e-005	7.0000e-005	7.0000e-005	7.0000e-005	7.0000e-005	7.0000e-005	0.0000	0.9610	0.9610	2.0000e-005	2.0000e-005	0.9667
<b>Total</b>		<b>4.1700e-003</b>	<b>0.0359</b>	<b>0.0175</b>	<b>2.4000e-004</b>	<b>2.8800e-003</b>	<b>2.8800e-003</b>	<b>2.8800e-003</b>	<b>2.8800e-003</b>	<b>2.8800e-003</b>	<b>2.8800e-003</b>	<b>0.0000</b>	<b>41.2097</b>	<b>41.2097</b>	<b>7.9000e-004</b>	<b>7.6000e-004</b>	<b>41.4545</b>

Garden Grove Blvd Mixed Use Proj 2021 - Orange County, Annual

5.2 Energy by Land Use - NaturalGas

**Mitigated**

Land Use	NaturalGas Use kBTU/yr	ROG	NOx	CO	SO2	tons/yr					MT/yr						
						Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Apartments Low Rise	663499	3.5800e-003	0.0306	0.0130	2.0000e-004	2.4700e-003	2.4700e-003	2.4700e-003	0.0000	2.4700e-003	2.4700e-003	0.0000	35.4068	35.4068	6.8000e-004	6.5000e-004	35.6172
Enclosed Parking with Elevator	0	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Medical Office Building	90732.8	4.9000e-004	4.4500e-003	3.7400e-003	3.0000e-005	3.4000e-004	3.4000e-004	3.4000e-004	0.0000	3.4000e-004	3.4000e-004	0.0000	4.8419	4.8419	9.0000e-005	9.0000e-005	4.8706
Strip Mall	18008	1.0000e-004	8.8000e-004	7.4000e-004	1.0000e-005	7.0000e-005	7.0000e-005	7.0000e-005	0.0000	7.0000e-005	7.0000e-005	0.0000	0.9610	0.9610	2.0000e-005	2.0000e-005	0.9667
<b>Total</b>		<b>4.1700e-003</b>	<b>0.0359</b>	<b>0.0175</b>	<b>2.4000e-004</b>	<b>2.8800e-003</b>	<b>2.8800e-003</b>	<b>2.8800e-003</b>	<b>0.0000</b>	<b>2.8800e-003</b>	<b>2.8800e-003</b>	<b>0.0000</b>	<b>41.2097</b>	<b>41.2097</b>	<b>7.9000e-004</b>	<b>7.6000e-004</b>	<b>41.4545</b>

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**5.3 Energy by Land Use - Electricity**

**Unmitigated**

Land Use	Electricity Use kWh/yr	Total CO2	CH4	N2O	CO2e
MT/yr					
Apartments Low Rise	216327	68.9263	2.8500e-003	5.9000e-004	69.1729
Enclosed Parking with Elevator	426608	135.9265	5.6100e-003	1.1600e-003	136.4127
Medical Office Building	138879	44.2497	1.8300e-003	3.8000e-004	44.4080
Strip Mall	103006	32.8198	1.3500e-003	2.8000e-004	32.9373
<b>Total</b>		<b>281.9224</b>	<b>0.0116</b>	<b>2.4100e-003</b>	<b>282.9310</b>

**5.3 Energy by Land Use - Electricity**

**Mitigated**

Land Use	Electricity Use kWh/yr	Total CO2	CH4	N2O	CO2e
			MT/yr		
Apartment's Low Rise	216327	68.9263	2.8500e-003	5.9000e-004	69.1729
Enclosed Parking with Elevator	426608	135.9265	5.6100e-003	1.1600e-003	136.4127
Medical Office Building	138879	44.2497	1.8300e-003	3.8000e-004	44.4080
Strip Mall	103006	32.8198	1.3500e-003	2.8000e-004	32.9373
<b>Total</b>		<b>281.9224</b>	<b>0.0116</b>	<b>2.4100e-003</b>	<b>282.9310</b>

**6.0 Area Detail**

**6.1 Mitigation Measures Area**

Use only Natural Gas Hearths



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Category	ROG	NOx	CO	SO2	tons/yr			MT/yr					CO2e			
					Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	
Mitigated	0.3046	0.0159	0.5431	9.0000e-005	3.7600e-003	3.7600e-003	3.7600e-003	3.7600e-003	3.7600e-003	3.7600e-003	0.0000	12.1194	12.1194	1.0700e-003	2.1000e-004	12.2075
Unmitigated	0.4738	0.0197	0.8695	8.7000e-004	0.0526	0.0526	0.0526	0.0526	0.0526	0.0526	5.5234	11.4950	17.0184	0.0173	3.7000e-004	17.5633

**6.2 Area by SubCategory**  
**Unmitigated**

SubCategory	ROG	NOx	CO	SO2	tons/yr			MT/yr					CO2e			
					Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	
Architectural Coating	0.0261					0.0000	0.0000		0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Consumer Products	0.2610					0.0000	0.0000		0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Hearth	0.1703	0.0135	0.3306	8.4000e-004	0.0497	0.0497	0.0497	0.0497	0.0497	0.0497	5.5234	10.6141	16.1375	0.0165	3.7000e-004	16.6610
Landscaping	0.0164	6.2100e-003	0.5389	3.0000e-005	2.9800e-003	2.9800e-003	2.9800e-003	2.9800e-003	2.9800e-003	2.9800e-003	0.0000	0.8810	0.8810	8.6000e-004	0.0000	0.9023
<b>Total</b>	<b>0.4738</b>	<b>0.0197</b>	<b>0.8695</b>	<b>8.7000e-004</b>	<b>0.0526</b>	<b>0.0526</b>	<b>0.0526</b>	<b>0.0526</b>	<b>0.0526</b>	<b>0.0526</b>	<b>5.5234</b>	<b>11.4950</b>	<b>17.0184</b>	<b>0.0173</b>	<b>3.7000e-004</b>	<b>17.5633</b>

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**6.2 Area by SubCategory**

**Mitigated**

SubCategory	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio-CO2	NBio-CO2	Total CO2	CH4	N2O	CO2e
tons/yr																
MT/yr																
Architectural Coating	0.0261					0.0000	0.0000		0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Consumer Products	0.2610					0.0000	0.0000		0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Hearth	1.1400e-003	9.7000e-003	4.1300e-003	6.0000e-005		7.8000e-004	7.8000e-004		7.8000e-004	7.8000e-004	0.0000	11.2384	11.2384	2.2000e-004	2.1000e-004	11.3052
Landscaping	0.0164	6.2100e-003	0.5389	3.0000e-005		2.9800e-003	2.9800e-003		2.9800e-003	2.9800e-003	0.0000	0.8810	0.8810	8.6000e-004	0.0000	0.9023
<b>Total</b>	<b>0.3046</b>	<b>0.0159</b>	<b>0.5431</b>	<b>9.0000e-005</b>		<b>3.7600e-003</b>	<b>3.7600e-003</b>		<b>3.7600e-003</b>	<b>3.7600e-003</b>	<b>0.0000</b>	<b>12.1194</b>	<b>12.1194</b>	<b>1.0800e-003</b>	<b>2.1000e-004</b>	<b>12.2075</b>

**7.0 Water Detail**

**7.1 Mitigation Measures Water**

Garden Grove Blvd Mixed Use Proj 2021 - Orange County, Annual

Category	Total CO2				CO2e			
	CH4	N2O	MT/yr		CH4	N2O	CO2e	
Mitigated	0.1740	4.3500e-003	33.5204	39.1677	0.1740	4.3500e-003	33.5204	39.1677
Unmitigated	0.1740	4.3500e-003	33.5204	39.1677	0.1740	4.3500e-003	33.5204	39.1677

**7.2 Water by Land Use**  
**Unmitigated**

Land Use	Indoor/Outdoor Use	Mgal	Total CO2				CO2e
			CH4	N2O	MT/yr		
Apartments Low Rise	3.38801 / 2.13592	22.6818	0.1113	2.7900e-003	26.3059		
Enclosed Parking with Elevator	0 / 0	0.0000	0.0000	0.0000	0.0000		
Medical Office Building	1.24602 / 0.237337	6.4049	0.0409	1.0100e-003	7.7271		
Strip Mall	0.666653 / 0.408594	4.4237	0.0219	5.5000e-004	5.1347		
<b>Total</b>		<b>33.5204</b>	<b>0.1740</b>	<b>4.3500e-003</b>	<b>39.1677</b>		

Garden Grove Blvd Mixed Use Proj 2021 - Orange County, Annual

**7.2 Water by Land Use**

**Mitigated**

Land Use	Indoor/Outdoor Use Mgal	Total CO2			CH4	N2O	CO2e
		MT/yr					
Apartments Low Rise	3.38801 / 2.13592	22.6918	0.1113	2.7900e-003		26.3059	
Enclosed Parking with Elevator	0 / 0	0.0000	0.0000	0.0000		0.0000	
Medical Office Building	1.24602 / 0.237337	6.4049	0.0409	1.0100e-003		7.7271	
Strip Mall	0.666653 / 0.408594	4.4237	0.0219	5.5000e-004		5.1347	
<b>Total</b>		<b>33.5204</b>	<b>0.1740</b>	<b>4.3500e-003</b>		<b>39.1677</b>	

**8.0 Waste Detail**

**8.1 Mitigation Measures Waste**

Garden Grove Blvd Mixed Use Proj 2021 - Orange County, Annual

Category/Year

	Total CO2	CH4	N2O	CO2e
	MT/yr			
Mitigated	28.5426	1.6868	0.0000	70.7130
Unmitigated	28.5426	1.6868	0.0000	70.7130

**8.2 Waste by Land Use**

**Unmitigated**

Land Use	Waste Disposed tons	Total CO2	CH4	N2O	CO2e
Apartment Low Rise	23.92	4.8555	0.2870	0.0000	12.0294
Enclosed Parking with Elevator	0	0.0000	0.0000	0.0000	0.0000
Medical Office Building	107.24	21.7688	1.2865	0.0000	53.9312
Strip Mall	9.45	1.9183	0.1134	0.0000	4.7524
<b>Total</b>		<b>28.5426</b>	<b>1.6868</b>	<b>0.0000</b>	<b>70.7130</b>

Garden Grove Blvd Mixed Use Proj 2021 - Orange County, Annual

**8.2 Waste by Land Use**

**Mitigated**

Land Use	Waste Disposed tons	Total CO2	CH4	N2O	CO2e
Apartment Low Rise	23.92	4.8555	0.2870	0.0000	12.0294
Enclosed Parking with Elevator	0	0.0000	0.0000	0.0000	0.0000
Medical Office Building	107.24	21.7688	1.2865	0.0000	53.9312
Strip Mall	9.45	1.9183	0.1134	0.0000	4.7524
<b>Total</b>		<b>28.5426</b>	<b>1.6868</b>	<b>0.0000</b>	<b>70.7130</b>

**9.0 Operational Offroad**

Equipment Type	Number	Hours/Day	Days/Year	Horse Power	Load Factor	Fuel Type
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**10.0 Stationary Equipment**

**Fire Pumps and Emergency Generators**

Equipment Type	Number	Hours/Day	Hours/Year	Horse Power	Load Factor	Fuel Type
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**Boilers**

Equipment Type	Number	Heat Input/Day	Heat Input/Year	Boiler Rating	Fuel Type
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**User Defined Equipment**

Garden Grove Blvd Mixed Use Proj 2021 - Orange County, Annual

Equipment Type	Number
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**11.0 Vegetation**

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Garden Grove Blvd Mixed Use Proj 2021 - Orange County, Summer

### Garden Grove Blvd Mixed Use Proj 2021

Orange County, Summer

## 1.0 Project Characteristics

### 1.1 Land Usage

Land Uses	Size	Metric	Lot Acreage	Floor Surface Area	Population
Medical Office Building	9.93	1000sqft	0.23	9,927.00	0
Enclosed Parking with Elevator	182.00	Space	0.00	72,800.00	0
Apartments Low Rise	52.00	Dwelling Unit	1.42	52,000.00	149
Strip Mall	9.00	1000sqft	0.21	9,004.00	0

### 1.2 Other Project Characteristics

Urbanization	Urban	Wind Speed (m/s)	2.2	Precipitation Freq (Days)	30
Climate Zone	8			Operational Year	2023

Utility Company Southern California Edison

CO2 Intensity (lb/MW/hr)	702.44	CH4 Intensity (lb/MW/hr)	0.029	N2O Intensity (lb/MW/hr)	0.006
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### 1.3 User Entered Comments & Non-Default Data



Garden Grove Blvd Mixed Use Proj 2021 - Orange County, Summer

Project Characteristics -

Land Use - 1.86 acre site

Construction Phase - Demo: 20 days, Grading: 4 days, Construction: 200 days, Paving: 10 days

Trips and VMT - site demoed

Demolition -

Vehicle Trips - trip rates per traffic report: 959 TPD

Construction Off-road Equipment Mitigation -

Area Mitigation -

Grading - 480 CU Export

Table Name	Column Name	Default Value	New Value
tblGrading	MaterialExported	0.00	480.00
tblLandUse	LotAcreage	1.64	0.00
tblLandUse	LotAcreage	3.25	1.42
tblVehicleTrips	ST_TR	7.16	6.63
tblVehicleTrips	ST_TR	42.04	33.87
tblVehicleTrips	SU_TR	6.07	6.63
tblVehicleTrips	SU_TR	20.43	33.87
tblVehicleTrips	WD_TR	6.59	6.63
tblVehicleTrips	WD_TR	36.13	31.13
tblVehicleTrips	WD_TR	44.32	33.87

**2.0 Emissions Summary**



Garden Grove Blvd Mixed Use Proj 2021 - Orange County, Summer

**2.2 Overall Operational**  
**Unmitigated Operational**

Category	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
lb/day																
Area	15.3300	1.1286	30.7562	0.0677	3.9960	3.9960	3.9960	3.9960	3.9960	3.9960	487.0798	943.7687	1,430.8485	1.4601	0.0331	1,477.2029
Energy	0.0228	0.1967	0.0958	1.2400e-003	0.0158	0.0158	0.0158	0.0158	0.0158	0.0158	248.9088	248.9088	248.9088	4.7700e-003	4.5600e-003	250.3879
Mobile	1.1990	4.0443	14.8603	0.0580	5.4306	0.0393	5.4699	1.4522	0.0364	1.4886	5,899.0096	5,899.0096	5,899.0096	0.2327		5,904.8273
<b>Total</b>	<b>16.5519</b>	<b>5.3696</b>	<b>45.7123</b>	<b>0.1269</b>	<b>5.4306</b>	<b>4.0510</b>	<b>9.4816</b>	<b>1.4522</b>	<b>4.0482</b>	<b>5.5004</b>	<b>487.0798</b>	<b>7,091.6871</b>	<b>7,578.7668</b>	<b>1.6976</b>	<b>0.0376</b>	<b>7,632.4181</b>

**Mitigated Operational**

Category	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
lb/day																
Area	1.7950	0.8260	4.6419	5.1800e-003	0.0866	0.0866	0.0866	0.0866	0.0866	0.0866	0.0000	998.8275	998.8275	0.0265	0.0182	1,004.9055
Energy	0.0228	0.1967	0.0958	1.2400e-003	0.0158	0.0158	0.0158	0.0158	0.0158	0.0158	248.9088	248.9088	248.9088	4.7700e-003	4.5600e-003	250.3879
Mobile	1.1990	4.0443	14.8603	0.0580	5.4306	0.0393	5.4699	1.4522	0.0364	1.4886	5,899.0096	5,899.0096	5,899.0096	0.2327		5,904.8273
<b>Total</b>	<b>3.0169</b>	<b>5.0670</b>	<b>19.5980</b>	<b>0.0644</b>	<b>5.4306</b>	<b>0.1416</b>	<b>5.5722</b>	<b>1.4522</b>	<b>0.1388</b>	<b>1.5910</b>	<b>0.0000</b>	<b>7,146.7459</b>	<b>7,146.7459</b>	<b>0.2640</b>	<b>0.0227</b>	<b>7,160.1207</b>

Garden Grove Blvd Mixed Use Proj 2021 - Orange County, Summer

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio-CO2	NBio-CO2	Total CO2	CH4	N2O	CO2e
Percent Reduction	81.77	5.64	57.13	49.25	0.00	96.50	41.23	0.00	96.57	71.08	100.00	-0.78	5.70	84.45	39.58	6.19

### 3.0 Construction Detail

#### Construction Phase

Phase Number	Phase Name	Phase Type	Start Date	End Date	Num Days Week	Num Days	Phase Description
1	Demolition	Demolition	1/1/2022	1/28/2022	5	20	
2	Grading	Grading	1/29/2022	2/3/2022	5	4	
3	Building Construction	Building Construction	2/4/2022	11/10/2022	5	200	
4	Paving	Paving	11/11/2022	11/24/2022	5	10	
5	Architectural Coating	Architectural Coating	11/25/2022	12/8/2022	5	10	

Acres of Grading (Site Preparation Phase): 0

Acres of Grading (Grading Phase): 1.5

Acres of Paving: 0

Residential Indoor: 105,300; Residential Outdoor: 35,100; Non-Residential Indoor: 28,397; Non-Residential Outdoor: 9,466; Striped Parking Area: 4,368 (Architectural Coating – sqft)

#### OffRoad Equipment

Garden Grove Blvd Mixed Use Proj 2021 - Orange County, Summer

Phase Name	Offroad Equipment Type	Amount	Usage Hours	Horse Power	Load Factor
Demolition	Concrete/Industrial Saws	1	8.00	81	0.73
Demolition	Rubber Tired Dozers	1	8.00	247	0.40
Demolition	Tractors/Loaders/Backhoes	3	8.00	97	0.37
Grading	Graders	1	6.00	187	0.41
Grading	Rubber Tired Dozers	1	6.00	247	0.40
Grading	Tractors/Loaders/Backhoes	1	7.00	97	0.37
Building Construction	Cranes	1	6.00	231	0.29
Building Construction	Forklifts	1	6.00	89	0.20
Building Construction	Generator Sets	1	8.00	84	0.74
Building Construction	Tractors/Loaders/Backhoes	1	6.00	97	0.37
Building Construction	Welders	3	8.00	46	0.45
Paving	Cement and Mortar Mixers	1	6.00	9	0.56
Paving	Pavers	1	6.00	130	0.42
Paving	Paving Equipment	1	8.00	132	0.36
Paving	Rollers	1	7.00	80	0.38
Paving	Tractors/Loaders/Backhoes	1	8.00	97	0.37
Architectural Coating	Air Compressors	1	6.00	78	0.48

Trips and VMT

Phase Name	Offroad Equipment Count	Worker Trip Number	Vendor Trip Number	Hauling Trip Number	Worker Trip Length	Vendor Trip Length	Hauling Trip Length	Worker Vehicle Class	Vendor Vehicle Class	Hauling Vehicle Class
Demolition	5	13.00	0.00	0.00	14.70	6.90	20.00	LD_Mix	HDT_Mix	HHDT
Grading	3	8.00	0.00	60.00	14.70	6.90	20.00	LD_Mix	HDT_Mix	HHDT
Building Construction	7	74.00	21.00	0.00	14.70	6.90	20.00	LD_Mix	HDT_Mix	HHDT
Paving	5	13.00	0.00	0.00	14.70	6.90	20.00	LD_Mix	HDT_Mix	HHDT
Architectural Coating	1	15.00	0.00	0.00	14.70	6.90	20.00	LD_Mix	HDT_Mix	HHDT

Garden Grove Blvd Mixed Use Proj 2021 - Orange County, Summer

**3.1 Mitigation Measures Construction**

Water Exposed Area

**3.2 Demolition - 2022**

**Unmitigated Construction On-Site**

Category	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
lb/day																
Fugitive Dust					0.0000	0.0000	0.0000	0.0000	0.0000	0.0000			0.0000			0.0000
Off-Road	1.6889	16.6217	13.9605	0.0241		0.8379	0.8379		0.7829	0.7829		2.323,416 8	2.323,416 8	0.5921		2,338,219 1
<b>Total</b>	<b>1.6889</b>	<b>16.6217</b>	<b>13.9605</b>	<b>0.0241</b>	<b>0.0000</b>	<b>0.8379</b>	<b>0.8379</b>	<b>0.0000</b>	<b>0.7829</b>	<b>0.7829</b>		<b>2,323,416 8</b>	<b>2,323,416 8</b>	<b>0.5921</b>		<b>2,338,219 1</b>

Garden Grove Blvd Mixed Use Proj 2021 - Orange County, Summer

**3.2 Demolition - 2022**

**Unmitigated Construction Off-Site**

Category	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
lb/day																
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000
Worker	0.0443	0.0257	0.3685	1.3200e-003	0.1453	9.2000e-004	0.1462	0.0385	8.5000e-004	0.0394	131.7162	131.7162	2.6600e-003	131.7827		131.7827
<b>Total</b>	<b>0.0443</b>	<b>0.0257</b>	<b>0.3685</b>	<b>1.3200e-003</b>	<b>0.1453</b>	<b>9.2000e-004</b>	<b>0.1462</b>	<b>0.0385</b>	<b>8.5000e-004</b>	<b>0.0394</b>	<b>131.7162</b>	<b>131.7162</b>	<b>2.6600e-003</b>	<b>131.7827</b>		<b>131.7827</b>

**Mitigated Construction On-Site**

Category	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
lb/day																
Fugitive Dust					0.0000	0.0000	0.0000	0.0000	0.0000	0.0000			0.0000			0.0000
Off-Road	1.6889	16.6217	13.9605	0.0241	0.8379	0.8379	0.8379	0.7829	0.7829	0.7829	0.0000	2.323.4168	2.323.4168	0.5921		2.338.2191
<b>Total</b>	<b>1.6889</b>	<b>16.6217</b>	<b>13.9605</b>	<b>0.0241</b>	<b>0.8379</b>	<b>0.8379</b>	<b>0.8379</b>	<b>0.7829</b>	<b>0.7829</b>	<b>0.7829</b>	<b>0.0000</b>	<b>2.323.4168</b>	<b>2.323.4168</b>	<b>0.5921</b>		<b>2.338.2191</b>

Garden Grove Blvd Mixed Use Proj 2021 - Orange County, Summer

**3.2 Demolition - 2022**

**Mitigated Construction Off-Site**

Category	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
lb/day																
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	0.0443	0.0257	0.3685	1.3200e-003	0.1453	9.2000e-004	0.1462	0.0385	8.5000e-004	0.0394	131.7162	131.7162	2.6600e-003	131.7827		131.7827
<b>Total</b>	<b>0.0443</b>	<b>0.0257</b>	<b>0.3685</b>	<b>1.3200e-003</b>	<b>0.1453</b>	<b>9.2000e-004</b>	<b>0.1462</b>	<b>0.0385</b>	<b>8.5000e-004</b>	<b>0.0394</b>	<b>131.7162</b>	<b>131.7162</b>	<b>2.6600e-003</b>	<b>131.7827</b>		<b>131.7827</b>

**3.3 Grading - 2022**

**Unmitigated Construction On-Site**

Category	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
lb/day																
Fugitive Dust					4.9278	0.0000	4.9278	2.5277	0.0000	2.5277			0.0000			0.0000
Off-Road	1.0832	12.0046	5.9360	0.0141	0.5173	0.5173	0.5173	0.4759	0.4759	0.4759	1,364.8198	1,364.8198	0.4414	0.4414		1,375.8551
<b>Total</b>	<b>1.0832</b>	<b>12.0046</b>	<b>5.9360</b>	<b>0.0141</b>	<b>4.9278</b>	<b>0.5173</b>	<b>5.4451</b>	<b>2.5277</b>	<b>0.4759</b>	<b>3.0036</b>	<b>1,364.8198</b>	<b>1,364.8198</b>	<b>0.4414</b>	<b>0.4414</b>		<b>1,375.8551</b>



Garden Grove Blvd Mixed Use Proj 2021 - Orange County, Summer

**3.3 Grading - 2022**

**Unmitigated Construction Off-Site**

Category	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
lb/day																
Hauling	0.1025	3.5039	1.0531	0.0111	0.2611	0.0104	0.2715	0.0715	9.9100e-003	0.0814		1,247.2797	1,247.2797	0.1294		1,250.5149
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Worker	0.0273	0.0158	0.2268	8.1000e-004	0.0894	5.7000e-004	0.0900	0.0237	5.2000e-004	0.0242		81.0561	81.0561	1.6400e-003		81.0970
<b>Total</b>	<b>0.1298</b>	<b>3.5197</b>	<b>1.2799</b>	<b>0.0120</b>	<b>0.3506</b>	<b>0.0109</b>	<b>0.3615</b>	<b>0.0952</b>	<b>0.0104</b>	<b>0.1056</b>		<b>1,328.3358</b>	<b>1,328.3358</b>	<b>0.1311</b>		<b>1,331.6119</b>

**Mitigated Construction On-Site**

Category	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
lb/day																
Fugitive Dust					2.2175	0.0000	2.2175	1.1375	0.0000	1.1375			0.0000			0.0000
Off-Road	1.0832	12.0046	5.9360	0.0141		0.5173	0.5173	0.4759	0.4759	0.4759		1,364.8198	1,364.8198	0.4414		1,375.8551
<b>Total</b>	<b>1.0832</b>	<b>12.0046</b>	<b>5.9360</b>	<b>0.0141</b>	<b>2.2175</b>	<b>0.5173</b>	<b>2.7348</b>	<b>1.1375</b>	<b>0.4759</b>	<b>1.6134</b>	<b>0.0000</b>	<b>1,364.8198</b>	<b>1,364.8198</b>	<b>0.4414</b>		<b>1,375.8551</b>

Garden Grove Blvd Mixed Use Proj 2021 - Orange County, Summer

**3.3 Grading - 2022**

**Mitigated Construction Off-Site**

Category	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
lb/day																
Hauling	0.1025	3.5039	1.0531	0.0111	0.2611	0.0104	0.2715	0.0715	9.9100e-003	0.0814		1,247.2797	1,247.2797	0.1294		1,250.5149
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000	0.0000		0.0000
Worker	0.0273	0.0158	0.2268	8.1000e-004	0.0894	5.7000e-004	0.0900	0.0237	5.2000e-004	0.0242		81.0561	81.0561	1.6400e-003		81.0970
<b>Total</b>	<b>0.1298</b>	<b>3.5197</b>	<b>1.2799</b>	<b>0.0120</b>	<b>0.3506</b>	<b>0.0109</b>	<b>0.3615</b>	<b>0.0952</b>	<b>0.0104</b>	<b>0.1056</b>		<b>1,328.3358</b>	<b>1,328.3358</b>	<b>0.1311</b>		<b>1,331.6119</b>

**3.4 Building Construction - 2022**

**Unmitigated Construction On-Site**

Category	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
lb/day																
Off-Road	1.6487	12.5031	12.7264	0.0221		0.5889	0.5889		0.5689	0.5689		2,001.5429	2,001.5429	0.3486		2,010.2581
<b>Total</b>	<b>1.6487</b>	<b>12.5031</b>	<b>12.7264</b>	<b>0.0221</b>		<b>0.5889</b>	<b>0.5889</b>		<b>0.5689</b>	<b>0.5689</b>		<b>2,001.5429</b>	<b>2,001.5429</b>	<b>0.3486</b>		<b>2,010.2581</b>

Garden Grove Blvd Mixed Use Proj 2021 - Orange County, Summer

**3.4 Building Construction - 2022**  
**Unmitigated Construction Off-Site**

Category	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
lb/day																
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000
Vendor	0.0527	1.8639	0.5159	5.1200e-003	0.1342	3.5600e-003	0.1377	0.0386	3.4000e-003	0.0420		558.9205	558.9205	0.0429		559.9924
Worker	0.2524	0.1464	2.0977	7.5200e-003	0.8272	5.2500e-003	0.8324	0.2194	4.8300e-003	0.2242		749.7689	749.7689	0.0151		750.1475
<b>Total</b>	<b>0.3051</b>	<b>2.0103</b>	<b>2.6136</b>	<b>0.0126</b>	<b>0.9613</b>	<b>8.8100e-003</b>	<b>0.9701</b>	<b>0.2580</b>	<b>8.2300e-003</b>	<b>0.2662</b>		<b>1,308.6894</b>	<b>1,308.6894</b>	<b>0.0580</b>		<b>1,310.1399</b>

**Mitigated Construction On-Site**

Category	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
lb/day																
Off-Road	1.6487	12.5031	12.7264	0.0221		0.5889	0.5889		0.5689	0.5689	0.0000	2,001.5429	2,001.5429	0.3486		2,010.2581
<b>Total</b>	<b>1.6487</b>	<b>12.5031</b>	<b>12.7264</b>	<b>0.0221</b>		<b>0.5889</b>	<b>0.5889</b>		<b>0.5689</b>	<b>0.5689</b>	<b>0.0000</b>	<b>2,001.5429</b>	<b>2,001.5429</b>	<b>0.3486</b>		<b>2,010.2581</b>

Garden Grove Blvd Mixed Use Proj 2021 - Orange County, Summer

**3.4 Building Construction - 2022**

**Mitigated Construction Off-Site**

Category	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
lb/day																
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000
Vendor	0.0527	1.8639	0.5159	5.1200e-003	0.1342	3.5600e-003	0.1377	0.0386	3.4000e-003	0.0420		558.9205	558.9205	0.0429		559.9924
Worker	0.2524	0.1484	2.0977	7.5200e-003	0.8272	5.2500e-003	0.8324	0.2194	4.8300e-003	0.2242		749.7689	749.7689	0.0151		750.1475
<b>Total</b>	<b>0.3051</b>	<b>2.0103</b>	<b>2.6136</b>	<b>0.0126</b>	<b>0.9613</b>	<b>8.8100e-003</b>	<b>0.9701</b>	<b>0.2580</b>	<b>8.2300e-003</b>	<b>0.2662</b>		<b>1,308.6894</b>	<b>1,308.6894</b>	<b>0.0580</b>		<b>1,310.1399</b>

**3.5 Paving - 2022**

**Unmitigated Construction On-Site**

Category	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
lb/day																
Off-Road	0.6877	6.7738	8.8060	0.0135		0.3474	0.3474		0.3205	0.3205		1,297.3789	1,297.3789	0.4113		1,307.6608
Paving	0.0000					0.0000	0.0000		0.0000	0.0000			0.0000			0.0000
<b>Total</b>	<b>0.6877</b>	<b>6.7738</b>	<b>8.8060</b>	<b>0.0135</b>		<b>0.3474</b>	<b>0.3474</b>		<b>0.3205</b>	<b>0.3205</b>		<b>1,297.3789</b>	<b>1,297.3789</b>	<b>0.4113</b>		<b>1,307.6608</b>

Garden Grove Blvd Mixed Use Proj 2021 - Orange County, Summer

**3.5 Paving - 2022**

**Unmitigated Construction Off-Site**

Category	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
lb/day																
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	0.0443	0.0257	0.3685	1.3200e-003	0.1453	9.2000e-004	0.1462	0.0385	8.5000e-004	0.0394	131.7162	131.7162	131.7162	2.6600e-003		131.7827
<b>Total</b>	<b>0.0443</b>	<b>0.0257</b>	<b>0.3685</b>	<b>1.3200e-003</b>	<b>0.1453</b>	<b>9.2000e-004</b>	<b>0.1462</b>	<b>0.0385</b>	<b>8.5000e-004</b>	<b>0.0394</b>	<b>131.7162</b>	<b>131.7162</b>	<b>131.7162</b>	<b>2.6600e-003</b>		<b>131.7827</b>

**Mitigated Construction On-Site**

Category	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
lb/day																
Off-Road	0.6877	6.7738	8.8060	0.0135		0.3474	0.3474		0.3205	0.3205	0.0000	1,297.3789	1,297.3789	0.4113		1,307.6608
Paving	0.0000					0.0000	0.0000		0.0000	0.0000			0.0000			0.0000
<b>Total</b>	<b>0.6877</b>	<b>6.7738</b>	<b>8.8060</b>	<b>0.0135</b>		<b>0.3474</b>	<b>0.3474</b>		<b>0.3205</b>	<b>0.3205</b>	<b>0.0000</b>	<b>1,297.3789</b>	<b>1,297.3789</b>	<b>0.4113</b>		<b>1,307.6608</b>

Garden Grove Blvd Mixed Use Proj 2021 - Orange County, Summer

**3.5 Paving - 2022**

**Mitigated Construction Off-Site**

Category	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
lb/day																
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000
Worker	0.0443	0.0257	0.3685	1.3200e-003	0.1453	9.2000e-004	0.1462	0.0385	8.5000e-004	0.0394	131.7162	131.7162	2.6600e-003	131.7162		131.7827
<b>Total</b>	<b>0.0443</b>	<b>0.0257</b>	<b>0.3685</b>	<b>1.3200e-003</b>	<b>0.1453</b>	<b>9.2000e-004</b>	<b>0.1462</b>	<b>0.0385</b>	<b>8.5000e-004</b>	<b>0.0394</b>	<b>131.7162</b>	<b>131.7162</b>	<b>2.6600e-003</b>	<b>131.7162</b>		<b>131.7827</b>

**3.6 Architectural Coating - 2022**

**Unmitigated Construction On-Site**

Category	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
lb/day																
Archit. Coating	52.1118					0.0000	0.0000		0.0000	0.0000			0.0000			0.0000
Off-Road	0.2045	1.4085	1.8136	2.9700e-003	0.0817	0.0817	0.0817	0.0817	0.0817	0.0817	281.4481	281.4481	0.0183	0.0183		281.9062
<b>Total</b>	<b>52.3163</b>	<b>1.4085</b>	<b>1.8136</b>	<b>2.9700e-003</b>	<b>0.0817</b>	<b>0.0817</b>	<b>0.0817</b>	<b>0.0817</b>	<b>0.0817</b>	<b>0.0817</b>	<b>281.4481</b>	<b>281.4481</b>	<b>0.0183</b>	<b>0.0183</b>		<b>281.9062</b>

Garden Grove Blvd Mixed Use Proj 2021 - Orange County, Summer

**3.6 Architectural Coating - 2022**  
**Unmitigated Construction Off-Site**

Category	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
lb/day																
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000		0.0000
Worker	0.0512	0.0297	0.4252	1.5200e-003	0.1677	1.0600e-003	0.1687	0.0445	9.8000e-004	0.0455	151.9802	151.9802	3.0700e-003	0.0000		152.0569
<b>Total</b>	<b>0.0512</b>	<b>0.0297</b>	<b>0.4252</b>	<b>1.5200e-003</b>	<b>0.1677</b>	<b>1.0600e-003</b>	<b>0.1687</b>	<b>0.0445</b>	<b>9.8000e-004</b>	<b>0.0455</b>	<b>151.9802</b>	<b>151.9802</b>	<b>3.0700e-003</b>	<b>0.0000</b>		<b>152.0569</b>

**Mitigated Construction On-Site**

Category	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
lb/day																
Archit. Coating	52.1118					0.0000	0.0000		0.0000	0.0000			0.0000			0.0000
Off-Road	0.2045	1.4085	1.8136	2.9700e-003	0.0817	0.0817	0.0817	0.0817	0.0817	0.0817	0.0000	281.4481	281.4481	0.0183		281.9062
<b>Total</b>	<b>52.3163</b>	<b>1.4085</b>	<b>1.8136</b>	<b>2.9700e-003</b>	<b>0.0817</b>	<b>0.0817</b>	<b>0.0817</b>	<b>0.0817</b>	<b>0.0817</b>	<b>0.0817</b>	<b>0.0000</b>	<b>281.4481</b>	<b>281.4481</b>	<b>0.0183</b>		<b>281.9062</b>

Garden Grove Blvd Mixed Use Proj 2021 - Orange County, Summer

**3.6 Architectural Coating - 2022**  
**Mitigated Construction Off-Site**

Category	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
lb/day																
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	0.0512	0.0297	0.4252	1.5200e-003	0.1677	1.0600e-003	0.1687	0.0445	9.8000e-004	0.0455	151.9802	151.9802	151.9802	3.0700e-003		152.0569
<b>Total</b>	<b>0.0512</b>	<b>0.0297</b>	<b>0.4252</b>	<b>1.5200e-003</b>	<b>0.1677</b>	<b>1.0600e-003</b>	<b>0.1687</b>	<b>0.0445</b>	<b>9.8000e-004</b>	<b>0.0455</b>	<b>151.9802</b>	<b>151.9802</b>	<b>151.9802</b>	<b>3.0700e-003</b>		<b>152.0569</b>

**4.0 Operational Detail - Mobile**

**4.1 Mitigation Measures Mobile**



Garden Grove Blvd Mixed Use Proj 2021 - Orange County, Summer

Category	lb/day															
	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Mitigated	1.1900	4.0443	14.8603	0.0580	5.4306	0.0393	5.4699	1.4522	0.0364	1.4886	5,899,009	6	5,899,009	0.2327		5,904,827
Unmitigated	1.1900	4.0443	14.8603	0.0580	5.4306	0.0393	5.4699	1.4522	0.0364	1.4886	5,899,009	6	5,899,009	0.2327		5,904,827

4.2 Trip Summary Information

Land Use	Average Daily Trip Rate				Unmitigated		Mitigated	
	Weekday	Saturday	Sunday	Annual VMT	Annual VMT	Annual VMT	Annual VMT	
Apartment Low Rise	344.76	344.76	344.76	1,178,097	1,178,097	1,178,097	1,178,097	
Enclosed Parking with Elevator	0.00	0.00	0.00	611,213	611,213	611,213	611,213	
Medical Office Building	309.03	88.95	15.39	580,226	580,226	580,226	580,226	
Strip Mall	304.97	304.97	304.97	2,369,536	2,369,536	2,369,536	2,369,536	
Total	958.75	738.67	665.11					

4.3 Trip Type Information

Land Use	Miles				Trip %				Trip Purpose %						
	H-W or C-W	H-S or C-C	H-O or C-NW	H-W or C-W	H-S or C-C	H-O or C-NW	Primary	Diverted	Pass-by	H-W or C-W	H-S or C-C	H-O or C-NW	Primary	Diverted	Pass-by
Apartment Low Rise	14.70	5.90	8.70	40.20	19.20	40.60	86	11	3						
Enclosed Parking with Elevator	16.60	8.40	6.90	0.00	0.00	0.00	0	0	0						
Medical Office Building	16.60	8.40	6.90	29.60	51.40	19.00	60	30	10						
Strip Mall	16.60	8.40	6.90	16.60	64.40	19.00	45	40	15						

4.4 Fleet Mix

Garden Grove Blvd Mixed Use Proj 2021 - Orange County, Summer

Land Use	LDA	LDT1	LDT2	MDV	LHD1	LHD2	MHD	HHD	OBUS	UBUS	MCY	SBUS	MH
Apartments Low Rise	0.563406	0.043070	0.209298	0.109958	0.015015	0.005784	0.026182	0.017546	0.001775	0.001524	0.004941	0.000598	0.000904
Enclosed Parking with Elevator	0.563406	0.043070	0.209298	0.109958	0.015015	0.005784	0.026182	0.017546	0.001775	0.001524	0.004941	0.000598	0.000904
Medical Office Building	0.563406	0.043070	0.209298	0.109958	0.015015	0.005784	0.026182	0.017546	0.001775	0.001524	0.004941	0.000598	0.000904
Strip Mall	0.563406	0.043070	0.209298	0.109958	0.015015	0.005784	0.026182	0.017546	0.001775	0.001524	0.004941	0.000598	0.000904

5.0 Energy Detail

Historical Energy Use: N

5.1 Mitigation Measures Energy

Category	lb/day										lb/day					
	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio-CO2	NBio-CO2	Total CO2	CH4	N2O	CO2e
NaturalGas Mitigated	0.0228	0.1967	0.0958	1.2400e-003		0.0158	0.0158		0.0158	0.0158		248.9088	248.9088	4.7700e-003	4.5600e-003	250.3879
NaturalGas Unmitigated	0.0228	0.1967	0.0958	1.2400e-003		0.0158	0.0158		0.0158	0.0158		248.9088	248.9088	4.7700e-003	4.5600e-003	250.3879

Garden Grove Blvd Mixed Use Proj 2021 - Orange County, Summer

**5.2 Energy by Land Use - NaturalGas**

**Unmitigated**

Land Use	NaturalGas Use kBtu/yr	lb/day															
		ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Apartments Low Rise	1817.8	0.0196	0.1675	0.0713	1.0700e-003	0.0135	0.0135	0.0135	0.0135	0.0135	0.0135	0.0000	0.0000	213.8594	4.1000e-003	3.9200e-003	215.1302
Enclosed Parking with Elevator	0	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Medical Office Building	248.583	2.6800e-003	0.0244	0.0205	1.5000e-004	1.8500e-003	1.8500e-003	1.8500e-003	1.8500e-003	1.8500e-003	1.8500e-003	0.0000	0.0000	29.2451	5.6000e-004	5.4000e-004	29.4188
Strip Mall	49.337	5.3000e-004	4.8400e-003	4.0600e-003	3.0000e-005	3.7000e-004	3.7000e-004	3.7000e-004	3.7000e-004	3.7000e-004	3.7000e-004	0.0000	0.0000	5.8044	1.1000e-004	1.1000e-004	5.8388
<b>Total</b>		<b>0.0228</b>	<b>0.1967</b>	<b>0.0958</b>	<b>1.2500e-003</b>	<b>0.0158</b>	<b>0.0158</b>	<b>0.0158</b>	<b>0.0158</b>	<b>0.0158</b>	<b>0.0158</b>			<b>248.9088</b>	<b>4.7700e-003</b>	<b>4.5700e-003</b>	<b>250.3879</b>

Garden Grove Blvd Mixed Use Proj 2021 - Orange County, Summer

**5.2 Energy by Land Use - NaturalGas**

**Mitigated**

Land Use	NaturalGas Use kBtu/yr	lb/day															
		ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Apartments Low Rise	1.8178	0.0196	0.1675	0.0713	1.0700e-003	0.0135	0.0135	0.0135	0.0135	0.0135	0.0135	213.8594	213.8594	4.1000e-003	3.9200e-003	215.1302	
Enclosed Parking with Elevator	0	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	
Medical Office Building	0.248583	2.6800e-003	0.0244	0.0205	1.5000e-004	1.8500e-003	1.8500e-003	1.8500e-003	1.8500e-003	1.8500e-003	1.8500e-003	29.2451	29.2451	5.6000e-004	5.4000e-004	29.4188	
Strip Mall	0.049337	5.3000e-004	4.8400e-003	4.0600e-003	3.0000e-005	3.7000e-004	3.7000e-004	3.7000e-004	3.7000e-004	3.7000e-004	3.7000e-004	5.8044	5.8044	1.1000e-004	1.1000e-004	5.8388	
<b>Total</b>		<b>0.0228</b>	<b>0.1967</b>	<b>0.0958</b>	<b>1.2500e-003</b>	<b>0.0158</b>	<b>0.0158</b>	<b>0.0158</b>	<b>0.0158</b>	<b>0.0158</b>	<b>0.0158</b>	<b>248.9088</b>	<b>248.9088</b>	<b>4.7700e-003</b>	<b>4.5700e-003</b>	<b>250.3879</b>	

**6.0 Area Detail**

**6.1 Mitigation Measures Area**

Use only Natural Gas Hearths

Garden Grove Blvd Mixed Use Proj 2021 - Orange County, Summer

Category	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Mitigated	1.7950	0.8260	4.6419	5.1800e-003	0.0866	0.0866	0.0866	0.0866	0.0866	0.0866	0.0000	998.8275	998.8275	0.0265	0.0182	1,004.9055
Unmitigated	15.3300	1.1286	30.7562	0.0677	3.9960	3.9960	3.9960	3.9960	3.9960	3.9960	487.0798	943.7687	1,430.8485	1.4601	0.0331	1,477.2029

**6.2 Area by SubCategory**  
**Unmitigated**

SubCategory	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Architectural Coating	0.1428				0.0000	0.0000	0.0000	0.0000	0.0000	0.0000			0.0000			0.0000
Consumer Products	1.4302				0.0000	0.0000	0.0000	0.0000	0.0000	0.0000			0.0000			0.0000
Hearth	13.6259	1.0789	26.4447	0.0675	3.9722	3.9722	3.9722	3.9722	3.9722	3.9722	487.0798	936.0000	1,423.0798	1.4526	0.0331	1,469.2456
Landscaping	0.1312	0.0497	4.3115	2.3000e-004	0.0238	0.0238	0.0238	0.0238	0.0238	0.0238	7.7687	7.7687	7.7687	7.5400e-003		7.9573
<b>Total</b>	<b>15.3300</b>	<b>1.1286</b>	<b>30.7562</b>	<b>0.0677</b>	<b>3.9960</b>	<b>3.9960</b>	<b>3.9960</b>	<b>3.9960</b>	<b>3.9960</b>	<b>3.9960</b>	<b>487.0798</b>	<b>943.7687</b>	<b>1,430.8485</b>	<b>1.4601</b>	<b>0.0331</b>	<b>1,477.2029</b>

Garden Grove Blvd Mixed Use Proj 2021 - Orange County, Summer

**6.2 Area by SubCategory**

**Mitigated**

SubCategory	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
	lb/day															
Architectural Coating	0.1428					0.0000	0.0000	0.0000	0.0000	0.0000			0.0000			0.0000
Consumer Products	1.4302					0.0000	0.0000	0.0000	0.0000	0.0000			0.0000			0.0000
Hearth	0.0909	0.7763	0.3304	4.9600e-003		0.0628	0.0628	0.0628	0.0628	0.0628	0.0000	991.0588	991.0588	0.0190	0.0182	996.9482
Landscaping	0.1312	0.0497	4.3115	2.3000e-004		0.0238	0.0238	0.0238	0.0238	0.0238		7.7687	7.7687	7.5400e-003		7.9573
<b>Total</b>	<b>1.7950</b>	<b>0.8260</b>	<b>4.6419</b>	<b>5.1900e-003</b>		<b>0.0866</b>	<b>0.0866</b>	<b>0.0866</b>	<b>0.0866</b>	<b>0.0866</b>	<b>0.0000</b>	<b>998.8275</b>	<b>998.8275</b>	<b>0.0265</b>	<b>0.0182</b>	<b>1,004.9055</b>

**7.0 Water Detail**

**7.1 Mitigation Measures Water**

**8.0 Waste Detail**

**8.1 Mitigation Measures Waste**

**9.0 Operational Offroad**

Equipment Type	Number	Hours/Day	Days/Year	Horse Power	Load Factor	Fuel Type
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**10.0 Stationary Equipment**

Garden Grove Blvd Mixed Use Proj 2021 - Orange County, Summer

**Fire Pumps and Emergency Generators**

Equipment Type	Number	Hours/Day	Hours/Year	Horse Power	Load Factor	Fuel Type
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**Boilers**

Equipment Type	Number	Heat Input/Day	Heat Input/Year	Boiler Rating	Fuel Type
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**User Defined Equipment**

Equipment Type	Number
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**11.0 Vegetation**

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